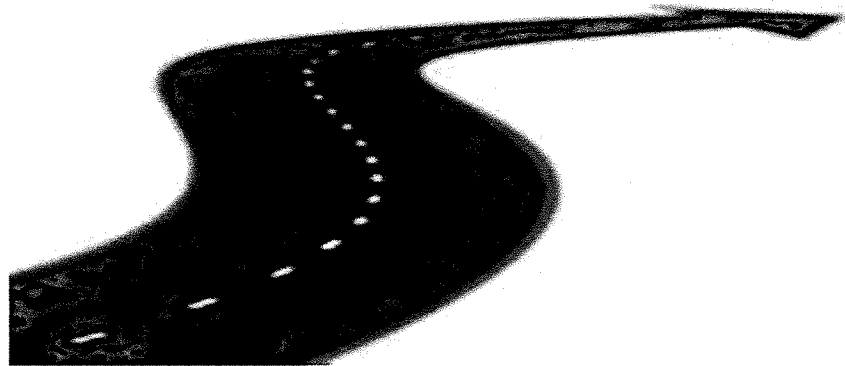


City of Kingsville  
Street Maintenance Improvement Program  
(SMIP)  
Special City Commission Workshop  
December 15, 2014



City of Kingsville  
 Street Maintenance Improvement Program (SMIP)  
 Special City Commission Workshop  
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# City of Kingsville, Texas

## Proposed Financing Methods for City Streets

### Commentary Assumptions & Perspectives Q&A

#### (1) **Commentary**

##### The Early Years- How did our streets, curbs & gutters get into this condition?

So far as I can tell the City of Kingsville has rarely had a consistently reliable, annual, ongoing source of funding for its streets, curbs and gutters. In my view, this reality has been and continues to be the leading cause of poor streets, curbs and gutters.

Staff's research, which included reviewing documents as far back as July 2015 (see documents following), shows that the City relied upon a number of different funding mechanisms comprised mostly of bonds, special street assessments, and an occasional property tax initiative. Between 1972 and 1986 when the Program ended, the City of Kingsville likely used and relied upon the Federal Revenue Sharing Program to pay for repairs and upgrades to these assets. When that program ended in 1986, the City was likely left once again without an adequate source of annual, ongoing funding for these assets.

##### Three Years Ago

Approximately three years ago City staff inventoried, assessed and quantified the condition (Good, Fair or Poor) of all City asphalt & concrete streets and curbs & gutter assets on a per mile basis. This information was later presented to the City Commission at which time it was agreed that staff needed to change and improve its construction methods in order to maximize the life expectancy of these assets at a lower average annual cost to our residents. It appeared to me at that time that the City had not, or at least not recently, inventoried or rated the condition of these assets. This lack of assessment violated an essential truth, which is that if it can't be measured, it can't be improved.

##### Today

Everything that has happened in the past leads us to this point in time. Since that first streets workshop with the City Commission two plus years ago, Charlie and his staff have developed and tested a Geographic Information

System (GIS) containing 66,000 data elements for every mile, block, and square yard of city street, curb and gutter. Had the City Commission not

# City of Kingsville, Texas

## Proposed Financing Methods for City Streets

### Commentary Assumptions & Perspectives Q&A

authorized the purchases of the GIS Software and Server three years ago our staff would not have been able to develop the City's GIS tool nor would we have the high quality and engineering certainty contained in the GIS data for making important decisions. It deserves mention that most City's would have hired an engineering/consulting firm at great expense to develop and refine their GIS. I am proud to say that our City's GIS tool has been created internally at a far lower cost. I congratulate Charlie and his team for this amazing and successful effort.

For each of the last three budget cycles, my budget message has included a comment to the City Commission that the use of bond funds is not the best way to fund street improvements. I have said that while bond funds are better than no funds, the use of bond funds doesn't achieve the City's objective of having a sufficient, annual, ongoing source of funding for its streets, curbs and gutters. Bond funds are, however, a preferred source of funding for larger, intermittent street and infrastructure projects.

Time is of the essence because the City's primary source of funding for streets, which has been the use of bond funds, is set to be exhausted at the end of the current 2015 fiscal year (9/30/15) only 9 months from now.

#### **(2) Assumptions & Perspectives**

- Our goal is the elimination of all poor streets (those having a CRI < 50 on a 100 point scale) and the high number of potholes that come with them. This is arguably the number one immediate priority for our City. Better streets = safer streets, fewer potholes, happier motorists, more attractive neighborhoods and corridors, and a more livable city.

- Anything of importance that is worth doing, is worth doing well - streets included.

- In order to do anything well, it must be planned, measured and monitored. Our plan will, if used properly and updated timely, outlast individual staff members and elected officials.

# City of Kingsville, Texas

## Proposed Financing Methods for City Streets

### Commentary Assumptions & Perspectives Q&A

●Streets satisfy two important public needs: traffic conveyance and water conveyance. Traffic conveyance is often financed using sales taxes, property taxes and other general fund sources. Water conveyance can and often is funded using a Storm Water Fee in a Utility Fund. The reality of our streets (design and function)make them an operational hybrid – part transportation system (General Fund) and part water conveyance system (Utility Fund).

●The most cost effective solutions for streets (best overall value for our residents) are those that cost the least in the long term even though they might cost more in the short term. Spending more now gives us streets that last longer so that over the life of the street the City is actually spending less per year.

●Unless there is a pressing need to do so, streets should not be replaced until they are at the very end of their useful lives. To replace a street before the end of its useful life would be to waste resident and taxpayer resources.

### (3) Q&A

#### (i) Why will Charlie's Plan (SMIP)Work?

- Better and more substantial financing
- Better management
- Better supervision
- Better workers
- Better equipment
- Better engineering science
- Better analytical tools
- Better consistency
- Better flexibility – ability to change course based on new information
- Better results

#### (ii) What justifies the use of storm water fees for improving City streets?

As already mentioned, the use of storm water fees is justified because streets function as a water conveyance system as well as a traffic conveyance system. The design and construction of streets has as much or

# City of Kingsville, Texas

## Proposed Financing Methods for City Streets

### Commentary Assumptions & Perspectives Q&A

more to do with avoiding water infiltration as it does withstanding traffic loads. Moreover, the single largest contributing factor to street deterioration is water. For an asphalt street, water, especially standing water, is like acid.

**(iii) Which is better, financing streets using property taxes or storm water drainage fees?**

I'd say they each have their place in financing City streets, but there is at least one large and important difference...storm water fees are charged to everyone because there are few, if any, exemptions to the storm water fee. Property tax, on the other hand, is riddled with exemptions, which means that those who don't pay property tax also don't help pay directly for City streets. Having part of our City streets paid for by storm water fees ensures that most every property owner will pay at least something towards city streets.

Paying for a portion of City streets using the storm water fee would produce a more equitable distribution of those costs while achieving a higher level of fairness.

Also, because more people pay the storm water fee than pay the property tax, the storm water fee generates higher amounts of revenue more quickly.

**(iv) If adopted, does your proposed use of storm water fees for street repair change or conflict with the engineering assumptions contained in the storm water study prepared by the engineering firm?**

No. I am proposing an increase to the storm water drainage fee and have not changed or proposed anything that is contrary to the underlying engineering assumptions or measurements. The initial storm water fee was purposely limited and established at a rate per ERU that would only cover amounts needed to make bond payments. The ERU rate itself bears no relationship to a cost-of-service study or anything of that nature. Staff and I reviewed the proposed fee increase (the concept not the specific amount) with the engineer who performed the Storm Water analysis. Although I

**City of Kingsville, Texas**  
**Proposed Financing Methods for City Streets**

**Commentary**  
**Assumptions & Perspectives**  
**Q&A**

would stop short of saying that the engineer gave us his blessing, he did not offer any comments that indicated this was not a viable or appropriate financing option, so long as we left the engineering assumption in place.

**(v) If the new revenue is approved how will you track and account for it?**

The new revenue will be used solely to achieve the objectives outlined in the Street, Maintenance Improvement Program (SMIP) provided to you and prepared by Charlie Cardenas and his staff. Before the revenue is expended, it will be placed in a self-restricted account to be used solely for streets. Any moneys collected, but not expended in a given year, will be carried over and used exclusively for street and curb/gutter expenditures in subsequent years.

**(vi) Is any of the City's current property tax used for streets, curbs and gutters?**

Yes. The City's General Fund revenues (mostly sales and property taxes) are used to finance all of the City's personnel, supplies and other operational costs in the Streets Division of Public Works Department. These costs are substantial and are budgeted for FY2015 at \$1,456,616. If we assume that this amount is funded entirely from the City's M&O Tax levy, it would equate to a levy of \$.20391 or 31% of the total M&O Tax levy of \$.65558

**(vii) If the General Fund and property taxes are used to pay for personnel, supplies and other operational costs, what will the new revenue be used for?**

The new revenue will mostly replace the bond proceeds, which have been used to pay for street materials (asphaltic material used by City streets employees) and subcontractor costs (concrete, fog seal, rejuvenation and other subcontractor materials and services)

**(viii) If the City is no longer using bond funds for annual, ongoing street, curb and gutter repairs and replacements, will you still issue bonds?**

Yes. I would propose that the future bond funds be used for larger and intermittent street, infrastructure and capital outlay projects.

4/22/70 - City Commission OK's Contract for Road Funds. ①

TOPICS Program (Traffic Operations Prog to Increase Capacity)

Federal Program to assist municipalities to meet increasing demand of st. construct. & maint.

\* 1.25M set aside for DoT & Tex

6/24/70 Tax Rate Adopted  
Went from \$1.65 to 2.00

Adtd rate would raise approx 150,000 New revenue

\* 100,000 set aside for street maint & repair

8/28/70 \$112,000 St. Imp Progs.  
from Budget

2/3/71 - Called \$3M  
Vote 25+2

approved 2/28/71 { \$500,000 for St. Improvements  
\$700,000 for storm sewer/drainage work

5/5/71 - Announcement of New <sup>St.</sup> Parking Policy  
Low Income Homeowners to contract w/ city for their share of parking costs then pay back w/ no



Limited to Northeast section of town

Voluntary petition

City pays 1/3 Property owners on other two sides 1/3 ea

Spread over 3 years

8/4/71 - Approval of 20 Blk Paving Program  
Low Income Area  
over 5 year

8% interest

Issue "Time Warrant" to finance cost of entire program

City's 1/3 to be pd from general fund

9/1/71 - Budget Adopted \$2.5M  
included \$105,000 addtl dollars for st. improve.

9/12/71 - Advertised Bids for sale of \$550,000 in GO Bond  
to finance 1st work on thoroughfare program  
to improve Santa Gertrudi + Armstrong

9/22/71 - \$550,000 Bonds approved  
Part of the \$3M authorized by voters Feb 71

10/27/71 - Assessments determined for Paving Program  
Prop Owners \$75,138.45 = \$2.50/linear ft  
City \$51,000.00 = \$2.40 curbs/gutters  
w/side lot for \$1.25/ft  
\$2.40 curb gutters

11/7/71 \$300,000 grant from HUD

(3)

\$250,000 public improvement

\$50,000 housing rehab

11/3/71 Code Enforcement Project 2

include water lines, streets, curbs + gutters

Part of the \$3M Bonds

\$900,000 Fed. Code Enforcement Program

12/22/71 - \$394,274 approved for st. paving, sidewalks  
curbs, gutters, drainage improve  
for Cadillac Terrace

subject to approval of HUD who is  
participating in funding

\$141,756 Fed Funds

198,000 Bonds

54,518 City Funds

Chandler Acres paving program

5 yrs @ 7% Int.

3/15/72 - Paving Assessment Program 72-A

20 Blks in NE part of City

Similar to existing 25 Blk Progr.

5 yrs. 8% Int

3/29/72 Aulsie St. approved for repaving  
Contractor to redo  
No Cost to City

6/7/72 2nd Paving Proj Assessment Approved

\$4.90 linear foot

3.65 linear foot

Owners not able to pay paving assessments  
financed thru City Loan

6/18/72 - 2nd Code Program Gets Federal Approval  
Covers Fairview area on NE side

\$187,106 - public improv for neighborhood  
improv. residential home.

some for reramping curbs/gutter  
streets/drainage

6/21/72 Repaving assessment program

15 blk bounded by 14th + King

5 yrs @ 8%

6/28/72 City receives 2nd yr funds for Code Enforcement  
Projects

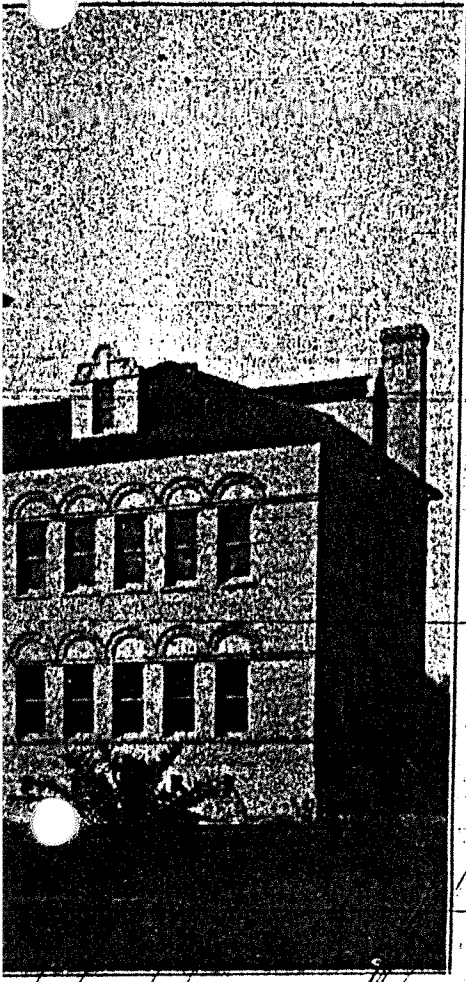
\$800,000

Complete reconstruction of Armstrong

Curbs/Gutter work

# STREET PAVING BONDS SOLD; ELECTION TO BE HELD JAN. 12

## S. L. Austin & Company Purchase \$125,000 Issue, Paying Five Per Cent and Accrued Interest.—Gentry & Agar Awarded Contract for Engineering, and Have Already Begun Work.



ED AT COST OF \$125,300

## BIG TIME VAUDEVILLE AT AM. LEGION HALL SATURDAY NIGHT, 12

Legion Boys Bringing Best Vaudeville Talent of Big Circuit to Show in Kingsville Weekly.

Kingsville isn't such an awfully "hick" town after all. Not so long ago there was a real Grand Opera entertainment here, although there are some Kingsvillians who probably did not know about it or if they did know and knew what grand opera was they probably said it only bunk and that grand opera wouldn't come to Kingsville, nevertheless, it did. And

(continued on last page)

The second important step in Kingsville's proposed paving program was taken last Tuesday afternoon at the city offices when bonds in the sum of \$125,000 were sold to S. L. Austin and Company, of Austin, Texas, for 5 per cent and accrued interest.

Present at this meeting were members of the Commercial Club's street and road committee, composed of Gay Brinson, chairman, B. O. Sims, John D. Finnegan, Jno. Cypher, Herbert Andrews and E. B. Erard, secretary. Mayor Gonde and Commissioners Hollingsworth and Mecklin represented the city, while representatives from four bonding houses were present in person, with several others bidding by telegram.

Bids were opened and read before this body and were given some hour or more discussion when the total was hammered down to two concerns, the above named Austin & Company bid being accepted over the one competitor because of the fact that the Austin concern agreed to deposit the total sum, minus commission in some bank designated by the Commission, upon receipt of the bonds, while the competitive bonding house agreed to pay in installments, reaching over a period of several months.

The price at which the bonds were sold is considered very good by the Commercial Club committee, many of them familiar with the bond market, and the opinion was general that the immediate present was an accepted time for disposition as there is marked tendency toward lower levels in the eastern markets for this paper.

Immediately following acceptance of the bonding company's offer, and signature of agreement by the Mayor, the Commercial Club secretary was instructed to circulate a petition with the legally required number of taxpayers, petition a call for an election. This election, according to law, cannot be

held until January 12th; a full thirty days following publication of first Election notice, to be found in another section of this paper.

It is of course understood that the \$125,000 as represented in the sale of city of Kingsville bonds, incorporates the town's one-third of the paving issue, all street intersections and expense of storm sewer installation. The full paving program as outlined by the Commercial Club committee and City Commission calls for an approximate expenditure of two hundred thousand dollars, with the abutting property owners bearing the usual two-thirds of the actual paving issue.

At a recessed meeting of the City Commission, Thursday afternoon, the bond sale was legally approved, and the firm of Gentry and Agar, civil engineers, employed to carry out that part of the program. This same firm had the preliminary survey work in charge, Mr. Gentry, junior member of the firm, reported for duty Monday of the present week, in response to telegram, and will immediately set about preparation of matter that the work may be placed before construction companies for a competitive bid. These bids will be received as soon as possible and the contract let in ample time preceding the election that the tax payers might know exactly the amount of money to be expended and routes finally decided upon.

The rather unusual process of selling bonds and awarding of contracts was occasioned by the delay experienced in bringing the proposition up to its present status. The commercial club's street and road committee has been working on the proposed project for several months, and it is hoped to witness actual construction immediately following the election on January the 12th, when everything will be in readiness for the contractors to begin laying the cement or other type of paving selected.

# W. L. PIERSON COMPANY TO BEGIN PAVING AND SEWER WORK MARCH 1

## Refusal of Attorney General to Approve Paving and Sewer Bond Election Will Not Delay Construction Work.—New Election Is Called for March 27th.—Pierson and Company to Accept City Warrants.

Both work of digging ditches for storm sewers and excavation for actual laying of pavement will begin about March first, is the latest news, Tuesday afternoon, from the Mayor's office. This conclusion was reached last Friday afternoon, after consultation held with Mr. W. L. Pierson, of the W. L. Pierson Company, paving contractors, having been awarded the contract for this work. Mr. Pierson has agreed to begin work at once, accepting City of Kingsville warrants in payment up to some eighty thousand dollars, or near the sum of money to be advanced by the city for that portion of paving cost assessed against abutting property, before such sum, in warrants would have been taken over and absorbed by street improvement bonds. It is understood that the issuance of these warrants and their acceptance by W. L. Pierson and Company entails no extra expense, but is merely a matter of accommodation by the paving concern that work should not be delayed any longer.

Mayor Goode, in conversation with the Recorder, expressed himself, and voiced the sentiment of the City Commission, as bitterly disappointed that the Attorney General had not seen fit to approve the record of Kingsville street improvement bonds recently voted. "Another election is called for March 27th," said the Mayor, "but it is our determination that work of paving shall not be delayed beyond the time originally set for such work. We believe the people will approve this decision, and vote at the next election one hundred per cent. Our representative, Mr. E. H. Crenshaw, Jr., in charge of the record and sent to Austin with the papers for approval, states that he was surprised that the record was not approved, but in view of the Supreme Court decision relative to the Archer county case, the Attorney General had be-

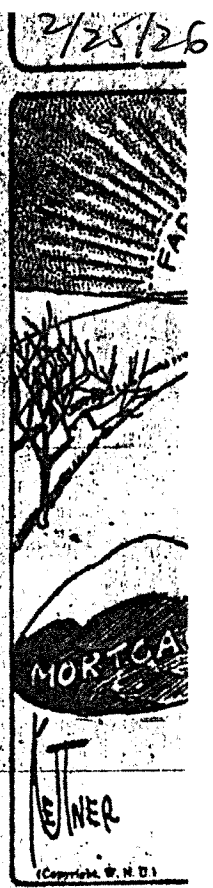
come much more technical in the matter of approving bonds. The record was not approved because the order so read as to include both street improvement and storm sewer work. The mistake will not again be made."

It is understood that the city of Ft. Worth, seeking approval of a six million dollar issue, only a few days before this, had the same experience; a dual purpose being incorporated in the election notice.

Lon Messer, with the W. L. Pierson Company, was in the city Monday of this week and stated that his company would begin moving ditch digging and street paving machinery to Kingsville this week, preparatory to beginning work about the first of March. As some ten blocks in the business section will not be affected by storm sewers direct, it is planned to start the work of cement paving along with sewer laying, the former to begin in the business section of town, on Kleberg avenue, while the latter work will be started west of the railroad tracks somewhere. At least this is the present plan, as suggested by the engineer and Mr. Messer. Ordinarily sewer contractors require a two weeks lead on paving crews, but due to the above recited facts, both will begin in Kingsville at the same time. It is also stated, by way of suggestion from Mr. Pierson, that a chemical process calculated to hasten solidification of concrete be used at least on the main business streets. This method will probably be adopted, as the time demanded for a 'closed block' will be reduced by several days.

Immediately after being notified by telephone message from Austin, that the Attorney General's office had seen fit to disapprove the record, members of the Commission in session, re-sold the street improvement bonds to Garrett & Company, of San Antonio, at

(continued on last page)



## NEW MACHINERY FOR MANUFACTURE OF FORD

Kingsville Cot 000 Worth Manufacture Linings.

There is being Kingsville Cot something like new machinery ed during the dditional machi the manufactu ing for Ford to Manager Jo mill in the Sou transmission II being located Jones stated t large demand being the main of the lessee ( tonio Cotton N stall the machi in cotton price ton twine has other element ion to increase manufacture of Jones stated th

## EXTENSIVE PAVING PROGRA

**Additional Paving Financed Up To \$325,000—All Streets and Avenues Which Have Petitioned for Paving To Be Paved, and As Much More As Will Come Within Limit of Finances.—This In Addition to the Work Already Under Way.—W. L. Pearson Company to Continue the Work at Once.—Many of Residence Streets to Be Paved.**

At a meeting of the City Commissioners Tuesday afternoon, definite arrangements were made with the W. L. Pearson Company, contractors, who are doing the storm sewer and paving work in the city, for a comprehensive program of paving, covering not only that part of the city where petitions have been circulated, but on other streets where petitions may be circulated later on.

In other words, financial arrangements have been made up to \$325,000 for the continuance of a paving program in the city. The above mentioned amount is in excess of the present bond issue and the streets to be paved are in addition to that paving upon which the contract has already been let and upon which work is now in progress.

Mayor Goode stated Wednesday morning that he had been in conference with representatives of the Pearson Company from time to time to the end that some arrangement could be made looking to the continuance of the program and last week in conference with officials of the company, the present plan had been worked out.

Work of digging the present storm sewers will in all probability be completed this week, and it is planned to put the big ditching machine at work at once upon the extensions and paving on these streets will be completed just as rapidly as possible, though it is planned to complete that part of the work originally contracted for, first.

A number of petitions for paving have been turned in from time to time. Among the streets to be paved will be all of Ninth from Kleberg avenue out to the hospital, all of East Kennedy from Sixth street east to Eleventh street; two blocks on King (in addition to the one block already under contract); all of Lotl from Sixth east; two blocks on Fordyce; all of West King from Fifth out to the cemetery road; three blocks in Mexico; two blocks or more on Kleberg avenue west of the high school.

It is believed that as soon as it is known that additional paving can be had, that other petitions will be circulated and signed.

Mayor Goode stated that plans were being worked out whereby the cost on the additional paving to the property owners would be approximately the same as on the contract paving, tho these plans had not been worked out to that extent that the plans could be made public.

### BATHING SEASON OPENS AT BEACH

Hoyt Kenyon, manager of the Riviera, stated Sunday evening that the bathing season opened with a rush Sunday, the first real summer day of 1926, that is, a day when people took kindly to the water.

"From the standpoint of bathing and fishing the present season has been later than for a number of years," said Mr. Kenyon. "The water has been just a little too cold for bathing and it is the belief of fishermen that the fish have not come into the back bays in any great numbers because the waters are shallow and have been too cold. However the past week of sunshine and warm weather has made the water fine and we are expecting lots of business. Bathers report the water as 'just fine' this season and recent improvements in the Riviera Beach road, making it one of the very best roads in the county, will be a big drawing card to the Beach this season."

Mr. Kenyon stated that they expected to make especial preparations to take care of guests at the hotel this season and are expecting a splendid patronage. The road from Riviera out to the Beach is in splendid shape, much better than it has ever been. The water is very high for this season of the year and everything points to a most prosperous season for Kleberg county's playground on Bathing Bay.

### Write Mother Today



### MRS. SIMS IN RACE FOR COUNTY CLERK

Believes Experience Gained During Jersey Cattle Club and Blacklanders Fits Her for Duties of County's Most Important Office.

Mrs. G. E. Sims, who has served the city as clerk for the past several years in the most efficient manner, this week her intention of entering the race in the county for County and District Clerk, numbers of her friends in the city and over the county having solicited her to make the race and backed their solicitation by pledging her their active support.

"I have decided to enter the race for clerk," said Mrs. Sims. "For several reasons, chief among which is the fact that I believe that I am qualified to fill the position to the entire satisfaction of the people of Kleberg county. During the many years I have been connected with the city of Kingsville, the nature of my work has been such that I believe that it has fitted me to assume the office of county and district clerk. I am asking the people of the county for the office just as any taxpaying citizen has the right to ask for any office in the gift of the people. If elected I promise to serve you just as I have tried to serve the citizenship of Kingsville, in the very best way I know how."

"That there may be no misunderstanding, I wish to state that I expect to temporarily resign my position as City Clerk for sixty days in order that I may make a complete campaign over the entire county. That I may keep faith with the citizenship of Kingsville I will from time to time during this sixty days, take time to supervise and direct the work in the city clerk's office, though I will receive no remuneration for the same."

### JOINT BARBECUE TO BE STAGED MAY 13

Jersey Farm.—1000 Visitors Expected.

Thursday, May 13th, is to be a red letter day in the history of the dairy industry in Kleberg county.

On the above date the Texas Jersey Cattle Club will be the guests of the Blacklanders and the Kleberg County Dairy Association. The meeting and barbecue will be staged near the Santa Gertrudis dairy on the King ranch. Mr. Oscar Anderson, who is a member of the T. J. C. Club, states that he expects more than 200 members of the Club to make the trip to Kingsville. They will hail from several states, but for the most part from North Texas, and will arrive on a special train the morning of the 13. Secretary Erard is asking that citizens of the town furnish about 25 or 30 cars to convey the visitors to the Ranch and other points of interest after the meeting there is over.

The Blacklanders, on learning of the meeting of the Jersey Cattle Club at Kingsville, arranged to combine their meeting and barbecue with the dairymen, in order that their members could hear first hand from the speakers of the Jersey Club, some of the advantages of one form of diversification which seems most ideally fitted to South Texas. Mr. Vance Griffith, secretary of the Blacklanders, states that his organization is to have one speaker on the program and that an effort is being made to secure a speaker of national repute, who will deal with the subject of diversification.

Secretary Erard of the Commercial Club states that about 1000 visitors are

## STREET PAVING ACTUALLY BEGUN

Without any inkling of what was about to happen, City Engineer Porter closed Sixth Street between Kleberg and Yoakum Avenues this week and put a force of graders to work grading down the street preparatory to paving it. Every one was curious to know just what was happening, as absolutely nothing had been given out concerning any paving or anything of that nature. It developed that the city, together with the property owners, was to pave this block as an experiment, which if successful, will mean that a great deal of paving will be done.

It is understood that about five inches of gravel from Fordyce will be put down for foundation, this to be covered with Tarvia, a paving compound which is rapidly gaining popularity. The Tarvia will be put on about five-eighths of an inch thick and will be "treated" at least three times. It is understood that this kind of paving will last from five to ten years and that the cost is not much more than half of what the ordinary kinds of paving cost.

It will be remembered that members of the city council made a trip last year to see paving in other cities and gain information as to the best kind to use. At that time the paving of Kleberg Avenue was considered a certainty, but the European conflict and the consequent stagnation of business delayed the paving at that time. While it is not definitely given out that the paving of the avenue and interesting streets is to be done, it is pretty well understood that the present test is to determine whether this style of paving is satisfactory. If so, it is more than probable that quite a number of blocks will be paved at once.

## AUGUST FIRST LAST

## CEMENT COVER ON WATER TANK

The contract has been let to put a cement-roof over the storage tank of the water company. There has been some complaint from the public lately on account of there being no cover on the tank and the company, always willing to do their very best to accommodate the public, at once began perfecting plans to have the tank covered. Manager Kleberg asks that the people use the water as sparingly as possible while the work is going on, as it will be impossible to give the usual supply.

## NEAR SERIOUS FIRE AVERTED

Passers by discovered fire in Ben T. Laws' office Monday night about eleven o'clock. Entrance was forced into the building and it was discovered that the fire had started in one of the drawers of the desk, but had gained little headway. It was quickly extinguished, very little damage being done. Just how the fire started is not known, but it is supposed that some one laid down a lighted cigar or cigarette just before locking up for the night and that it fell in the drawer and slowly ignited the papers. Quite a number frequent the office and it is more than probable that the fire started in this way. A few papers were damaged to some extent.

## MANY CANINES LOSE LIBERTY

City Marshall Nichols and his corps of assistants have been busy this week rounding up the canines that are not properly tagged with a license to roam the streets of Kings-

## GOOD WORK JULY

Mrs. F. Stafford of the Junior League has been very successful in her campaign, which was completed several weeks ago. The campaign was so successful that there was no danger from disease. Stafford reports to pay for the work that has been raised, have been very successful in any one who felt out the Junior

a check or don-

An employment established in Civic League a lots who wish will communica-

A campaign yards has been League and e will be given t to make home s tractive. Arran made with the the penny pa every boy and thing at home.

## KING RAN IN SPI

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# PAVING PROGRAM UP TO THE PEOPLE

Committee Recommends Extensive Program.—Six Inch Concrete Type. Storm Sewers Necessary.

If plans approved by the Paving Committee of the Commercial Club, when that body met with the City Commission to hear the report of Mr. Bruce Gentry, who was employed to make preliminary surveys and estimates on the paving and drainage to be attempted, construction should start shortly after the first of the year, according to a statement made by Secretary Erard.

"Mr. Gentry has submitted a very complete report together with estimates on two types of construction, concrete and concrete bitullitic. His figures, which are on file in the office of the Mayor and which will be published in next week's Record, give each property holder along the street proposed for paving a very accurate estimate of the cost to be borne by him, in event either of the two types of paving are determine dupon

"Our committee, realizing the necessity of a conservative program and desiring to have really permanent streets, have recommended that the concrete type of six inches thickness be specified for the streets to be paved under the present plans.

"The streets recommended for immediate paving were,

Kleberg avenue from the court house to H. M. King school,

Third street from Kleberg to Santa Gertrudis avenue,

Santa Gertrudis from Third to the College,

Fifth from King to Yoakum;

Seventh from King to Yoakum.

Yoakum from Park avenue to 7th;

King from 6th to 7th.

"It is believed that this will provide for the business portion of the town where traffic most strongly demands paving, furnish a route to our High School, the C. H. Flato, Jr., school and the College and will furnish a base from which a complete plan of city paving can be extended as conditions and finances permit.

# "TEXAS HATMAKERS" PLAY "FIREMEN" ARMISTICE DAY

About the only celebration to be staged in Kingsville on Armistice day will be the football game between the Firemen of this city and the "Texas Hatmakers" of Laredo, and Manager Stuth of the Firemen says that this is going to be the best game of ball that has been played on a local field this season. The Firemen, since their tilt with Weslaco, have strengthened up the weak places in their fences and have been practicing hard. The Laredo bunch have played a number of games with independent teams over the state and have the reputation of being a hard-hitting team. The game will be played in the afternoon, beginning at 3:00 o'clock.

# BRAHMAS GO TO BROWNSVILLE FRIDAY

According to arrangements which have been made, the Brahmas will go to Brownsville next Friday for a game with the Brownsville high school team. This game will have nothing to do with the conference schedule and is being played simply as an exhibition game. The game at Robstown last Saturday put the Brahmas out of the race as far as conference games are concerned. It is understood that they will play the Corpus Christi Buccaneers at Corpus Christi on Armistice day, November 11th. The Brahmas showed plenty of fighting spirit at Robstown and there is a good chance that they will beat both Brownsville and Corpus Christi.

# "BOB KITTENS" PLAY HERE SATURDAY 7

The Javelinas of South Texas State Teachers College fame will meet the "Bob-Kittens" of the San Marcos

172/15  
WHY JOHN HAS THE  
ANOTHER WAR DECLAI



S.

Miss Lora Patter  
The College, visit  
Pattville on Saturd

Miss Grace V. Lu  
College, who has b  
County Hospital fo  
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sufficiently recover  
her home in Corpus  
is resting for a few  
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welcome her return  
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Mr. W. H. Le C  
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"The much discussed subject of drainage was definitely settled by the report of Mr. Gentry. He states that storm sewers are necessary even with the limited amount of paving contemplated in this program and states that the cost of this item will be \$30,000.

"To this \$30,000 is to be added \$81,000 as the city's share of the paving. For taking care of minor extensions where property holders wish to bear their share of continuous extensions, the committee recommends that the bond issue be in the sum of \$125,000. This will mean that on our present valuations which total about 4 million dollars, we will levy a tax of 25c to create a sinking fund to retire this bond issue.

"The cost of the property owner's share of the contemplated improvements will aggregate \$85,000. Plans are being worked out whereby the cost to each individual can be distributed over a period of years at a reasonable rate of interest. To every owner of property abutting the route mentioned as coming within this program, an invitation to visit the city office and get the exact amount that will be assessable against you in the event the program is completed.

"A petition is being prepared, which will be circulated by the Secretary and Committee of the Commercial Club, calling upon the Mayor and Commis-

Teachers College at the local grounds next Saturday afternoon. The "Bob-Kittens" have a reputation of playing hard foot ball this season, while the Javelinas have been considerably strengthened since their last appearance. There will be several new players in uniform and rumor hath it that these new players have added greatly to the fighting strength of the college boys. Coach Smith says there is going to be a mighty good game next Saturday.

The schedule calls for a game with McAllen at McAllen Armistice day, and it is supposed that the schedule will be carried out.

## RUNDLES PURCHASE THE RIVIERA HOTEL

Mr. and Mrs. H. O. Rundle who, it will be remembered, for a number of years were proprietors of the hotel at Riviera Beach and later of the hotel at Riviera, but who for the past year have been residents of San Antonio, have returned to Riviera, having purchased the hotel there from Mr. Theo. F. Koch, the owner. Mrs. Rundle states that they expect to thoroughly overhaul the hotel, remodel it in some respects, and put it into first-class condition. Both Mr. and Mrs. Rundle are well known in this section and their many friends are glad to learn of their return to Kleberg county. They are splendid hotel people and the Riviera hotel will prove an attraction under the managership, no doubt.

## COTTON GINNINGS TOTAL 3554 BALES

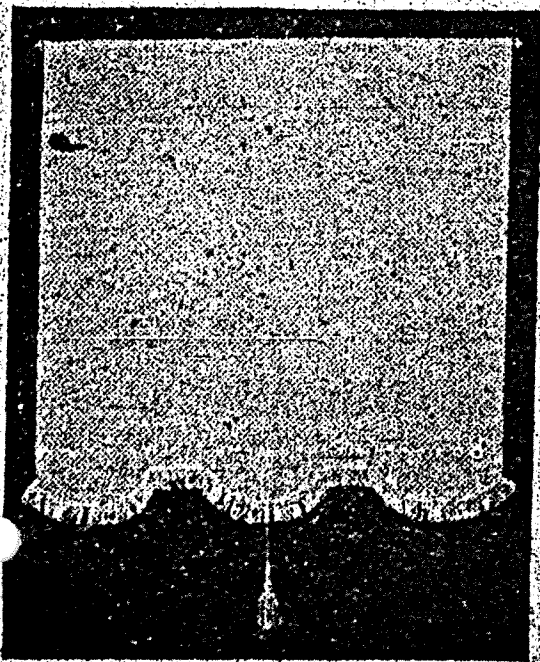
With the incessant rains and abundant feed crops now ready for curing, along with a great deal of fall plowing being done preparatory for another crop, it is interesting to know that cotton is still being ginned in the county when the 1925 crop is supposed to be history and the 1926 output in anticipation, or speculation. Most gins in the county have probably shut down for the season. At least the Record has heard nothing to the contrary, ex-

taken Yeager wound pressed ed to berg C has sufficiently rec his text-book duties has not entirely he

Following a cont the Annual Staff ar name "El Rancho" as one entirely suit which the College w Miss Vaugh Buch Falls, announced a bly that the name submitted by Jai Bishop, and that th annual would be giv a prize. Both the stu faculty feel that th most appropriate, g delightful bit of loc book and the instit

Miss McCloud, Di the College, will go giving, where she w Classical Section of ers Association. Mis First Vice Presiden Language Associati and Middle West, t which is a member Faculty of the Univ

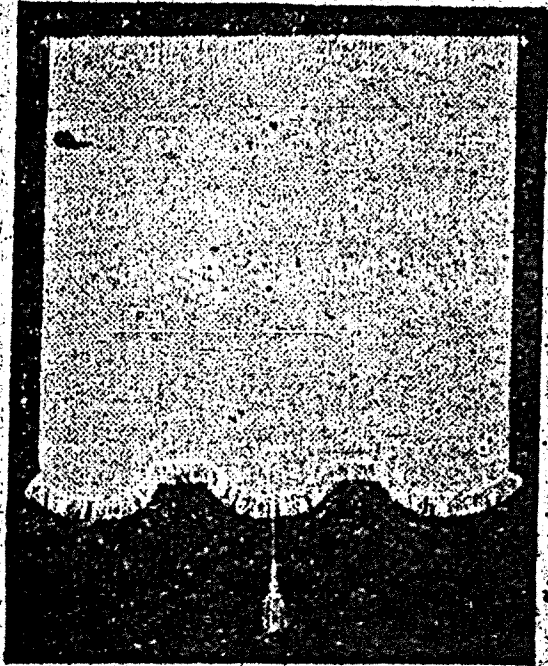
On Thursday ever the student body a college were deligh in the College Au Woman's Club of th rium was beautiful decorations. Festoor gold were hung fro from the stage, an cosmos were used stage. A large pot-of ed near the stage on es served delicious the evening. In on room was a witch's curious person was a surprise or shock th come. A radio progr alley were two delig the evening's entert culty and student bo opportunities of know the city, and appreci courtesy extended by Club of the city.



**Brenlin "AVONDALE"**

mentioned as coming within this program, an invitation to visit the city, office and get the exact amount that will be assessable against you in the event the program is completed.

"A petition is being prepared, which will be circulated by the Secretary and Committee of the Commercial Club, calling upon the Mayor and Commis-



## Brenlin "AVONDALE"

the long wearing window shade material

The durability and beauty of the Brenlin window shade combined with the artistic lines of scallop and fringe, assure the last touch of distinctive charm and attractiveness to the home.

We invite your inspection.

**Allen FURNITURE CO.**  
125 S. 10th St. - Dallas, Texas

at Riviera, but who for the past year have been residents of San Antonio, have returned to Riviera, having purchased the hotel there from Mr. Theo. F. Koch, the owner. Mrs. Rundle states that they expect to thoroughly overhaul the hotel, remodel it in some respects, and put it into first-class condition. Both Mr. and Mrs. Rundle are well known in this section and their many friends are glad to learn of their return to Kleberg county. They are splendid hotel people and the Riviera hotel will prove an attraction under the managership, no doubt.

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Ginnings in the county to date stand at:

Selman Gin; Ricardo	1671
L. C. Newton Gin, Kingsville	858
Farmers Gin, Kingsville	741
King Gin, Riviera	284

sioners to order an election on the question of the issuance of bonds. The sentiment expressed by all classes of citizens, tax payers big and little, indicates that the bond issue election will carry by a heavy majority.

Classical Sectioners Association. First Vice Pres Language Assoc and Middle We which is a mer Faculty of the

On Thursday the student body college were in the College Woman's Club c rium was beaut decorations. Fe gold were hung from the stage cosmos were u stage. A large p ed near the stag es served delict the evening. I room was a wit curious person v surprise or shoe come. A radio p alley were two the evening's e culity and studen opportunities of the city, and app courtesy. extende Club of the city.

Dr. Ainsworth, lan church of the er at assembly helpful message- ous ministers of enjoyed by the s

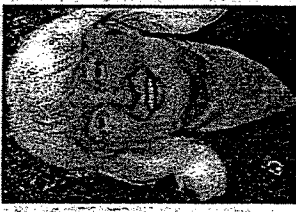
Because of the ing plant at the during the recent es were not held 10:00 o'clock Fri been made now, comfortably heat

There has just composers, the fir

5/4/12

# Paving the way to a better Kingsville

Generally, street paving isn't the most exciting topic around unless you are talking about paving Kleberg Avenue. Many folks noticed the surveys out taking measurements along Kleberg Avenue a few weeks back. Those were TAMUK students trying out their skills while working on a project that will be used to help Kingsville figure out what is the most beneficial thing to do. It is a very preliminary study.



**Pat Allison**

Guest Columnist

When Kingsville was born, the streets of the future downtown were surveyed a n d grubbed out of the mesquite brush. It wasn't until 1915 that people got serious about street paving. Imagine for a moment that every time the wind blew, a cloud of dust rose up from the street. Imagine again that every time there was a significant rain the street would turn to mushy mud. The city issued contracts for sprinkling water on the dry streets to keep the dust down.

Tarvia, a type of road surface under the surface needs to be updated. Most important in the effort is to plan how to accomplish the need-ed upgrade of Kleberg Avenue with the absolute least impact on businesses and customers. The city engineer and the dean of engineering at TAMUK have a number of interesting community projects that give students some hands-on experience in their fields of interest and benefit Kingsville at the same time.

There is more paving history in Kingsville that can be investigated. There is colorful, local folklore that can brighten our community. I only wish the pavement could talk to us about our history. What a story that would be!

Pat Allison is a board member for the Kleberg County Heritage Foundation and a member of the Kleberg County Historical Commission.

“ There is colorful, local folklore that can brighten our community. I only wish the pavement could talk to us about our history.”

— Pat Allison

By the 1925 election cycle, “good roads” became a political mantra from the federal level down to the cities and towns. People were demanding paved roads. The popularity of the automobile required it. An article in the Kingsville Record in 1925 declared the efforts to control the dust from unpaved roads to be unsuccessful. In November 1925, a committee was formed to plan for paving the town and installing modern storm sewers. The new pavement would be paid for through a bond election and a scheme where the city paid one-third of the cost of the road. The people and businesses on either side of the roadway would pay the balance of the cost. The bond election was set for Jan. 12, 1926. However, the city fathers were so sure that the bond election would pass that they signed a contract for \$182,000 with W. L. Pearson & Company out of Houston to get the job started 15 days after the passage of the bond election and to be completed 125 days after that.

The Jan. 13, 1926, issue of the Kingsville Record reported that the bond issue passed overwhelmingly, 345 in favor to 19 opposed. However, it

## KINGSVILLE RECORD AND BISHOP NEWS

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# 1.) Definitions

Page

- Definitions, terminology and information 1-1
- Failure descriptions 1-3

# City of Kingsville Definitions

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## Definitions, terminology and information

### **ASPHALT STREETS**

Included in this manual is a map “Asphalt Street Rating by Condition” which assigns a condition rating of “Good”, “Fair” or “Poor” to all City streets. These ratings have an associated “Pavement Condition Index” (PCI) value.

Good, 100 – 76

Fair, 75 – 51

Poor, 50 – 0

City staff calculated these condition ratings after field study counting and observing streets conditions for every street in Kingsville. Separate condition ratings (non PCI) were conducted for the City’s concrete streets, curbs and gutters, and sidewalks. Attached are sheets describing “Good”, “Fair” and “Poor” asphalt streets, concrete streets and curb and gutter.

### **CONCRETE STREETS**

Included in this manual is a map “Concrete Street Rating by Condition” which assigns a condition rating of “Good”, “Fair” or “Poor” to all City concrete streets. City staff assigned these condition ratings after observing concrete street conditions in Kingsville. Unlike asphalt streets, concrete street PCI is different as the life expectancy of a concrete street is over 50 years. The “Good”, “Fair” and “Poor” ratings for the City’s concrete streets are described in this manual.

### **CURB AND GUTTER**

Included in this manual is a map “Curb and Gutter Rating by Condition” which assigns a condition rating of “Good”, “Fair” or “Poor” to all City curbs and gutters. City staff assigned these condition ratings after and observing curb and gutter conditions for every curb and gutter in Kingsville. The “Good”, “Fair” and “Poor” ratings for the City’s curbs and gutters are defined in this manual. It is important to note that where there is a need to replace a curb as development grows, or where a street is reconstructed, 6” curb is being installed and not square or rollover curb.

**Pavement Condition Index (PCI)** – pavement scores given to an asphalt street based on the existing surface distressed condition.

**Full Depth Construction (FDC)** – The construction of an asphalt street that includes, subgrade, base material (limestone), seal coat and Hot Mixed Asphalt Concrete (HMAC).

**Overlay** – Top 2” surface of HMAC application of a street.

**Rejuvenator** – A chemical application applied to the surface of asphalt that will rejuvenate the top 2” of the surface of the street (used as a maintenance/street life extender).

# City of Kingsville Definitions

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**Graphic Information Systems (GIS)** – Computer program used as a data base to compute street life, scheduling, street characteristics, history and cost.

**Fog and Crack seal** – The maintenance method where a chemical application (much like the rejuvenator) is applied to the surface of the street for the purpose of sealing cracks, extending the life of the street. Cracks are sealed with rubber material.

## ALLIGATOR OR FATIGUE CRACKING (1)

### Description

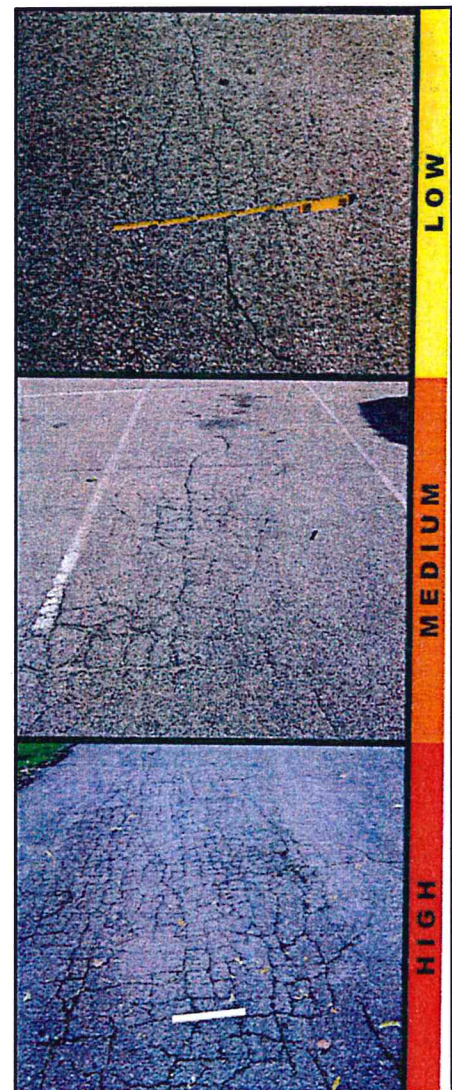
Alligator or fatigue cracking is a series of interconnection cracks caused by fatigue failure of the asphalt concrete surface under repeated traffic loading. Cracking begins at the bottom of the asphalt surface (or stabilized base) where tensile stress and strain are highest under a wheel load. The cracks propagate to the surface initially as a series of parallel longitudinal cracks. After repeated traffic loading, the cracks connect, forming many sided, sharp-angled pieces that develop a pattern resembling chicken wire of the skin of an alligator. The pieces are generally less than 1 1/2 ft. (0.5m) on the longest side. Alligator cracking occurs only in areas subjected to repeated traffic loading, such as wheel paths. (Pattern-type cracking that occurs over an entire area not subjected to loading is called "block cracking," which is not a load-associated distress.)

### Severity Levels

- L** Fine, longitudinal hairline cracks running parallel to each other no, or only a few interconnecting cracks. The cracks are not spalled.
- M** Further development of light alligator cracks into a pattern of network of cracks that may be lightly spalled.
- H** Network of pattern cracking has progressed so that the pieces are well defined and spalled at the edges. Some of the pieces may rock under traffic.

### How to Measure

Alligator cracking is measured in square feet (square meters) of surface area. The major difficulty in measuring this type of distress is that two or three levels of severity exist within one distressed area. If these portions can be easily distinguished from each other; they should be measured and recorded separately. However, if the different levels of severity cannot be divided easily, the entire area should be rated at the highest severity present. If alligator cracking and rutting occur in the same area, each is recorded separately at its respective severity level.



## EDGE CRACKING (7)

### Description

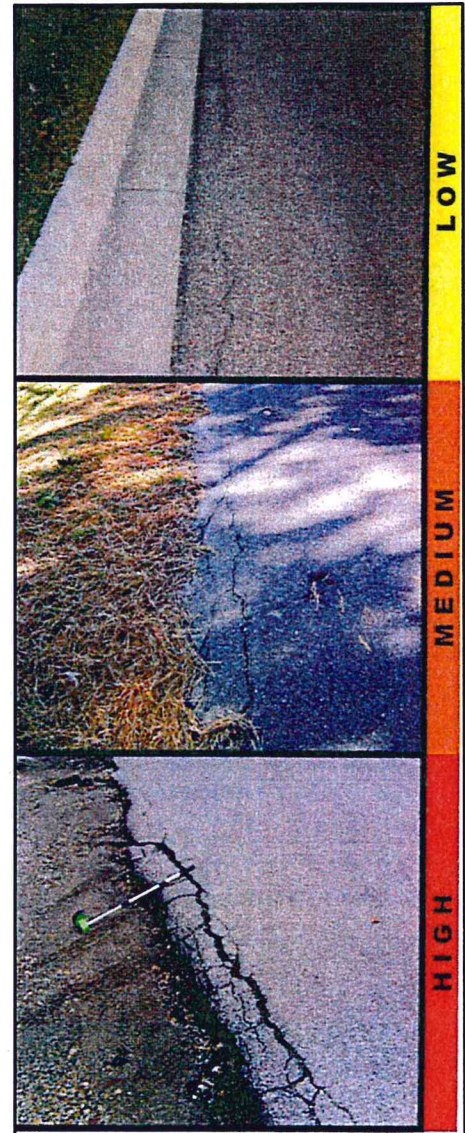
Edge cracks are parallel to and usually within 1 to 1 ½ feet (0.3 to 0.5 m) of the outer edge of the pavement. This distress is accelerated by traffic loading and can be caused by frost-weakened base of subgrade near the edge of the pavement. The area between the crack and pavement edge is classified as raveled if it is broke up (sometimes to the extent that pieces are removed).

### Severity Levels

- L** Low or medium cracking with no breakup or raveling.
- M** Medium cracks with some breakup and raveling.
- H** Considerable breakup of raveling along the edge.

### How To Measure

Edge cracking is measure in linear feet (linear meter)





## JOINT REFLECTION CRACKING (8) (FROM LONGITUDINAL AND TRANSVERSE PCC SLABS)

### Description

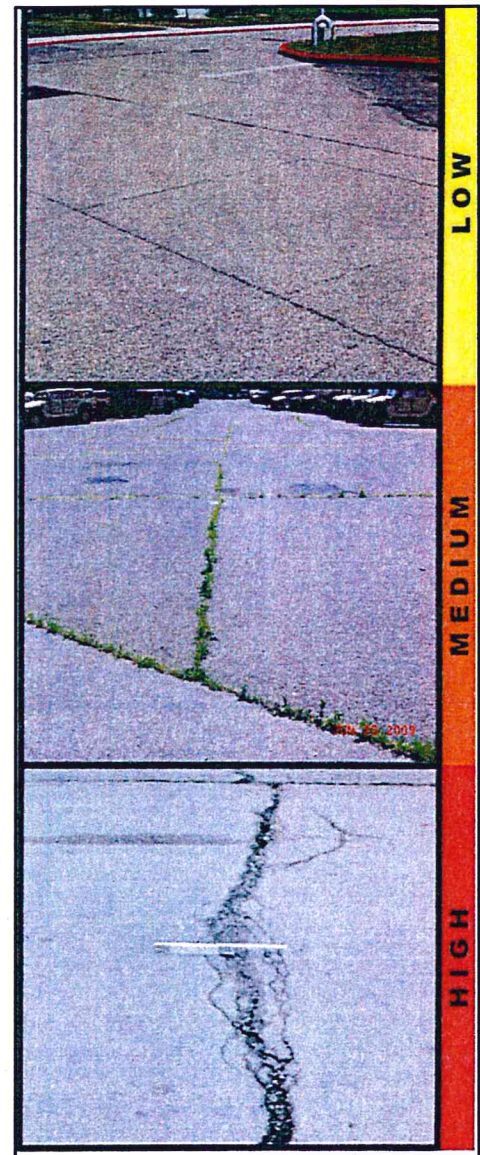
This distress occurs only on asphalt-surfaced pavements that have been laid over a PCC slab. It does not include reflection cracks from any other type of base (i.e. cement-or lime –stabilized); these cracks are caused mainly by thermal or moisture induced movement of the PCC slab beneath the AC surface. This distress is not load related; however, traffic loading may cause a breakdown of the AC surface near the crack. If the pavement is fragmented along a crack the crack is said to be spalled. A knowledge of slab dimension beneath the AC surface will help to identify these distresses.

### Severity Levels

- L** One of the following conditions exists: (1) Non-filled crack width is less than 3/8 in. (10mm), or (2) filled crack of any width (filler in satisfactory condition.)
- M** One of the following conditions exists: (1) Non-filled crack width is less than 3/8 in. (10mm) and less than 3 in. (75mm); (2) non-filled crack less than or equal to 3 in. (75 mm) surrounded by light secondary cracking or (3) filled crack of any width surrounded by light secondary cracking.
- H** One of the following conditions exists: (1) Any crack filled or non-filled surrounded by medium or high severity secondary cracking; (2) non-filled cracks greater than 3 in. (75 mm), or (3) A crack of any width where approximately 4 in. (100 mm) of pavement around the crack are severely raveled or broken.

### How To Measure

Joint reflection cracking is measured in linear feet (linear meters). The length and severity level of each crack should be identified and recorded separately. For example, a crack that is 50 feet (15m) long may have 10 feet (3m) of high severity cracks; which are all recorded separately. If a bump occurs at the reflection crack it is also recorded.



## LONGITUDINAL AND TRANSVERSE CRACKING (10) (NON-PCC SLAB JOINT REFLECTIVE)

### Description

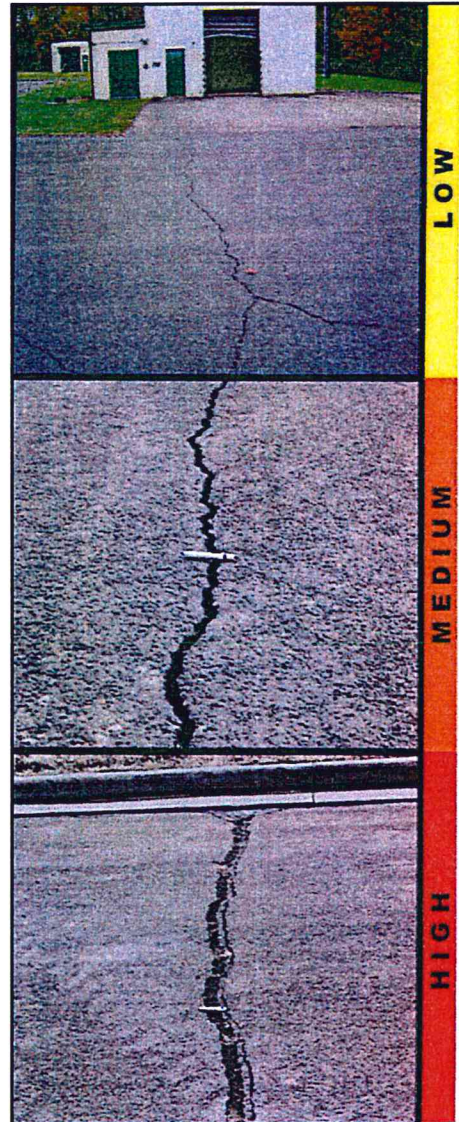
Longitudinal cracks are parallel to the pavement's centerline of laydown direction. They may be caused by;

1. A poorly constructed paving lane joint.
2. Shrinkage of the AC surface due to low temperatures of hardening of the asphalt and/or daily temperature cycling.
3. A reflective crack caused by cracking beneath the surface course, including cracks in PCC slabs (but not PCC joints).

Transverse cracks extend across the pavement at approximately right angles to the pavement centerline or direction of laydown. These types of cracks are not usually load-associated.

### Severity Levels

- L** One of the following conditions exists: (1) non-filled crack width is less than 3/8 in. (10 mm), or (2) Filled crack of any width (filler in satisfactory condition).
- M** One of the following conditions exists: (1) non-filled crack width is greater than or equal to 3/8 in. (10 mm) and less than 3 in. (75mm); (2) non-filled crack is less than or equal to 3 in. (75mm) surrounded by light and random cracking or (3) filled crack is of any width surrounded by light random cracking.
- H** One of the following conditions exists: (1) any crack filled or non-filled surrounded by medium or high severity random cracking; (2) non-filled crack greater than 3 in. (75mm), or (3) a crack of any width where approximately 4 in. (100 mm) of pavement around the crack is severely broken.



### How To Measure

Longitudinal and transverse cracks are measured in linear feet (linear meters). The length and severity of the each crack should be recorded. If the crack does not have the same severity level along its entire length, each portion of the crack having a different severity level should be recorded separately.

## PATCHING AND UTILITY CUT PATCHING (11)

### Description

A patch is an area of pavement that has been replaced with new material to repair the existing pavement. A patch is considered a defect not matter how well it is performing (a patched area or adjacent area usually does not perform as well as an original pavement section). Generally, some roughness is associated with this distress.

### Severity Levels

- L** Patch is in good condition and satisfactory. Ride quality is rates as low severity or better
- M** Patch is moderately deteriorated and/or ride quality is rated as medium severity.
- H** Patch is badly deteriorated and /or ride quality is rated as high severity. Needs replacement soon.

### How to Measure

Patching is rated in square feet (square meters) of surface area. However, if a single patch has areas of differing severity, these areas should be measured and recorded separately. For example, a  $27\text{ft}^2$  ( $2\frac{1}{2}\text{m}^2$ ) patch may have  $11\text{ft}^2$  ( $1\text{m}^2$ ) of medium severity and  $16\text{ft}^2$  ( $1\frac{1}{2}\text{m}^2$ ) of low severity. These areas would be recorded separately. Any distress found in a patched area will not be recorded; however, its effect on the patch will be considered when determining the patch's severity level. No other distresses (e.g., shoving and cracking) are recorded with a patch; even if the patch material is shoving or cracking, the area is rated only as a patch. If a large amount of pavement has been replaced, it should be recorded as a patch but considered as new pavement (e.g., replacement of complete intersection).

# POTHOLES

## Description

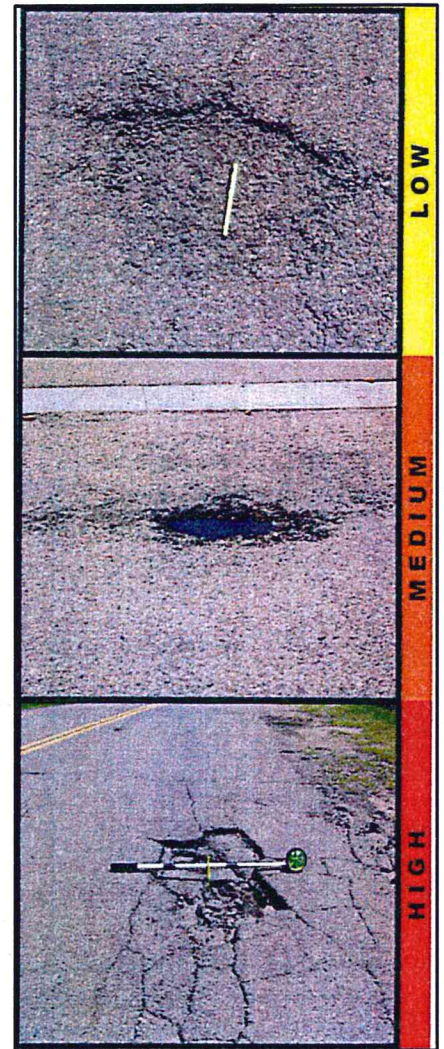
Potholes are small, usually less than 30 in. (760 mm) in diameter, bowl shaped depressions in the pavement surface. They generally have sharp edges and vertical sides near the top of the hole. When holes are created by high severity alligator cracking, they should be identified as potholes, not as weathering.

## Severity Levels

The levels of severity for potholes less than 30 in. (760 mm) in diameter are based on both the diameter and the depth of the pothole, according to Table 1

Table 1. Levels of Severity for Potholes

Maximum Depth of Pothole (in.) (mm)	4 to 8 in. (100 to 200 mm)	8 to 18 in. (200 to 460 mm)	18 to 30 in. (460 to 760 mm)
½ to ≤ 1 in. (13 to 25 mm)	L	L	M
>1 to ≥ 2 in. (25 to 50 mm)	L	M	H
>2 in. (50 mm)	M	M	H



If the pothole is more than 30 in. (760 mm) in diameter, the area should be determined in square feet and divided by  $5 \text{ ft}^2$  ( $0.5 \text{ m}^2$ ) to find the equivalent number of holes. If the depth is 1 in. (25 mm) or less, the holes are considered medium severity. If the depth is more than 1 in. (25 mm), they are considered high severity.

## HOW TO MEASURE

Potholes are measured by counting the number that are low, medium, and high severity, and recording them separately.

## RUTTING

### Description

A rut is a surface depression in the wheel paths. Pavement uplift may occur along the sides of the rut, but in many instances, ruts are noticeable only after rainfall when the paths are filled with water. Rutting stems from a permanent deformation in any of the pavement layers of subgrades, usually caused by consolidated or lateral movement of the materials due to the traffic load.

Severity Levels: Mean Rut Depth

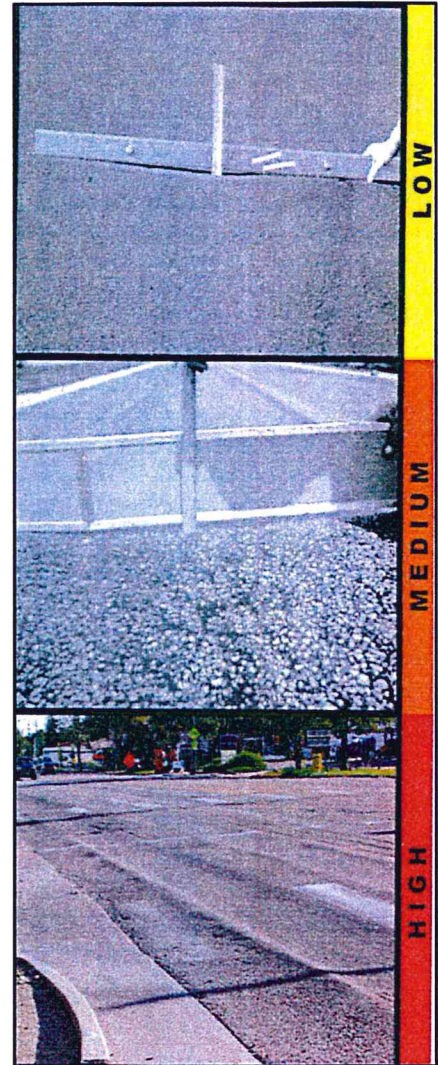
**L** ¼ to ½ inch (6 to 13 mm)

**M** > ½ to 1 inch (13 to 25 mm)

**H** > 1 inch (25 mm)

### How To Measure

Rutting is measured in square feet (square meters) of surface area and its severity is determined by the mean depth of the rut (see above). The mean rut depth is calculated by laying a straight edge across the rut, measuring its depth, and then using measurements taken along the length of the rut to compute its mean depth in inches (mm).



## 2.) Informed Assumptions

page

1. Assumptions

2-1

2. Factors Contributing to Current Conditions 2-2

# City of Kingsville Assumptions

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## Calculated and Informed Assumptions

**Asphalt Street Construction Rate of production (Poor to Good)** – City crews have the ability to repair and replace 40 blocks (3 miles) of poor streets a year.

**Street Drop off rate** – The rate in which streets are dropping from fair rated to poor rated (26 blocks, or 2 miles a year).

**Street Life (Remaining Life)** – This is a calculated assumption neglecting traffic volume, bus stops and traffic signal timing. The calculated assumed asphalt street life in Kingsville, Texas is 20 years (with maintenance activities).

**The use of Overlay vs Full Depth Construction (FDC)** – Upon physical inspection (after initial rating), boring samples and type of distress, the base material may be good for overlay only.

**Maintenance activities** – Maintenance activities are administered to streets with a PCI score (upper fair rating) of 65 and above. This ensures that the street life is extended efficiently.

**Maintenance activity (rejuvenator)** – This maintenance activity is administered after the first five years of a newly overlaid or FDC street.

**Maintenance activity (Fog and Crack seal)** – This maintenance activity is administered after ten (10) and fifteen (15) years of when a street is overlaid or FDC.

**Standard Block Length** – The standard assumed block length in Kingsville, Texas is **400 feet**. However, recognizing that not every block is the same (length and width), the Kingsville's GIS system is accurate with lengths and widths.

**Curb and Gutter** – It is assumed that both sides of a "curb and guttered" street are being repaired (and is rated as such). However, at the time of construction, repair and replacement lengths may be shortened (Alexander street only one side needed replacement).

# City of Kingsville

## Factors Contributing to Asphalt Street Failure

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	Estimated percentage <u>Of occurrence</u>
1. <b>Water infiltration</b> – Water seeps into the roadway through cracks That causes base failures.	45%
2. <b>Passenger traffic Loads</b> – Moderate weight causing minor but more distress. (In Kingsville, Texas, passenger traffic loads are negligible).	5%
3. <b>Commercial Traffic Loads</b> – Heavy weight causing severe distress.	15%
4. <b>Traffic Volumes</b> - This is when frequent traffic causes repetition Distress (In Kingsville, Texas, traffic volumes are negligible)	5%
5. <b>Vehicle turning movements</b> – Causes distress at intersections, driveways and back up motions.	5%
6. <b>Corrosion/Oxidation</b> – This can be done by Ultra Violet (UV) exposure. Chemical decomposition is also created. Several vehicle chemicals and solvents react chemically when contact Is made with the asphalt.	5%
7. <b>Utility Repairs</b> – Street cuts are made to the roadway compromising The uniformity and integrity of the roadway. This results in roadway Failure over a period of time.	5%
8. <b>Temperature Changes</b> – Extreme changes is temperature causes soil and materials to expand and contract. This causes cracking in the surface (as well as in the base material).	4%
9. <b>Weather Conditions</b> – The result of ice on the roadway causes the asphalt to become brittle and crack. Salt treatment affects the chemical compound of the asphalt causing decomposition. As in water infiltration, the integrity of the base material is affected.	1%
10. <b>Lack of Scheduled maintenance</b> – This causes the roadway to be short lived.	<u>10%</u> 100%



# City of Kingsville

## Factors Contributing to Concrete Street Failure

---

	Estimated percentage <u>Of occurrence</u>
1. <b>Water infiltration</b> – Water seeps into the roadway through cracks That causes base failures. This causes reinforcing steel to corrode.	50%
2. <b>Passenger traffic Loads</b> – Moderate weight causing minor but more distress.	5%
3. <b>Commercial Traffic Loads</b> – Heavy weight causing severe distress.	20%
4. <b>Traffic Volumes</b> - This is when frequent traffic causes repetition distress.	5%
5. <b>Vehicle turning movements</b> – Causes distress at intersections, driveways and back up motions.	1%
6. <b>Corrosion/Oxidation</b> – This can be done by Ultra Violet (UV) exposure. Chemical decomposition is also created. Several vehicle chemicals and solvents react chemically when contact is made with the reinforcing steel in concrete.	1%
7. <b>Utility Repairs</b> – Street cuts are made to the roadway compromising The uniformity and integrity of the roadway. This results in roadway Failure over a period of time.	1%
8. <b>Temperature Changes</b> – Extreme changes in temperature causes soil and materials to expand and contract. This causes the street to buckle.	6%
9. <b>Weather Conditions</b> – Salt treatment affects the chemical compound of the reinforcing steel to decompose and corrode.	10%
10. <b>Lack of Scheduled maintenance</b> – This causes the roadway to be short lived.	<u>1%</u> 100%

# City of Kingsville

## Factors Contributing to Curb and Gutter Failure

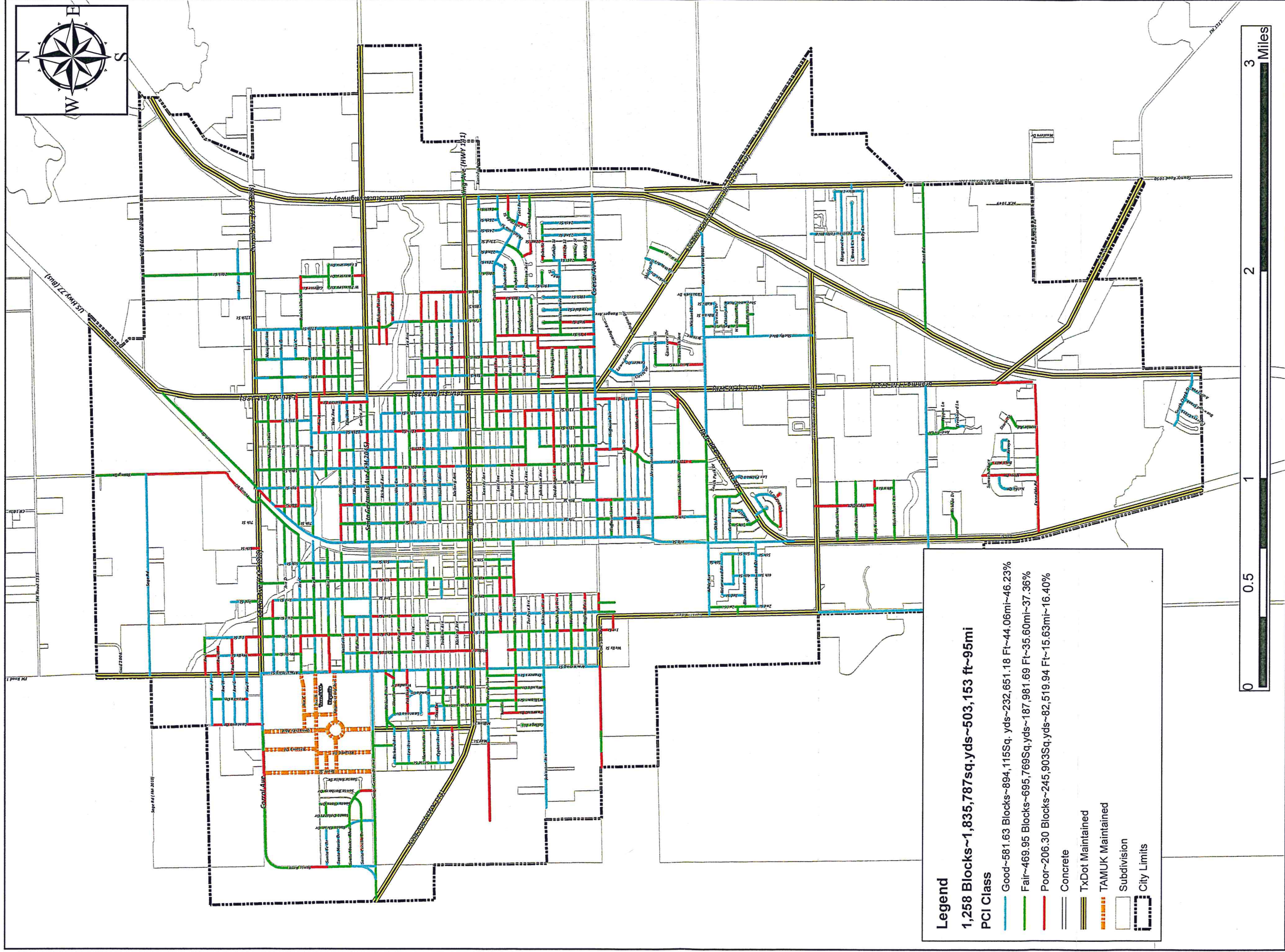
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	<u>Estimated percentage Of occurrence</u>
1. <b>Water infiltration</b> – Water seeps into the roadway through cracks That causes base failures. This causes reinforcing steel to corrode. This also encourages weeds to grow and expand.	45%
2. <b>Passenger traffic Loads</b> – Moderate weight causing minor but more distress. (riding up on the curb)	5%
3. <b>Commercial Traffic Loads</b> – Heavy weight causing severe distress. (riding up on the curb during deliveries)	5%
4. <b>Traffic Volumes</b> - This is when frequent traffic causes repetition distress.	5%
5. <b>Vehicle turning movements</b> – Causes distress at intersections, driveways and back up motions.	5%
6. <b>Corrosion/Oxidation</b> – This can be done by Ultra Violet (UV) exposure. Chemical decomposition is also created. Several vehicle chemicals and solvents react chemically when contact is made with the reinforcing steel in concrete.	3%
7. <b>Utility Repairs</b> – Street cuts are made to the roadway compromising the uniformity and integrity of the roadway. This results in the curb to Fail over a period of time.	20%
8. <b>Temperature Changes</b> – Extreme changes in temperature causes soil and materials to expand and contract. This causes the curb to buckle.	10%
9. <b>Weather Conditions</b> – Salt treatment affects the chemical compound of the reinforcing steel to decompose and corrode.	1%
10. <b>Lack of Scheduled maintenance</b> – This causes the curb to be short lived.	<u>1%</u> 100%

### 3.) Street, Curb & Gutter Inventories, GIS Pavement Model (data elements)

	<u>page</u>
1. Asphalt Rating by Condition	3-1
2. Concrete Street Rating by Condition	3-5
3. Curb and Gutter Rating by Condition	3-10
4. Reduction of Poor Streets and Increase of Good and Fair Streets.	3-14

# Asphalt Street Rating By Condition



Drawn By: Engineering Department

Last Update: 12/14/2014

Note:

3-1

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# City of Kingsville Asphalt Street Rating by Condition

---

## **Good, (PCI, 100 – 76) 46.3%**

Condition - Streets are in sound structural condition. The pavement surface has a smooth ride quality. The pavement will need several scheduled maintenance activities to extend the service life of the street. Characteristics of a good street are:

1. No alligator cracking
2. No potholes
3. Few longitudinal cracks joints.
4. Occasional transverse cracks.
5. Limited street rutting



**Good- Asphalt**

# City of Kingsville

## Asphalt Street Rating by Condition

---

### **Fair, (PCI, 75 – 51) 37.4%**

Condition - A street that is rated "Fair" is in good structural condition, but may, depending on its life and condition require seal coating, crack sealing, overlay, slurry seal, seal coat and/or other maintenance. "Fair" streets require patching and may have a loss of aggregate along the edges due mainly to normal storm drainage and vehicle wear. Inadequate storm water drainage accelerates the degradation of asphalt streets even more so. Some intersections may require reconstruction.

1. Longitudinal and transverse cracks present.
2. Block cracking present.
3. Patching in fair condition.
4. Alligator cracking present.
5. Pothole patching required.
6. Pavement edge needs repair.



Fair - Asphalt

# City of Kingsville Asphalt Street Rating by Condition

---

## **Poor (PCI, 50 – 0) 16.4%**

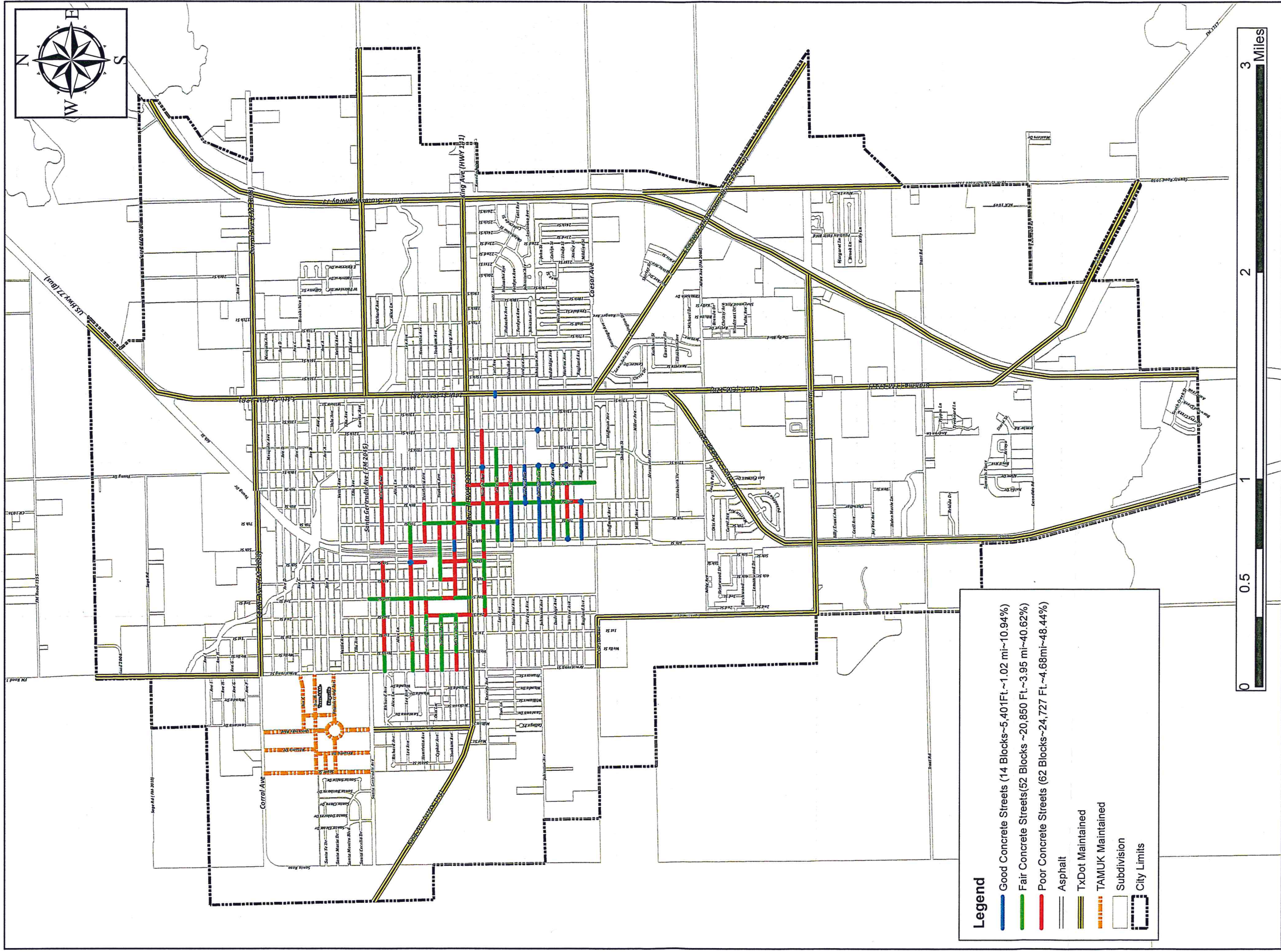
Condition – A street that is rated “Poor” is severely deteriorated and in need reconstruction. Severe distortion and extensive patches are in poor condition.

1. Severe distress with extensive loss of surface integrity.
2. Extensive patching in poor condition.



**Poor- Asphalt  
(12<sup>th</sup> Street)**

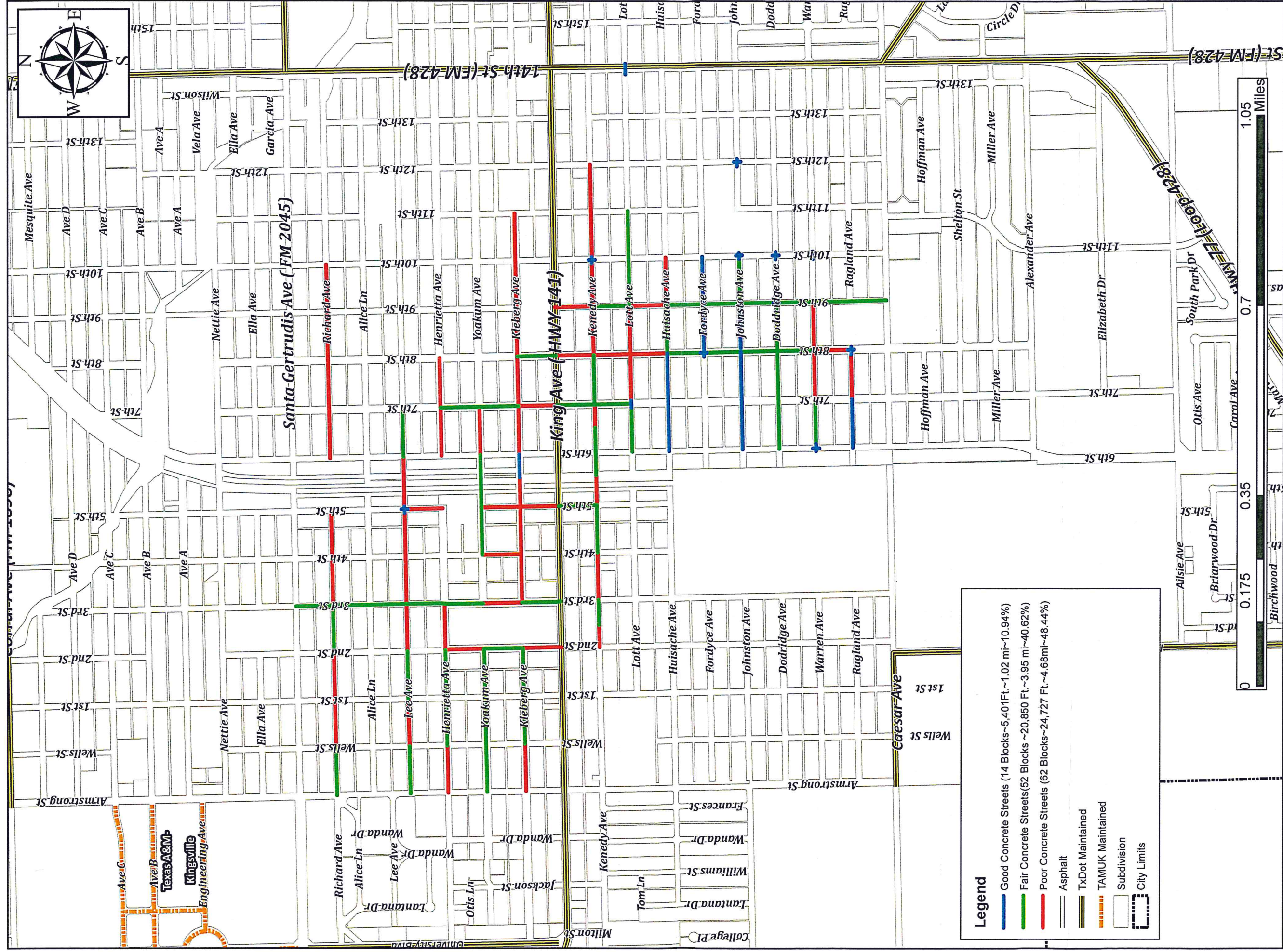
# Concrete Street Rating By Condition




<p>Drawn By: Engineering Department</p> <p>Last Update: 12/14/2014</p> <p>Note:</p>	<p>DISCLAIMER</p> <p>THIS MAP IS FOR VISUAL PURPOSES ONLY. THE INFORMATION ON THIS SHEET MAY CONTAIN INACCURACIES OR ERRORS. THE CITY OF KINGSVILLE IS NOT RESPONSIBLE IF THE INFORMATION CONTAINED HEREIN IS USED FOR ANY DESIGN, CONSTRUCTION, PLANNING, BUILDING, OR ANY OTHER PURPOSE.</p>		<p><b>CITY OF KINGSVILLE</b></p> <p><b>ENGINEERING DEPARTMENT</b></p> <p>200 E. Kleberg Kingsville, Texas 78363 Office: 361 595 8005 Fax: 361 595 8035</p>
			<p>3-5</p>



# Concrete Street Rating By Condition



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Drawn By: Engineering Department	Last Update: 12/14/2014
Note:	

# City of Kingsville

## Concrete Street Rating by Condition

---

### **Good, 10.9%**

Condition – A concrete streets that are in structural condition. The street has a smooth ride quality. Maintenance is limited.

1. No large broken concrete.
2. No patch work of asphalt on street.
3. Continuous smooth surface of concrete.



**Good- Concrete Street**

# City of Kingsville Concrete Street Rating by Condition

---

## **Fair, 40.6%**

Condition – A concrete street that is rated “Fair” may exhibit signs of slight reconstruction of surface, but is still in good structural condition.

1. Broken concrete.
2. Minimal patches of concrete and/or asphalt on street.
3. Continuous smooth surface of concrete.



**Fair- Concrete Street**

# City of Kingsville Concrete Street Rating by Condition

---

## **Poor, 48.4%**

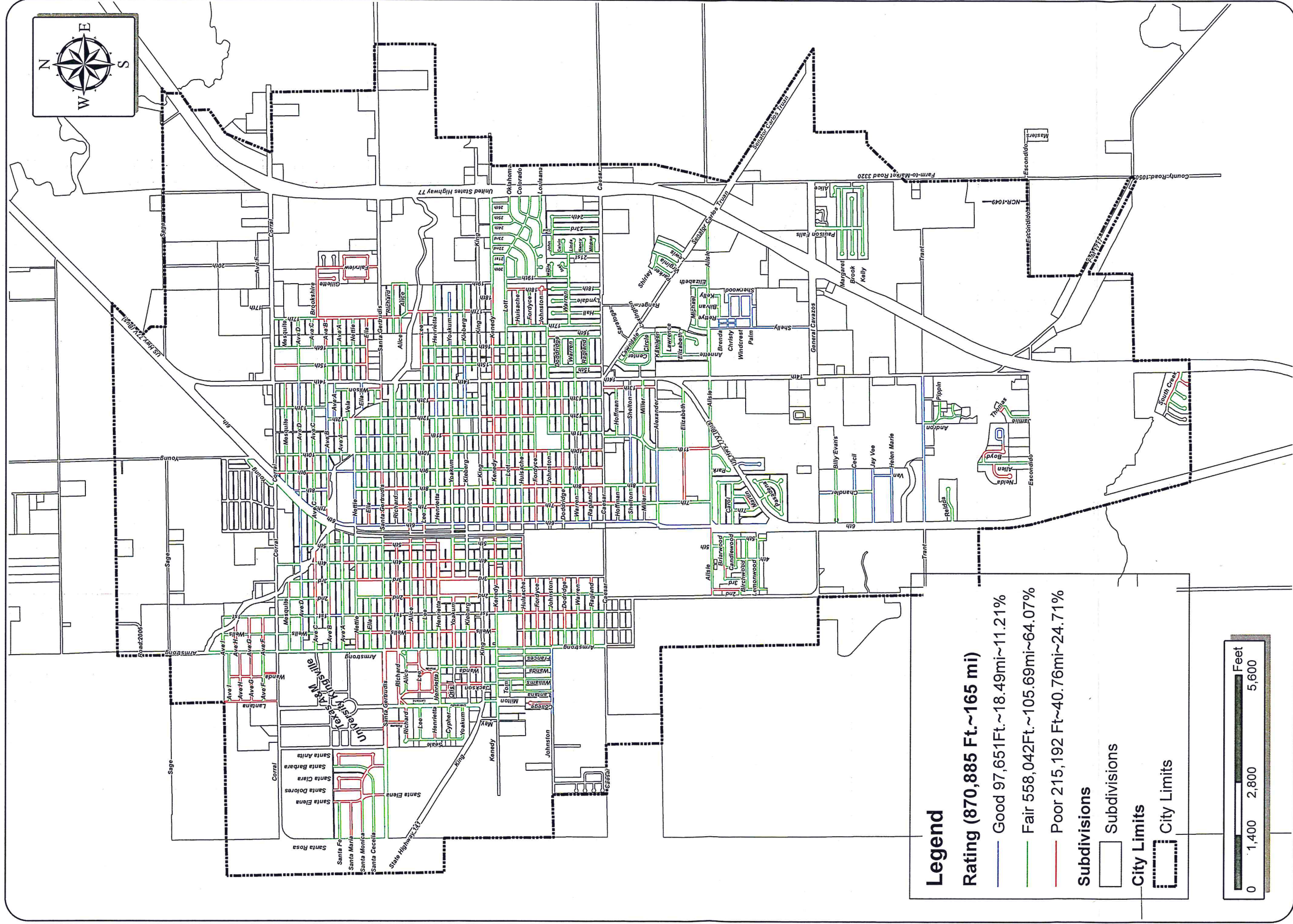
Condition – A concrete street that is rated “Poor” is severely in need of reconstruction.

1. Broken concrete in large radius areas.
2. Numerous patches of concrete and/or asphalt on street.
3. Rough surface with several cracks.



**Poor - Concrete Street**

# Curb and Gutter by Condition



## Legend

### Rating (870,885 Ft. ~165 mi)

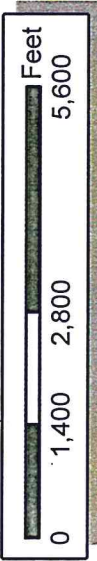
- Good 97,651 Ft. ~18.49 mi ~11.21%
- Fair 558,042 Ft. ~105.69 mi ~64.07%
- Poor 215,192 Ft. ~40.76 mi ~24.71%

### Subdivisions

□ Subdivisions

### City Limits

□ City Limits



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# City of Kingsville

## Curb and Gutter Rating by Condition

---

### **Good, 11.2%**

Condition – Curbs and gutters are in sound structural condition that optimizes storm water drainage capacity and function.

1. No major cracks.
2. No vegetation growing through joints.
3. No rebar showing.



**Good – Square Curb**



**Good- Rollover Curb**



**Good- 6" Curb**

# City of Kingsville Curb and Gutter Rating by Condition

---

## Fair, 64.1%

Condition – A curb and gutter that is rated “Fair” may exhibit signs of slight deterioration, but is still in good structural condition and is functional for the purpose of storm water drainage.

1. Slight cracks forming.
2. Small sections show signs of erosion.
3. Vegetation growing through joints.
4. Drainage issues are minimal.



Fair – Square Curb



Fair - Rollover Curb



Fair - 6" Curb

# City of Kingsville

## Curb and Gutter Rating by Condition

---

### **Poor, 24.7%**

Condition – A curb and gutter that is rated “Poor” is severely deteriorated and is in need of reconstruction. Severe distortion and extensive areas are in poor condition causing the curb and gutter to have limited storm water drainage capacity and function.

1. Severe distress with extensive loss of surface integrity.
2. Large cracks along surface with vegetation growing through at times.
3. Blocks of concrete missing from curb at various areas.
4. Rebar showing.
5. Drainage problems exist.



**Poor – Square Curb**



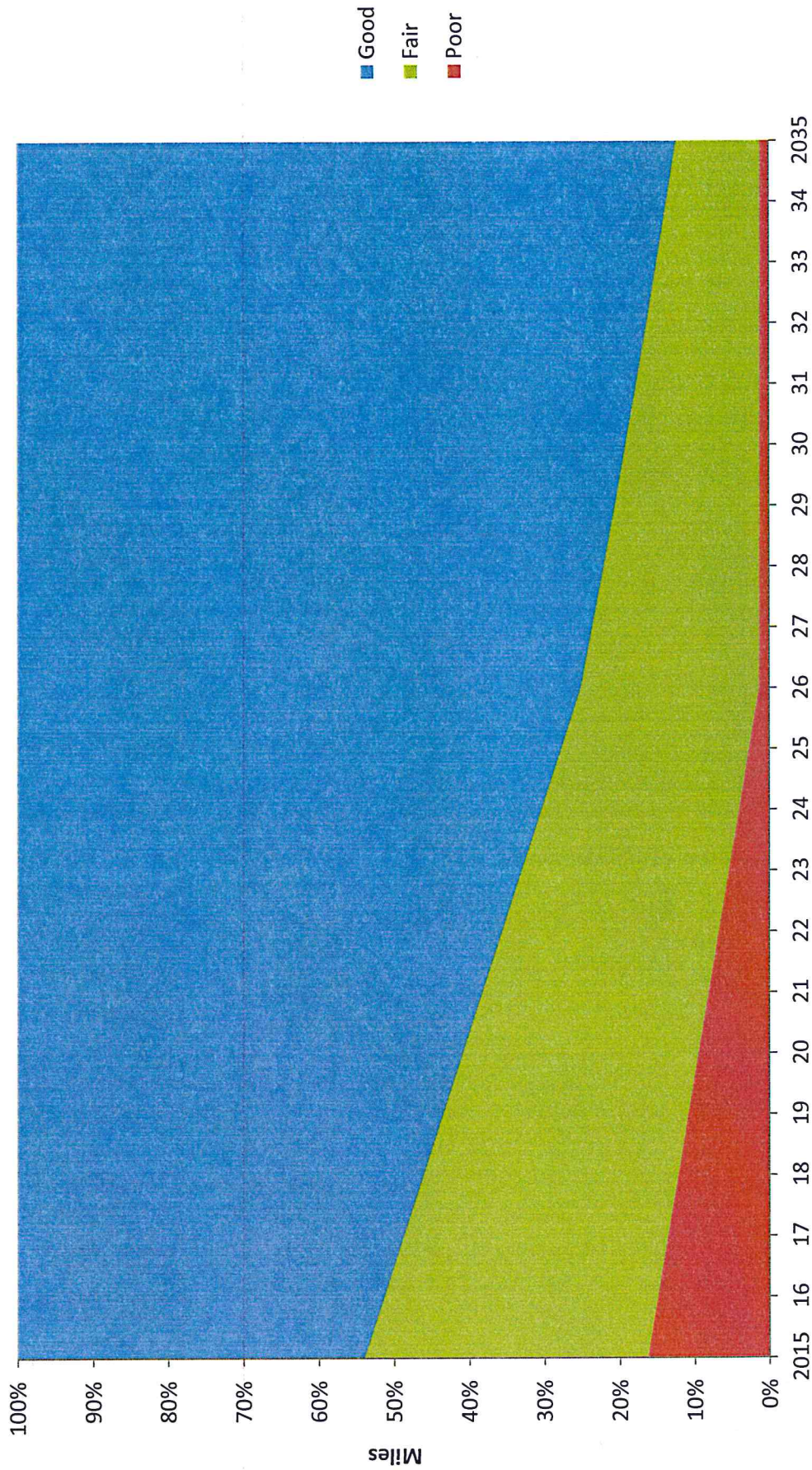
**Poor - Rollover Curb**



**Poor - 6" Curb**



# Asphalt Street Conditions Chart



## 4.) Data Elements

	<u>page</u>
1. Data Elements Key	4-1
2. Data Elements information	4-2

## Data Elements Key

1. Surface	27.2020
2. Zone	28.2021
3. Street Type	29.2022
4. Direction	30.2023
5. Block #	31.2024
6. Street Name	32.2025
7. From	33.2026
8. To	34.2027
9. PCI Class	35.2028
10. Status	36.2029
11. Activity	37.2030
12. Repair Year	38.2031
13. Length (Ft.)	39.2032
14. Length (Yds.)	40.2033
15. Width (Yds.)	41. Cost Overlay
16. Width (Ft.)	42. Cost Fog Seal
17. Square Yds.	43. Cost Rejuvenation
18. PCI	44. Cost Overlay Edge
19. Remaining Life	45. Cost FDC
20.2013	46. Cost Fog & Crack
21.2014	47. Maintenances
22.2015	48. MC 30
23.2016	49. AC 5
24.2017	50. Sealcoat
25.2018	51. Limestone
26.2019	52. Hot mix

### Legend

1. Street Identifiers
2. Conditions Rating
3. Past Street Activity
4. Future Street Activity
5. Cost Components

# City of Kingsville

## Data Elements Information

---

**Data Elements** – Every character entered into the GIS system used for mapping and in this case, also used for the creation of the pavement construction and maintenance model. Currently, there are 62,000 data elements entered in the GIS data base.

**GIS Data Element categories** – There are five (5) data element categories used in the GIS system:

1. Street identifiers – identify street characteristics (length width, type)
2. Condition ratings – ratings used for the bases of the pavement construction model
3. Past Street Activity – historical data used to update the pavement construction model.
4. Future Street Activity – Proposed street construction and maintenance.
5. Cost Components – Cost elements used to create yearly cost and derived from the GIS model.

## 5.) Construction and Maintenance Plan

	<u>page</u>
1. Construction and Maintenance examples	5-1
2. Street composition typical sections	5-7
3. Yearly Street Repairs	5-10
4. Yearly Street Repairs (seal coat)	5-1a

City of Kingsville  
Construction and maintenance methods

Full Depth Construction (FDC)

1. 6" Base
2. MC – 30 primer emulsion (seal of base material)
3. AC – 5 oil (tack for rock)
4. #4 grade gravel (for seal coat)
5. 2" of Hot Mixed Asphalt Concrete (HMAC)



# City of Kingsville Construction and maintenance methods

## Hot Mix Overlay

1. MC – 30 primer emulsion (seal of existing road base)
2. 2" of Hot Mixed Asphalt Concrete (HMAC)



City of Kingsville  
Construction and maintenance methods

Fog and Crack Seal

1. Bituminous asphalt oil (much like an AC – 5) used to cover/seal the street
2. Rubberized crack sealant to fill in lateral and longitudinal cracks.





City of Kingsville  
Construction and maintenance methods

Rejuvenator

1. A chemical compound used through a distributor to re-oxidize asphalt streets. Chemical reacts with top surface of the asphalt.



City of Kingsville  
Construction and maintenance methods

Concrete street

1. 6" base material (limestone)
2. 6" 3000 psi concrete
3. #4 rebar, 18" off center



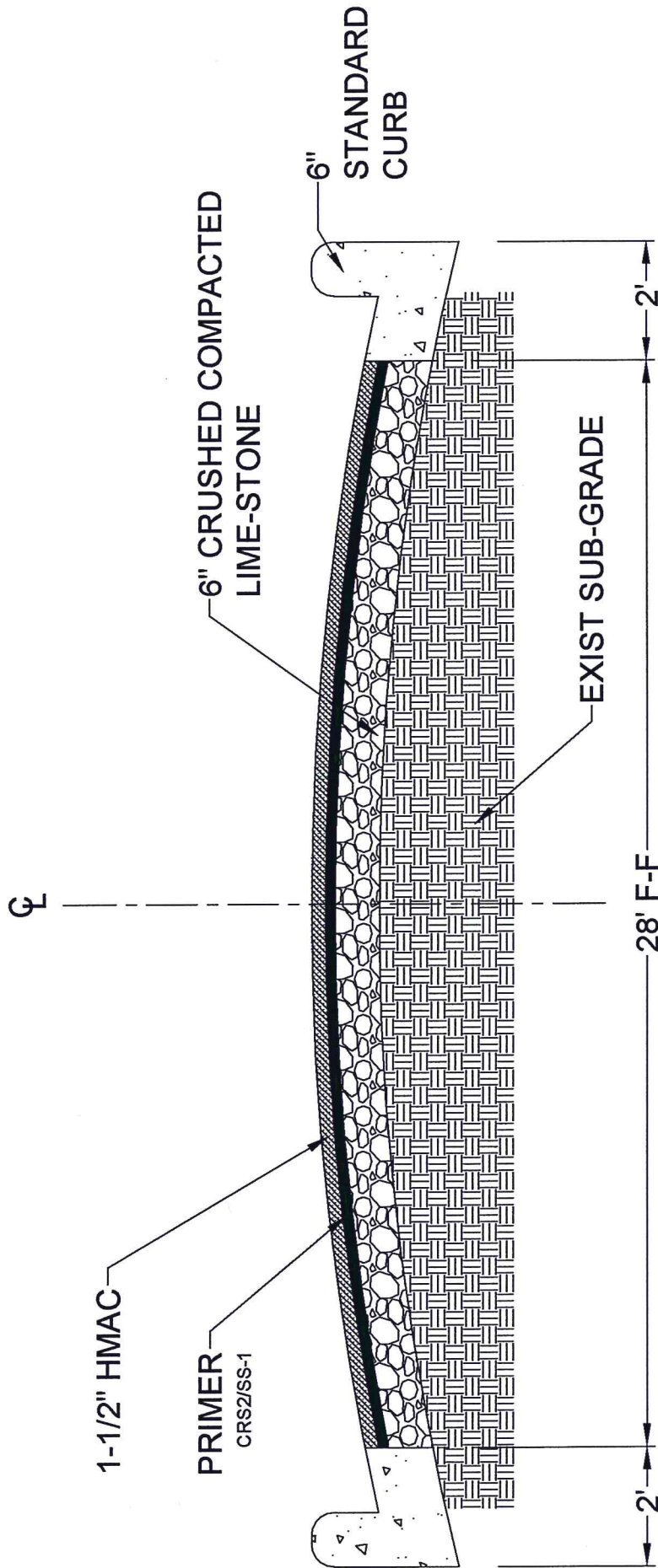
# City of Kingsville Construction and maintenance methods

## Concrete curb and gutter

1. 6" base material (limestone)
2. 6" 3000 psi concrete
3. #4 rebar, 18" off center



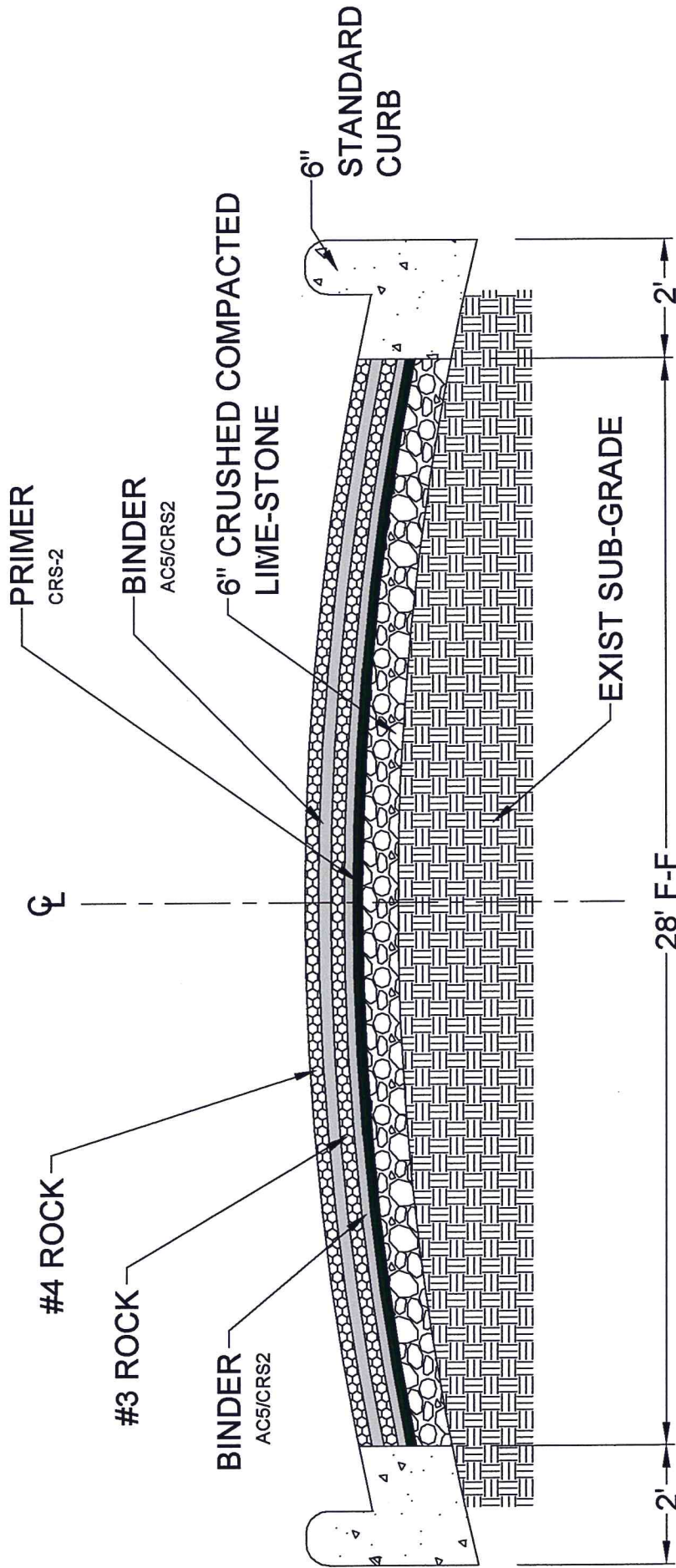
**STREET COMPOSITION - PAST METHOD**



**PAST METHOD OF STREET CONSTRUCTION (HMAC) - TYPICAL DISADVANTAGES**

Cost Estimate Item	Cost
Limestone	\$7,974
Primer (CRS-2)	\$475
1 1/2" Hot Mix	\$7,925
Total with new Flex. Base	\$16,374 / BLOCK
Total with exist. Flex. Base	\$8,400 / BLOCK

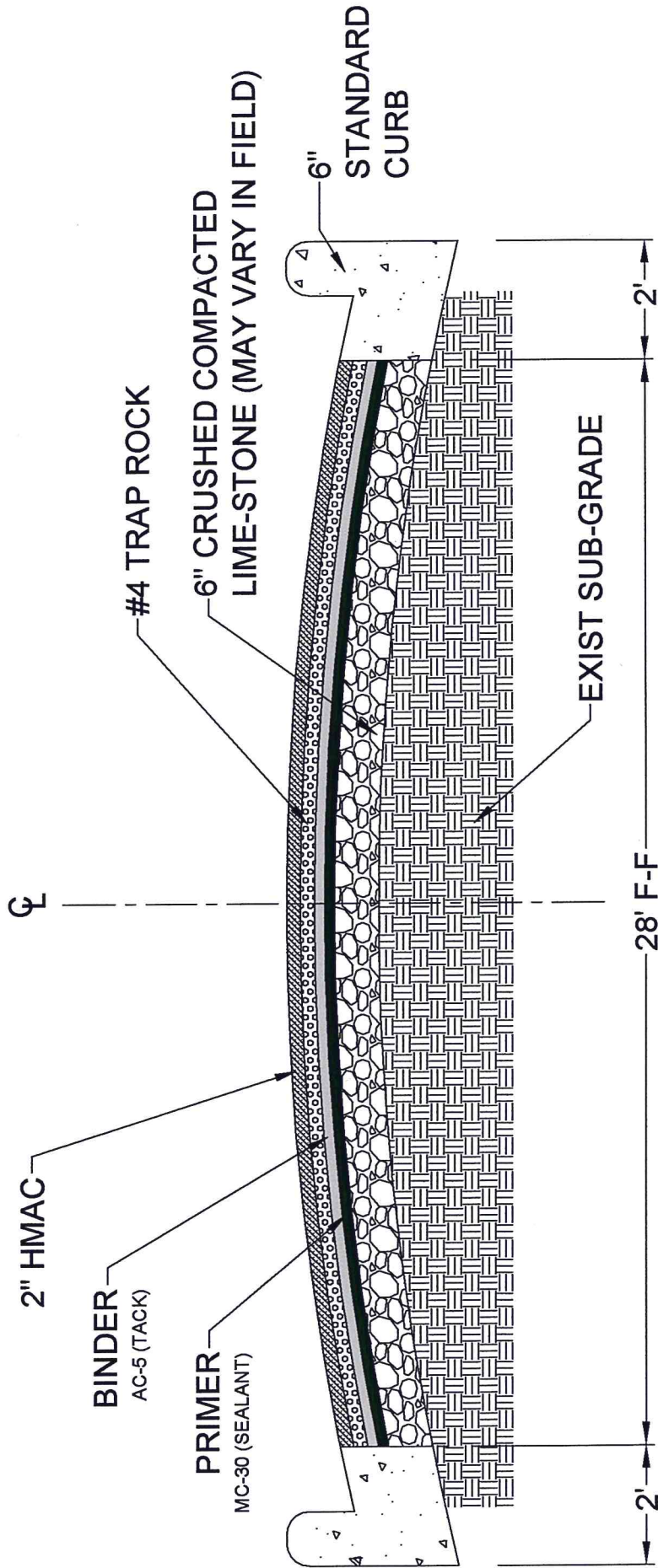
**STREET COMPOSITION - PAST METHOD**



**PAST METHOD OF STREET CONSTRUCTION (2 COURSE SEAL-COAT) - TYPICAL DISADVANTAGES**

Cost Estimate Item	Cost
Limestone	\$7,974
Primer (CRS-2)	\$475
#3 Rock	\$1,026
#4 Rock	\$821
Binder (AC-5)	\$1,684
Total with new Flex. Base	\$11,980 / BLOCK
Total with exist. Flex. Base	\$4,006 / BLOCK

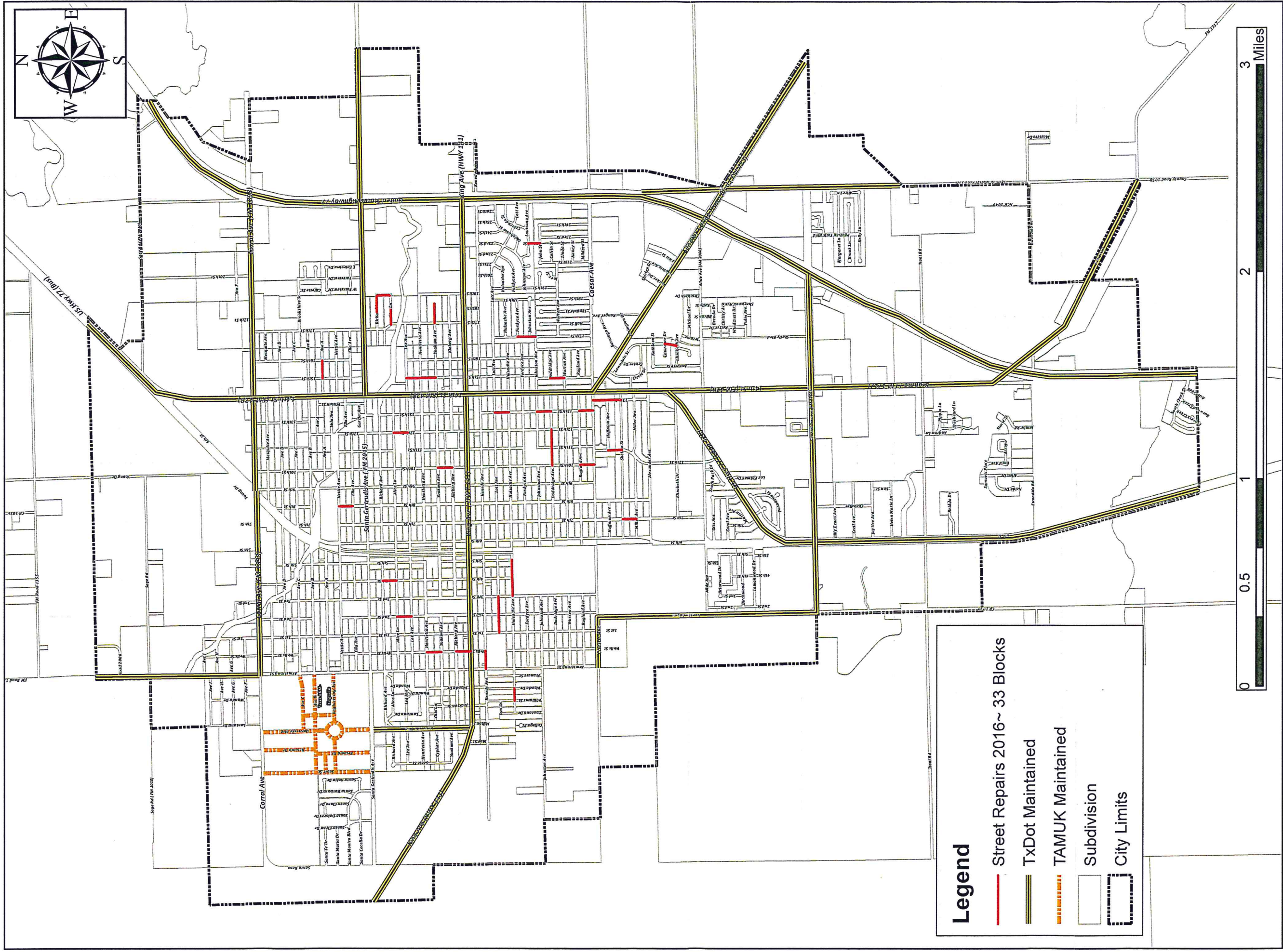
**STREET COMPOSITION - CURRENT IMPROVED METHOD**



**CURRENT METHOD OF STREET CONSTRUCTION (HMAC)**

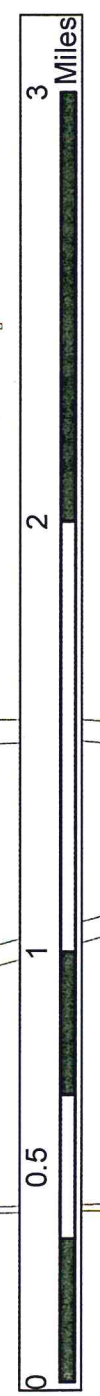
Cost Estimate Item	Cost
Limestone	\$7,974
Primer (MC-30)	\$1,867
2" Hot Mix	\$10,566
Trap Rock	\$747
Binder (AC-5)	\$918
<b>Total</b>	<b>\$22,072 / BLOCK</b>

# 2016 Street Repairs



**Legend**

- Street Repairs 2016~ 33 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits



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 Last Update: 12/14/2014  
 Note:

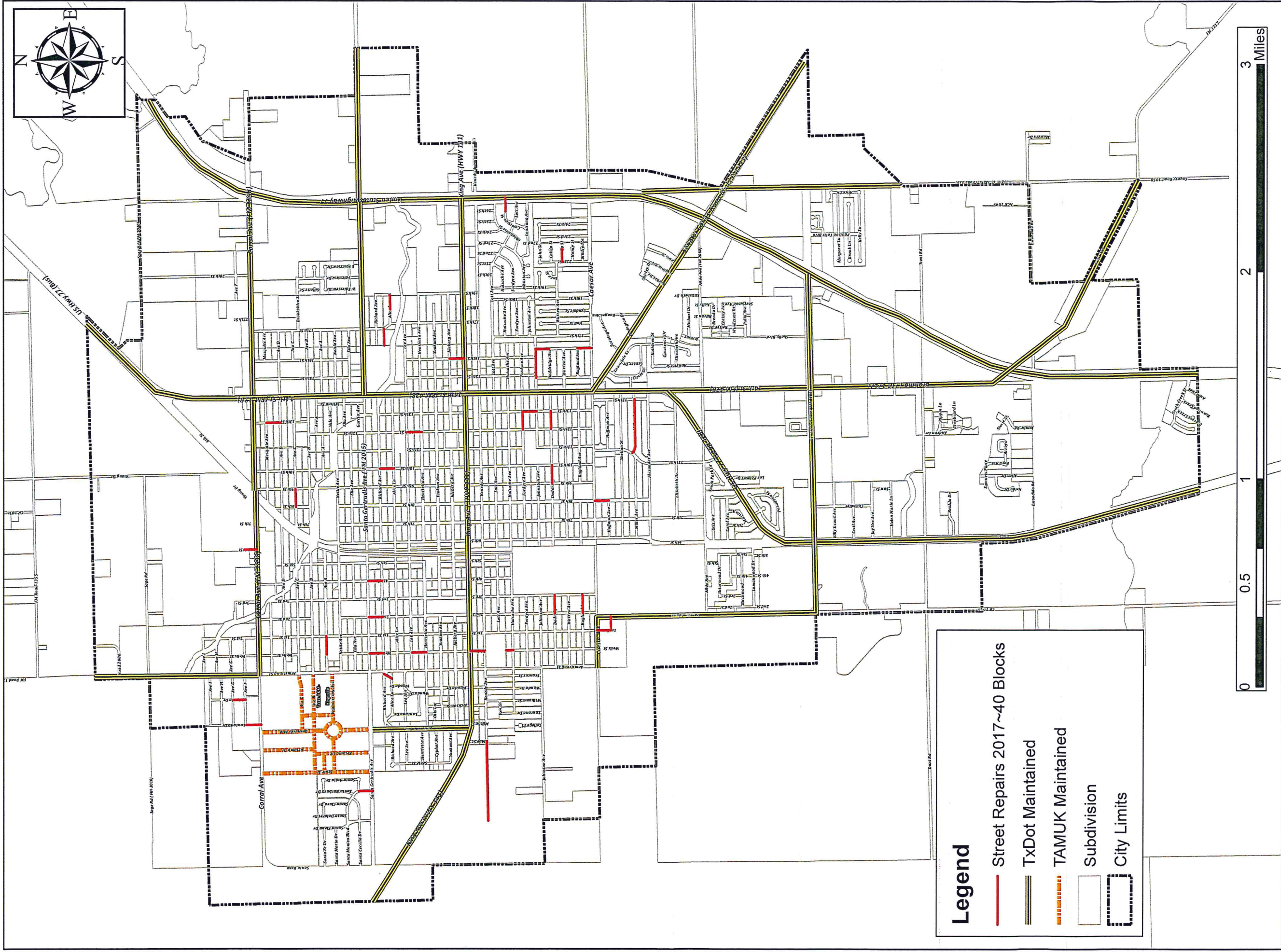
**5-9 A**

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# 2017 Street Repairs



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Last Update: 12/12/2014

Note:

5-10

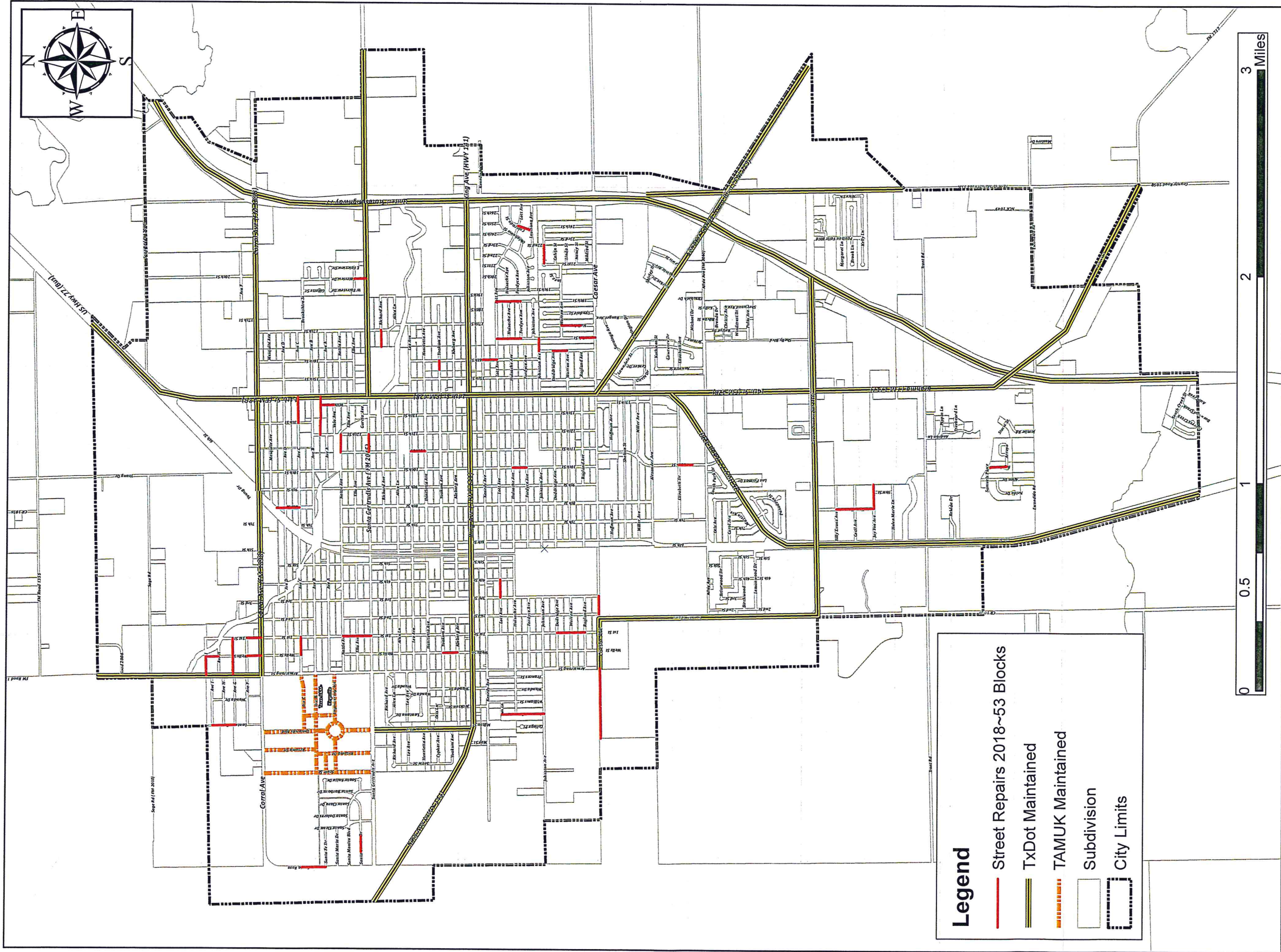
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# 2018 Street Repairs



**Legend**

- Street Repairs 2018~53 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

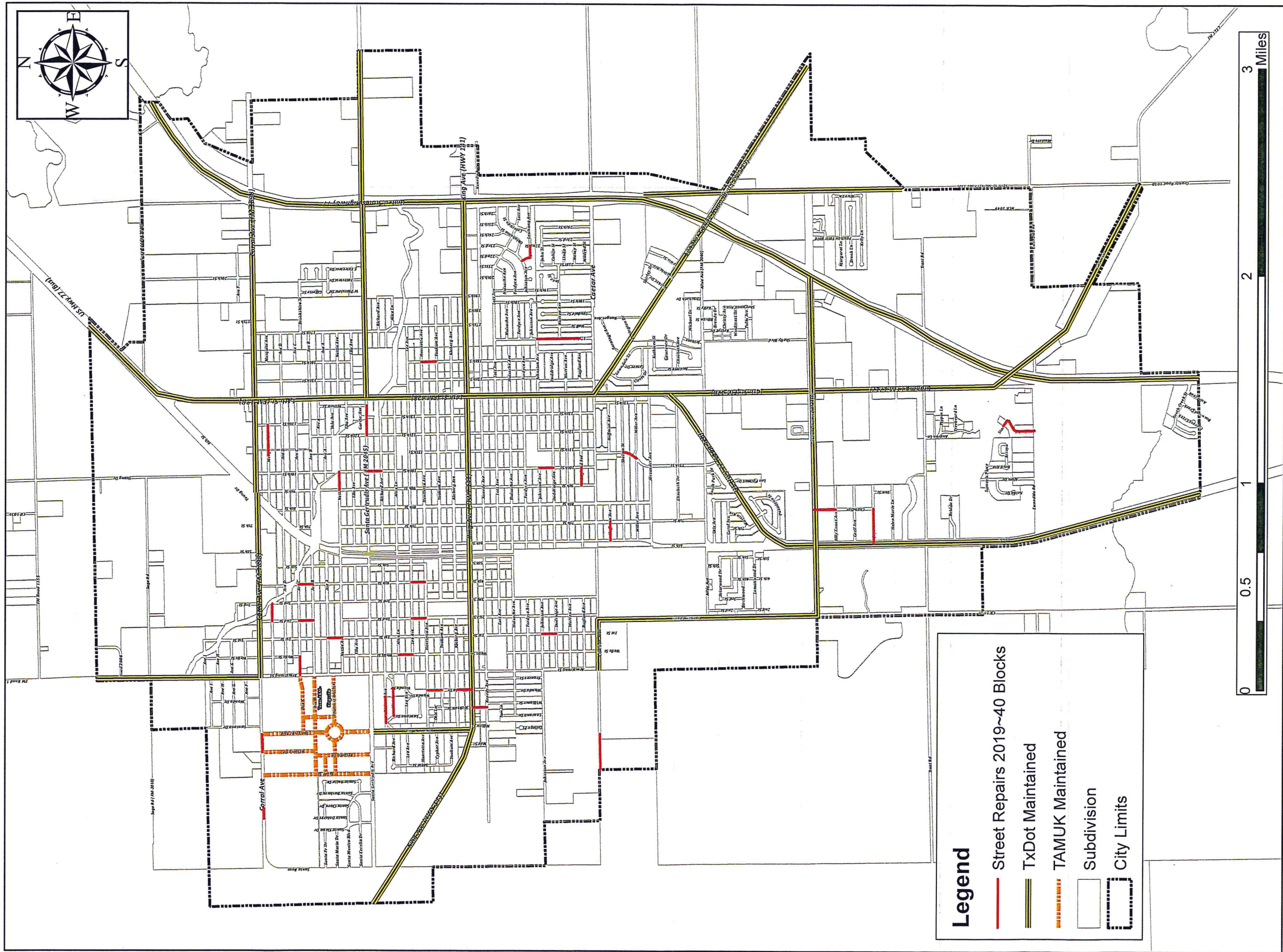
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# 2019 Street Repairs



**Legend**

- Street Repairs 2019~40 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

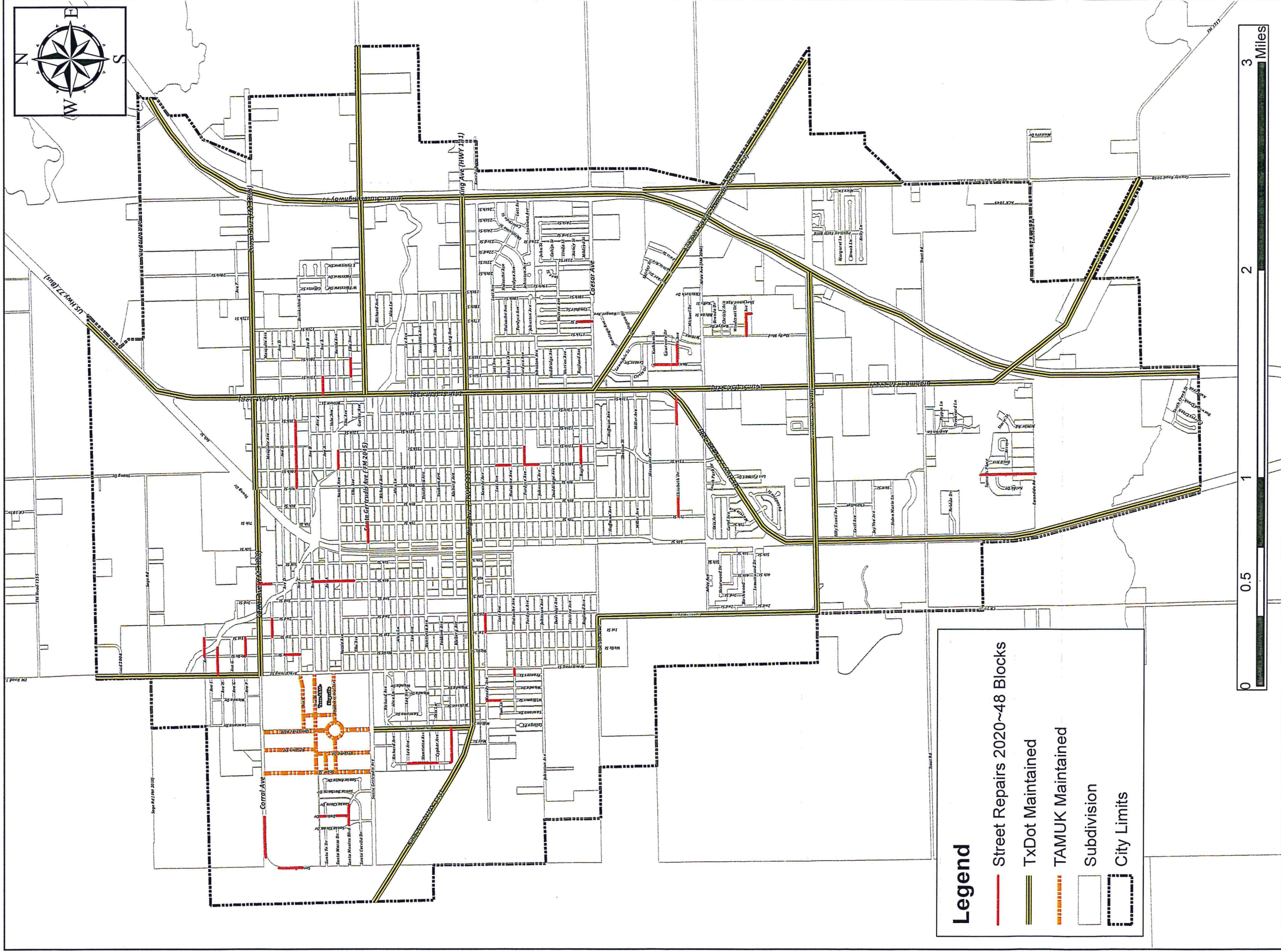
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# 2020 Street Repairs



**Legend**

- Street Repairs 2020~48 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

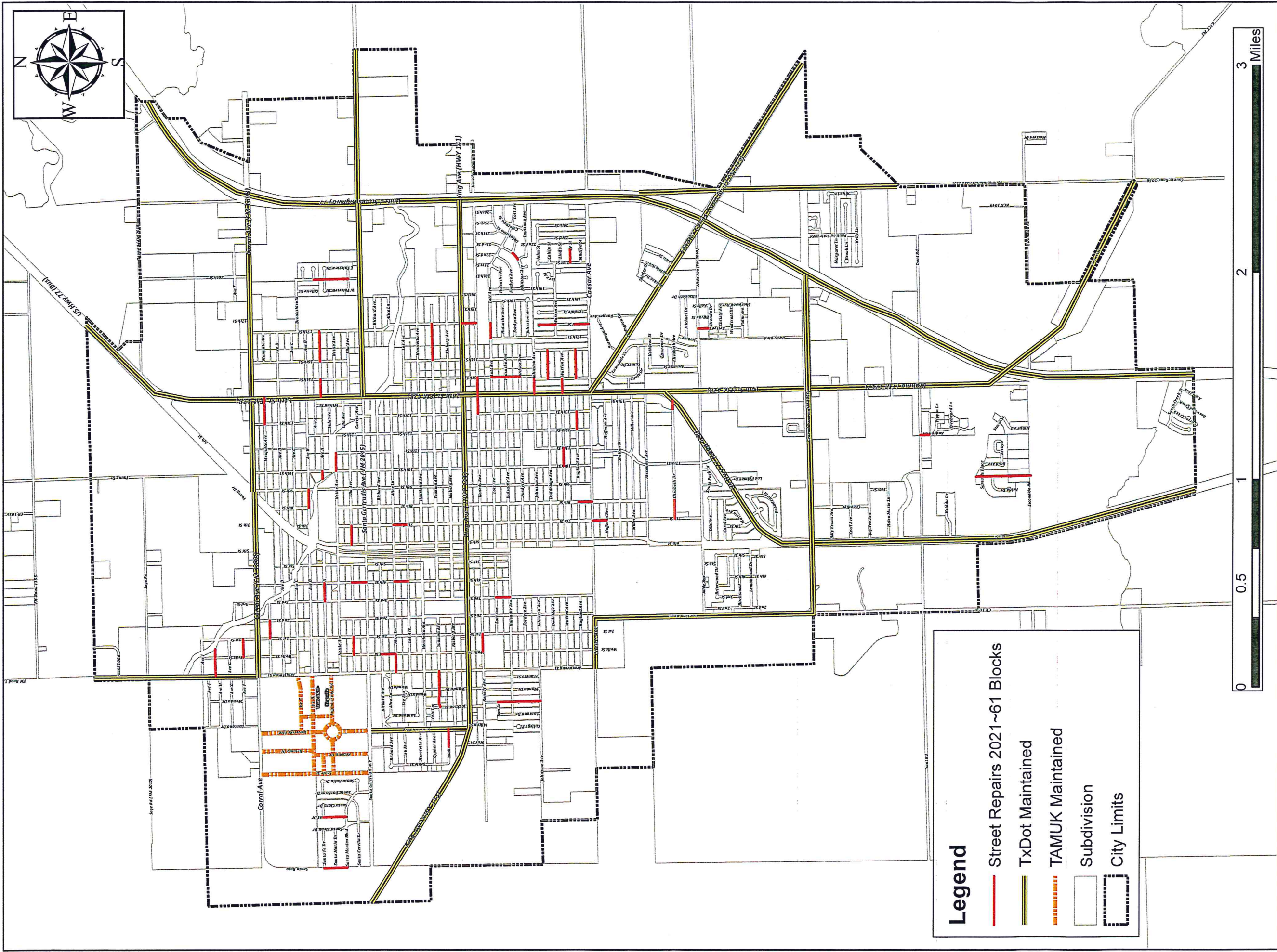
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# 2021 Street Repairs



**Legend**

- Street Repairs 2021~61 Blocks
- TxDot Maintained
- - - TAMUK Maintained
- Subdivision
- City Limits

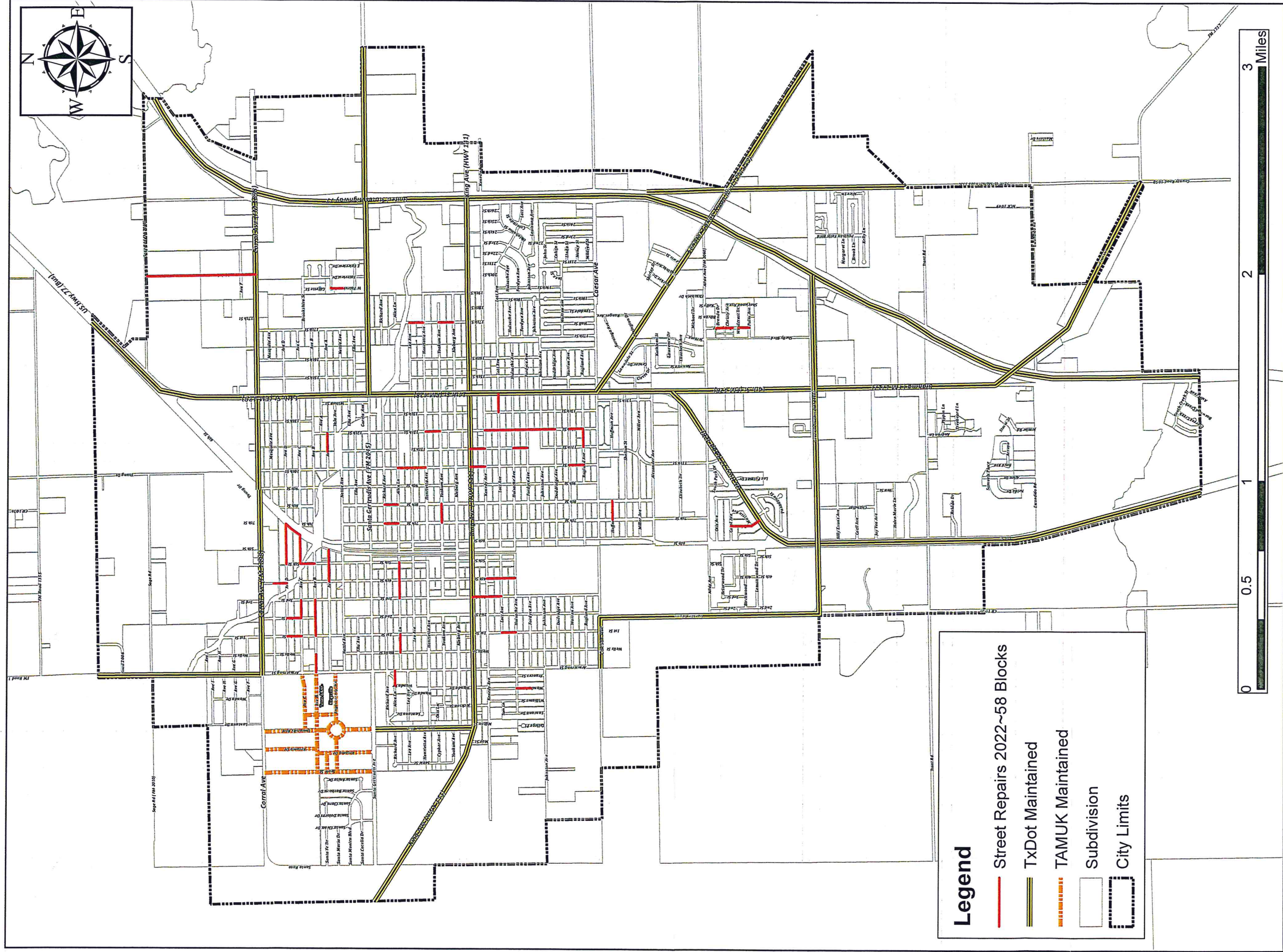
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# 2022 Street Repairs



**Legend**

- Street Repairs 2022~58 Blocks
- TxDot Maintained
- - - TAMUK Maintained
- Subdivision
- City Limits

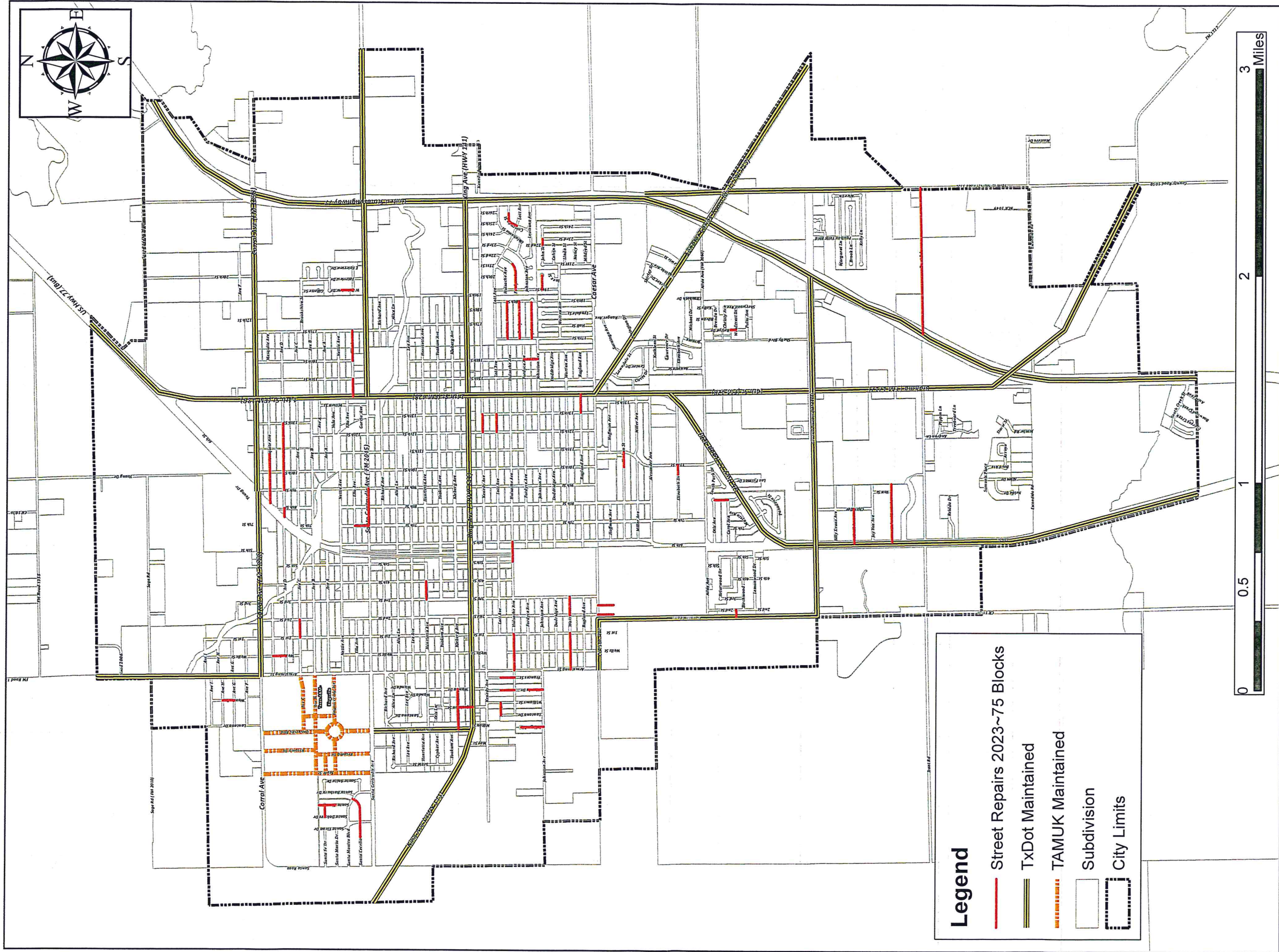


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 Note:

# 2023 Street Repairs



**Legend**

- Street Repairs 2023~75 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

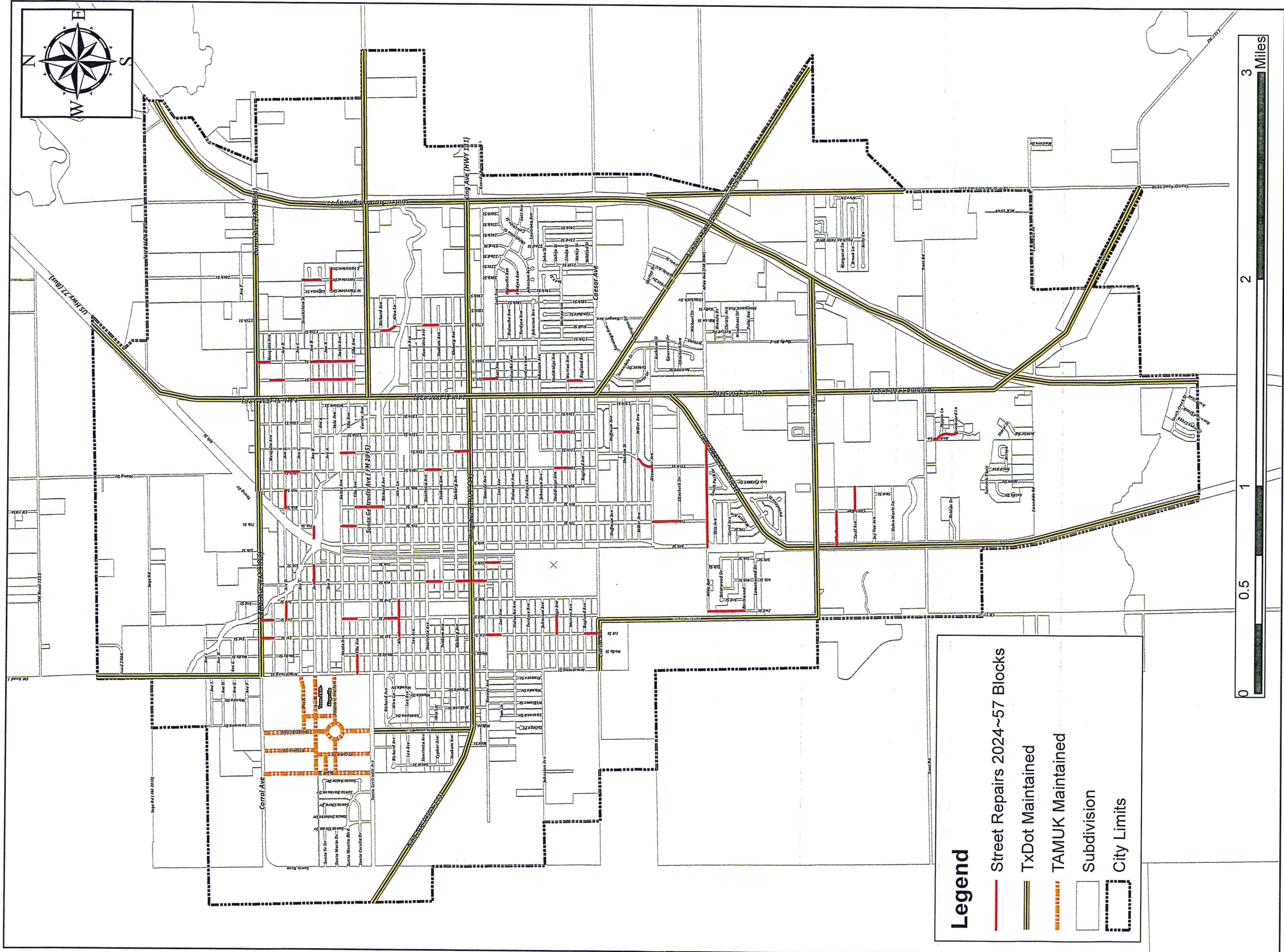
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Drawn By: Engineering Department  
Last Update: 12/12/2014  
Note:

# 2024 Street Repairs



**Legend**

- Street Repairs 2024~57 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

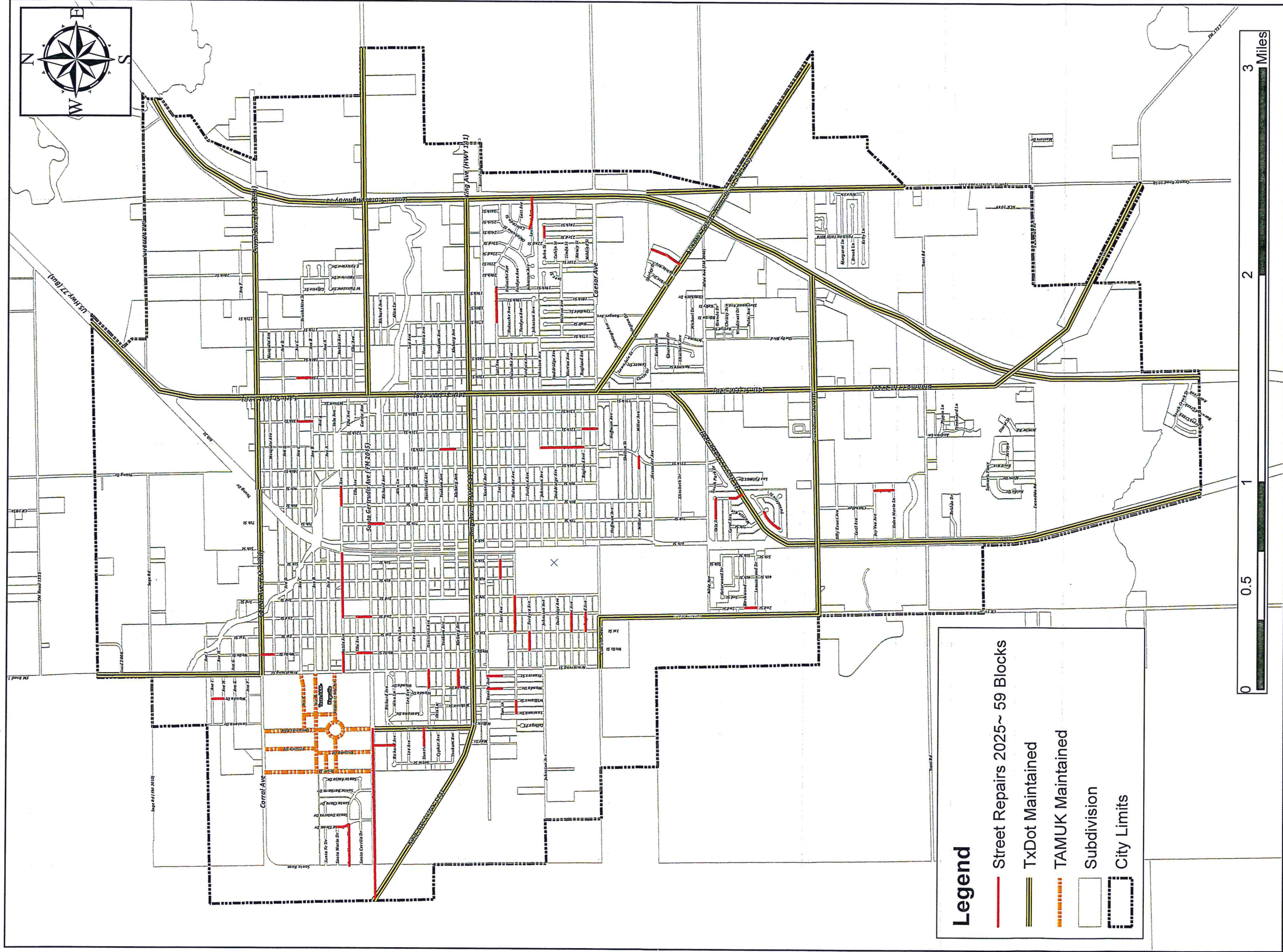
Drawn By: Engineering Department  
 Last Update: 12/12/2014  
 Note:

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# 2025 Street Repairs



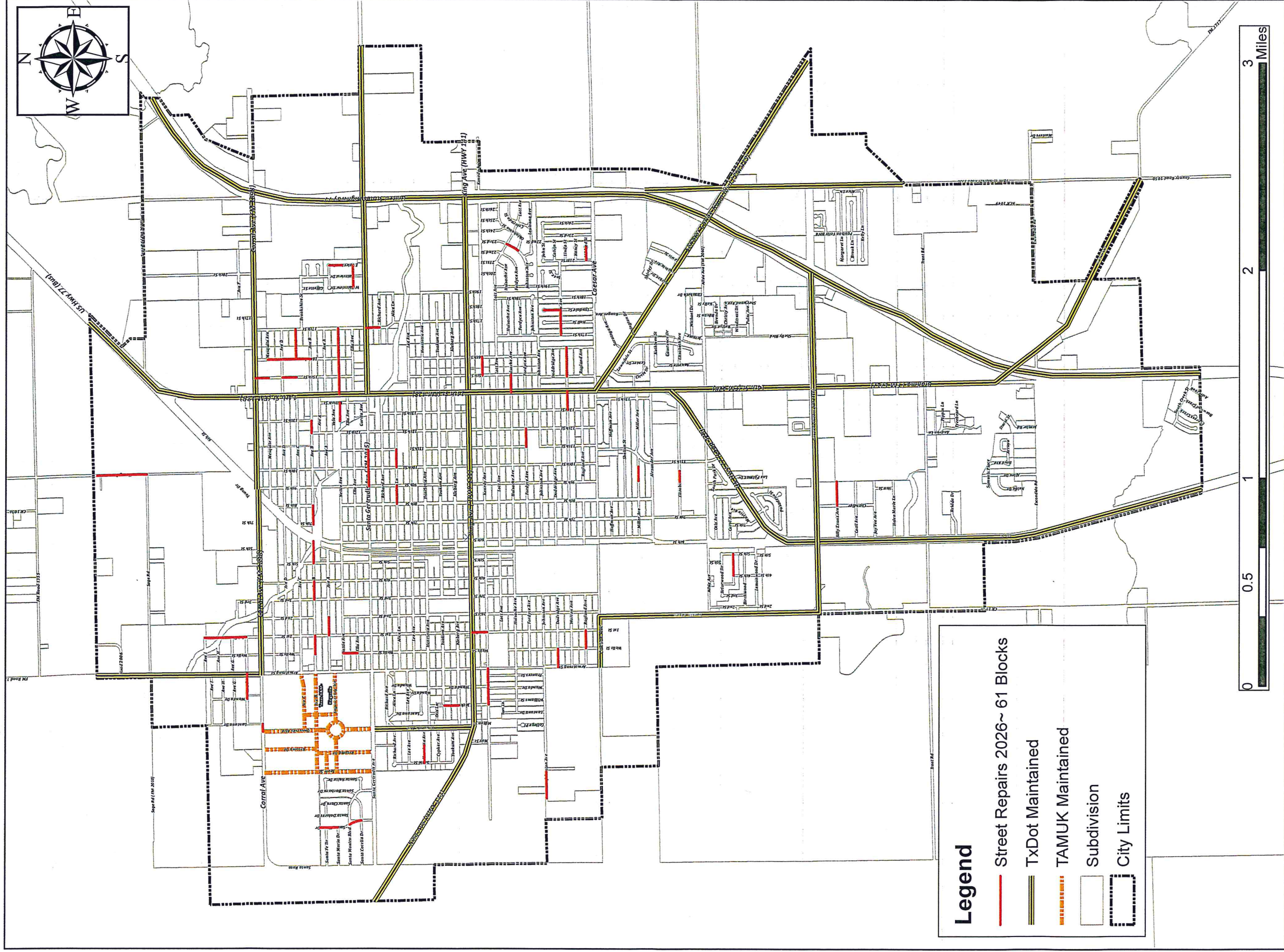
**Legend**

- Street Repairs 2025 ~ 59 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

	<p style="text-align: center;"><b>CITY OF KINGSVILLE</b>  <b>ENGINEERING DEPARTMENT</b>                  200 E. Kleberg                  Kingsville, Texas 78363                  Office: 361 595 8005                  Fax: 361 595 8035</p>
<p><b>DISCLAIMER</b>                  THIS MAP IS FOR VISUAL PURPOSES ONLY.                  THE INFORMATION ON THIS SHEET MAY                  CONTAIN INACCURACIES OR ERRORS.                  THE CITY OF KINGSVILLE IS NOT                  RESPONSIBLE IF THE INFORMATION CONTAINED                  HEREIN IS USED FOR ANY DESIGN,                  CONSTRUCTION, PLANNING, BUILDING,                  OR ANY OTHER PURPOSE.</p>	
Drawn By: Engineering Department	Last Update: 12/12/2014
Note:	
<p><b>5-18</b></p>	

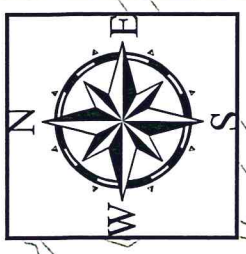


# 2026 Street Repairs




**Legend**

- Street Repairs 2026~ 61 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits



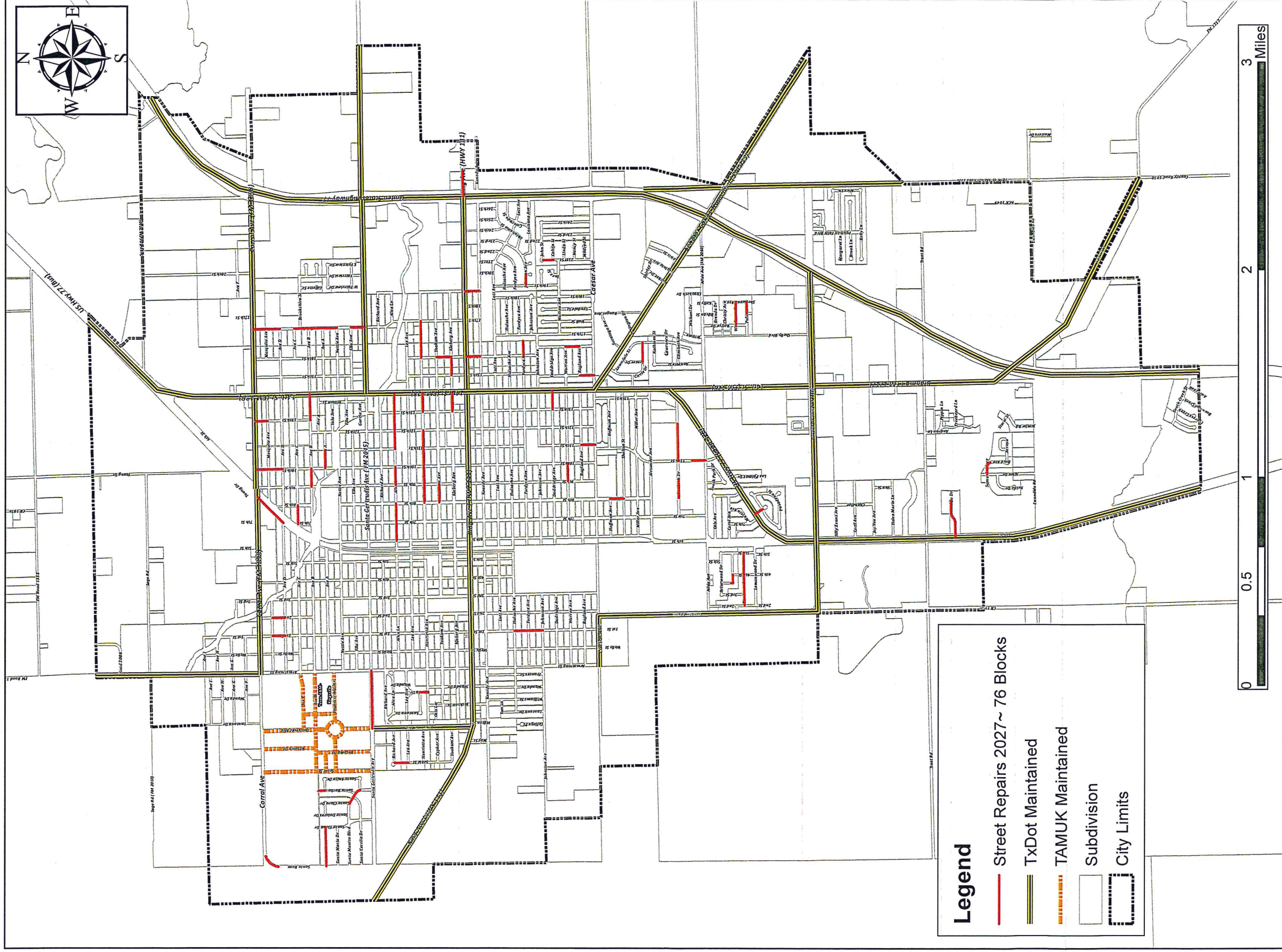
Drawn By: Engineering Department  
 Last Update: 12/12/2014  
 Note:

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# 2027 Street Repairs



**Legend**

- Street Repairs 2027 ~ 76 Blocks
- TxDot Maintained
- - - TAMUK Maintained
- Subdivision
- - - City Limits

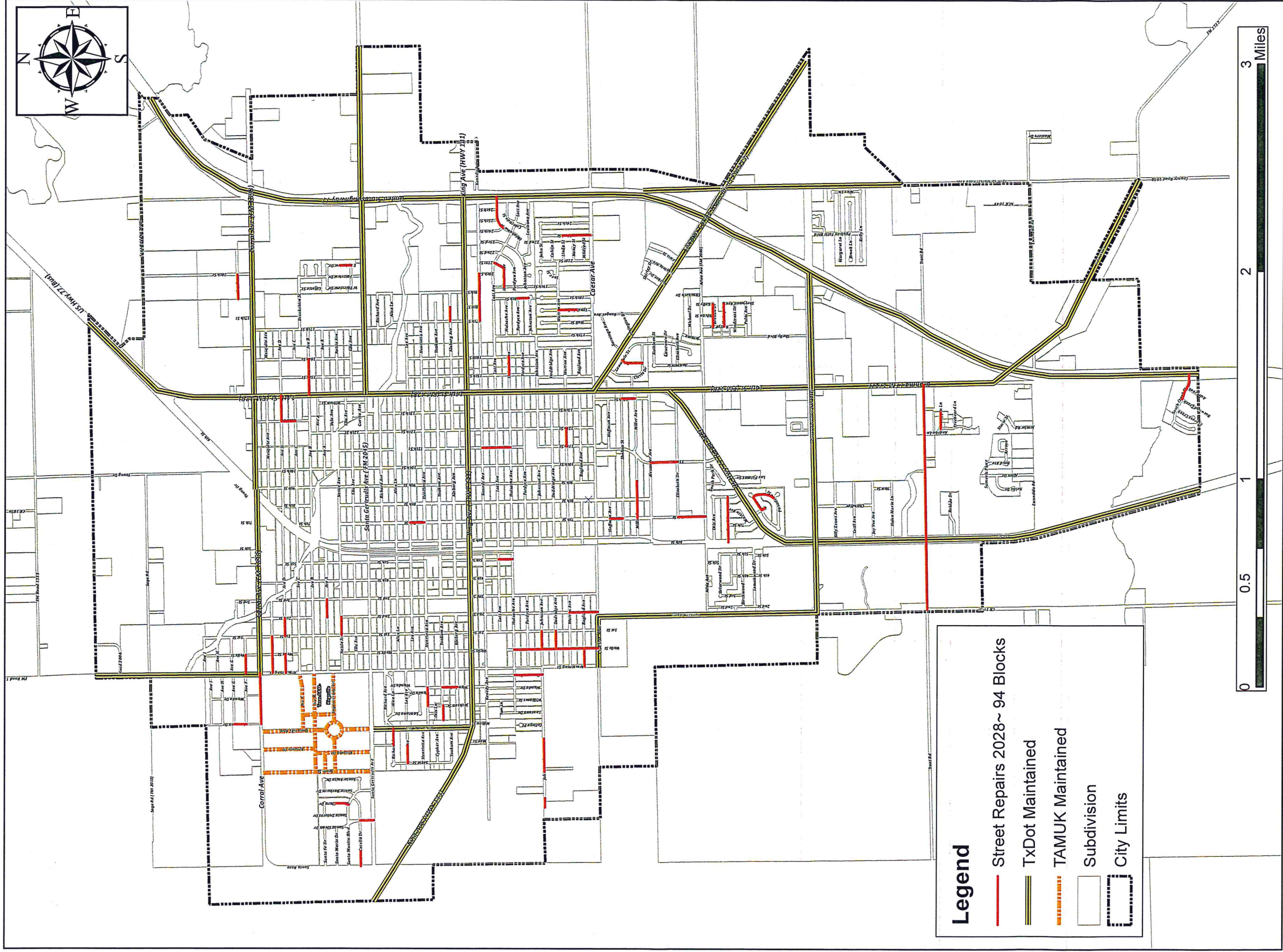
Drawn By: Engineering Department  
 Last Update: 12/12/2014  
 Note:

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# 2028 Street Repairs




**Legend**

- Street Repairs 2028 ~ 94 Blocks
- == TxDot Maintained
- - - TAMUK Maintained
- Subdivision
- City Limits

Drawn By: Engineering Department  
 Last Update: 12/12/2014  
 Note:

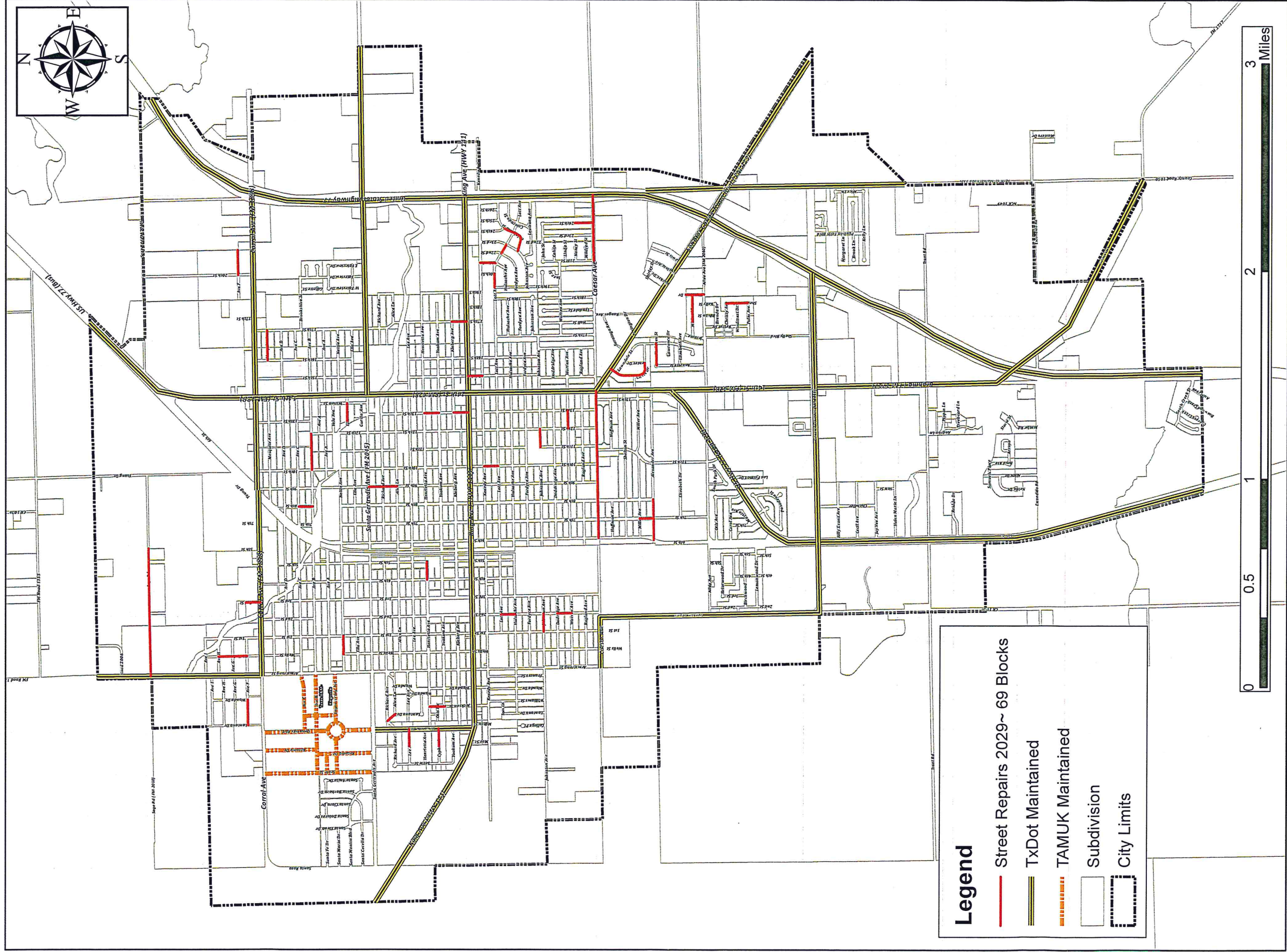
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# 2029 Street Repairs

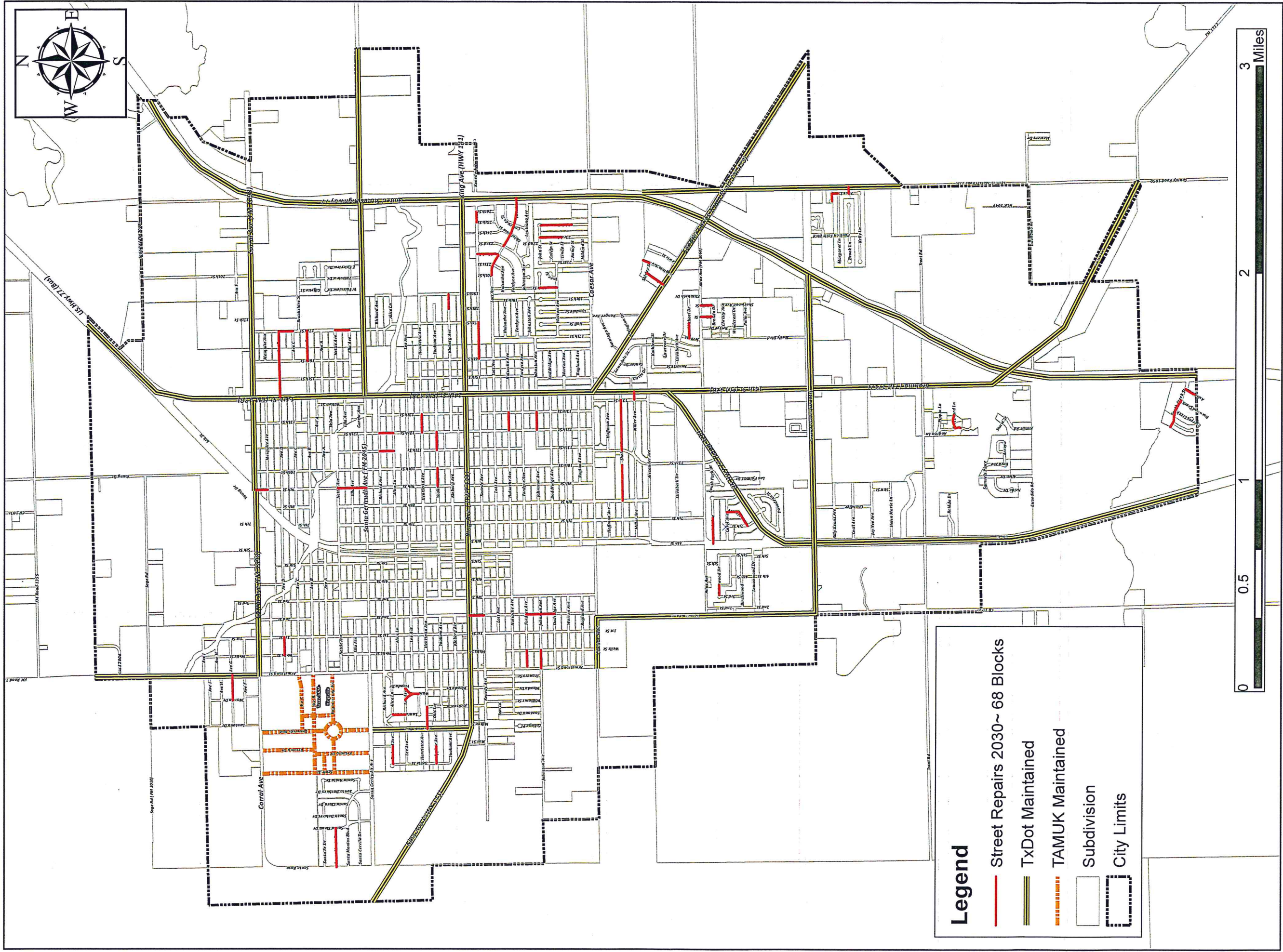


**Legend**

- Street Repairs 2029~ 69 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

<p><b>5-22</b></p>	<p>Drawn By: Engineering Department</p> <p>Last Update: 12/12/2014</p> <p>Note:</p>	<p><b>DISCLAIMER</b></p> <p>THIS MAP IS FOR VISUAL PURPOSES ONLY. THE INFORMATION ON THIS SHEET MAY CONTAIN INACCURACIES OR ERRORS. THE CITY OF KINGSVILLE IS NOT RESPONSIBLE IF THE INFORMATION CONTAINED HEREIN IS USED FOR ANY DESIGN, CONSTRUCTION, PLANNING, BUILDING, OR ANY OTHER PURPOSE.</p>
		<p><b>CITY OF KINGSVILLE</b>  <b>ENGINEERING DEPARTMENT</b>          200 E. Kleberg          Kingsville, Texas 78363          Office: 361 595 8005          Fax: 361 595 8035</p>

# 2030 Street Repairs



**Legend**

- Street Repairs 2030~ 68 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

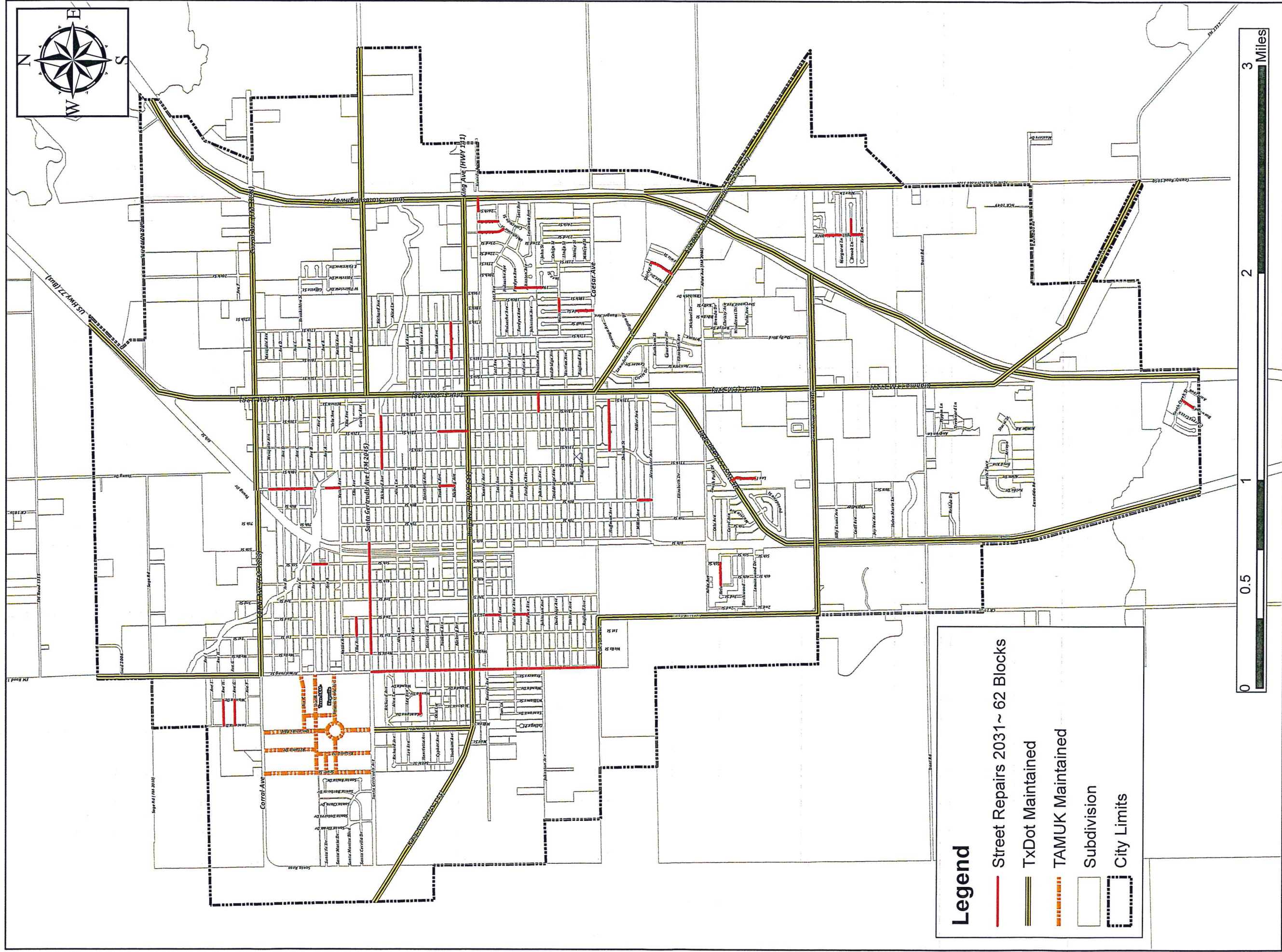
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Last Update: 12/12/2014  
Note:

# 2031 Street Repairs

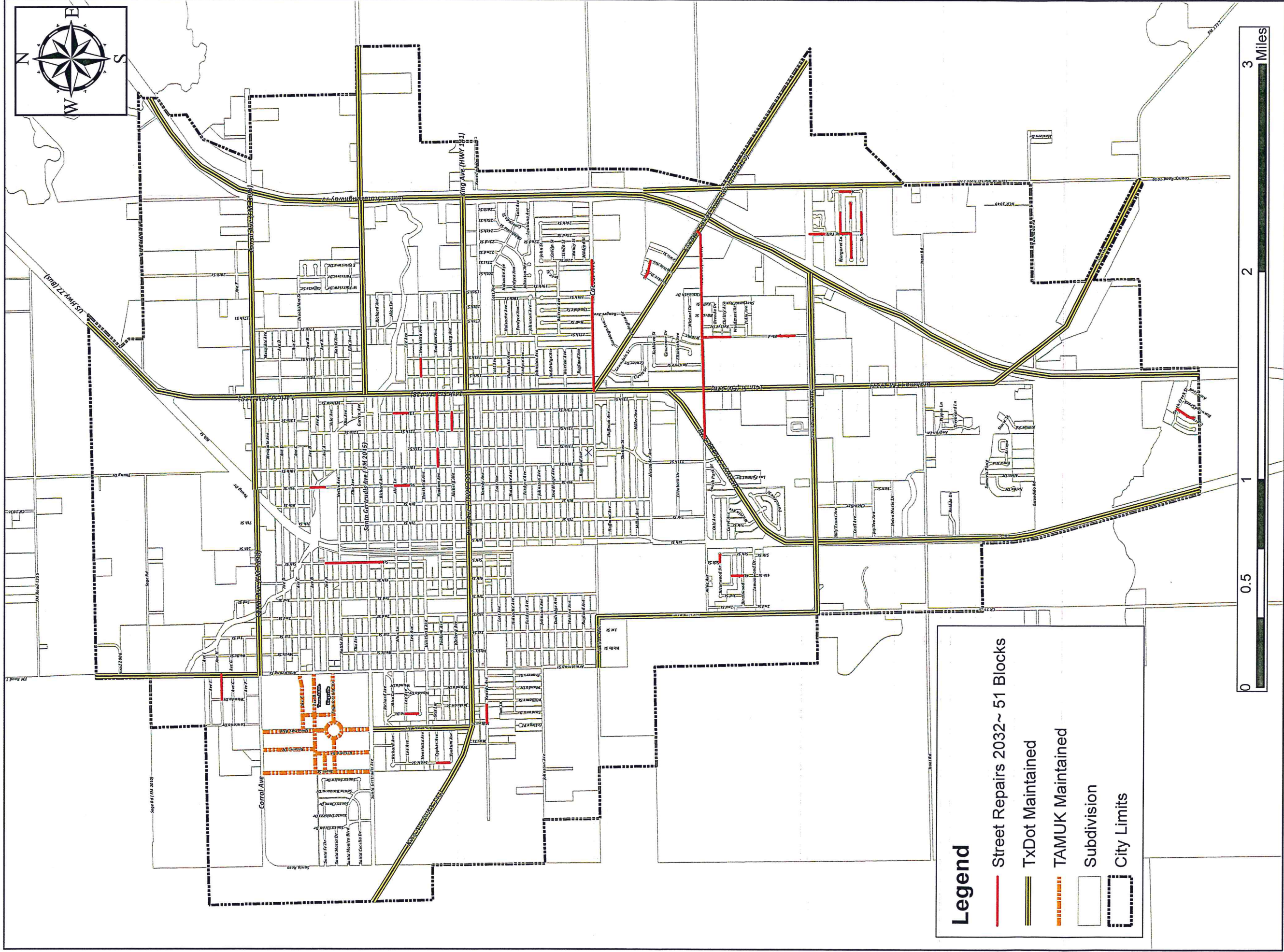


**Legend**

- Street Repairs 2031 ~ 62 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits


	<p style="text-align: center;"><b>CITY OF KINGSVILLE</b>  <b>ENGINEERING DEPARTMENT</b>                  200 E. Kleberg                  Kingsville, Texas 78363                  Office: 361 595 8005                  Fax: 361 595 8035</p>
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<p>Drawn By: Engineering Department</p>	<p>Last Update: 12/12/2014</p> <p>Note:</p>
<p><b>5-24</b></p>	

# 2032 Street Repairs

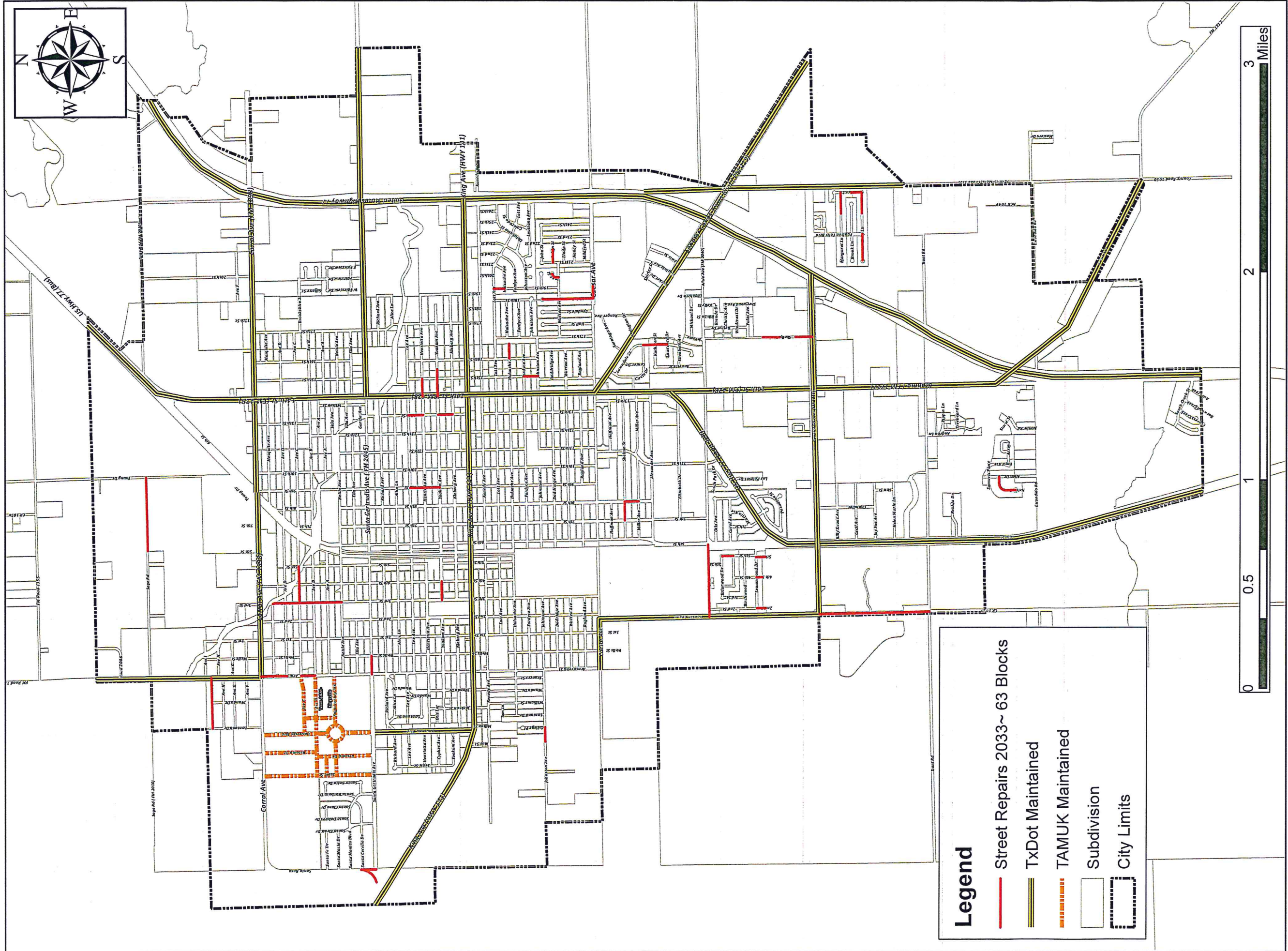


**Legend**

- Street Repairs 2032~ 51 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

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<p>Drawn By: Engineering Department</p>	<p>Last Update: 12/12/2014</p>
<p><b>5-25</b></p>	

# 2033 Street Repairs



**Legend**

- Street Repairs 2033~ 63 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

Drawn By: Engineering Department  
 Last Update: 12/14/2014  
 Note:

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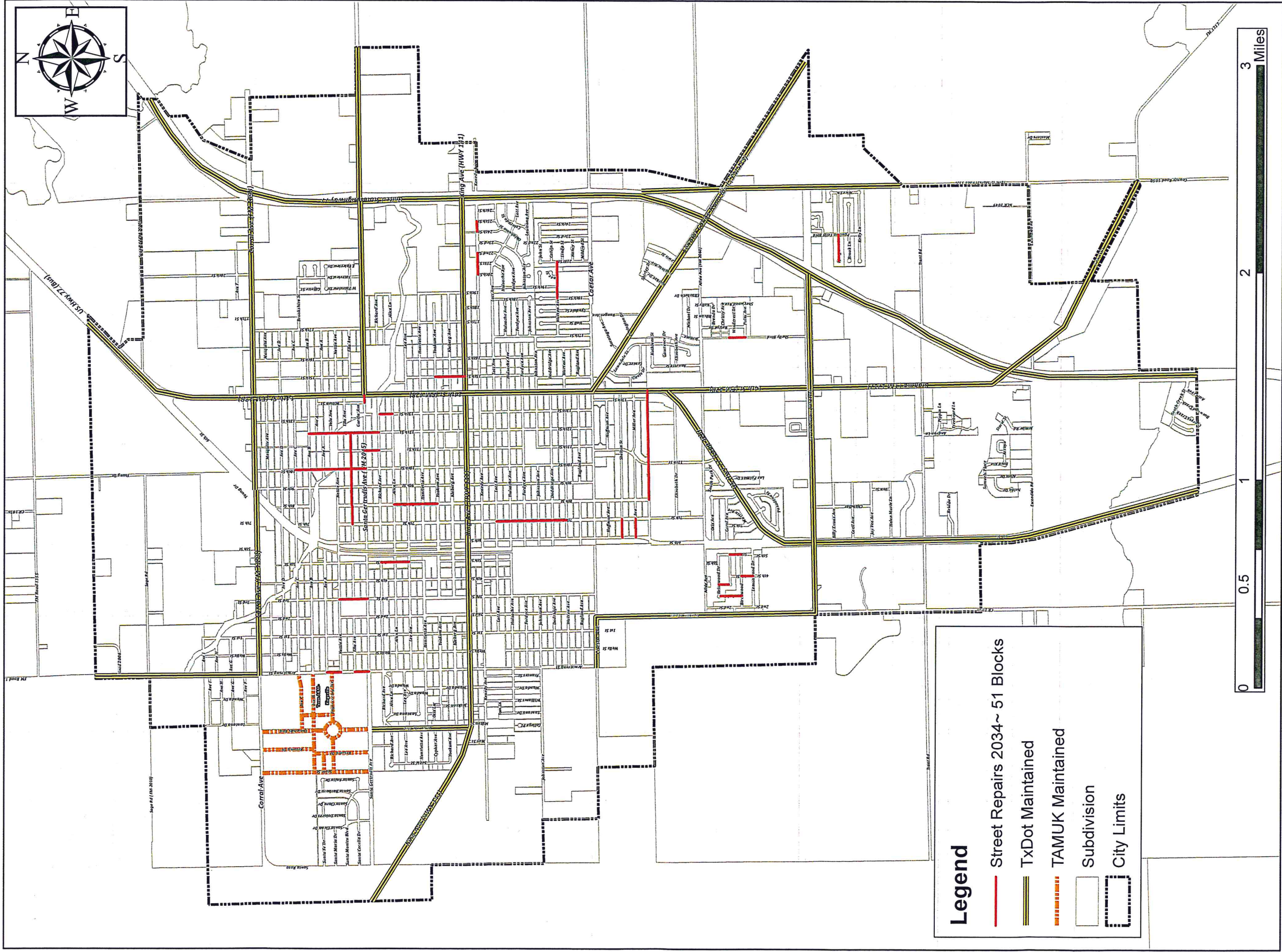


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# 2034 Street Repairs

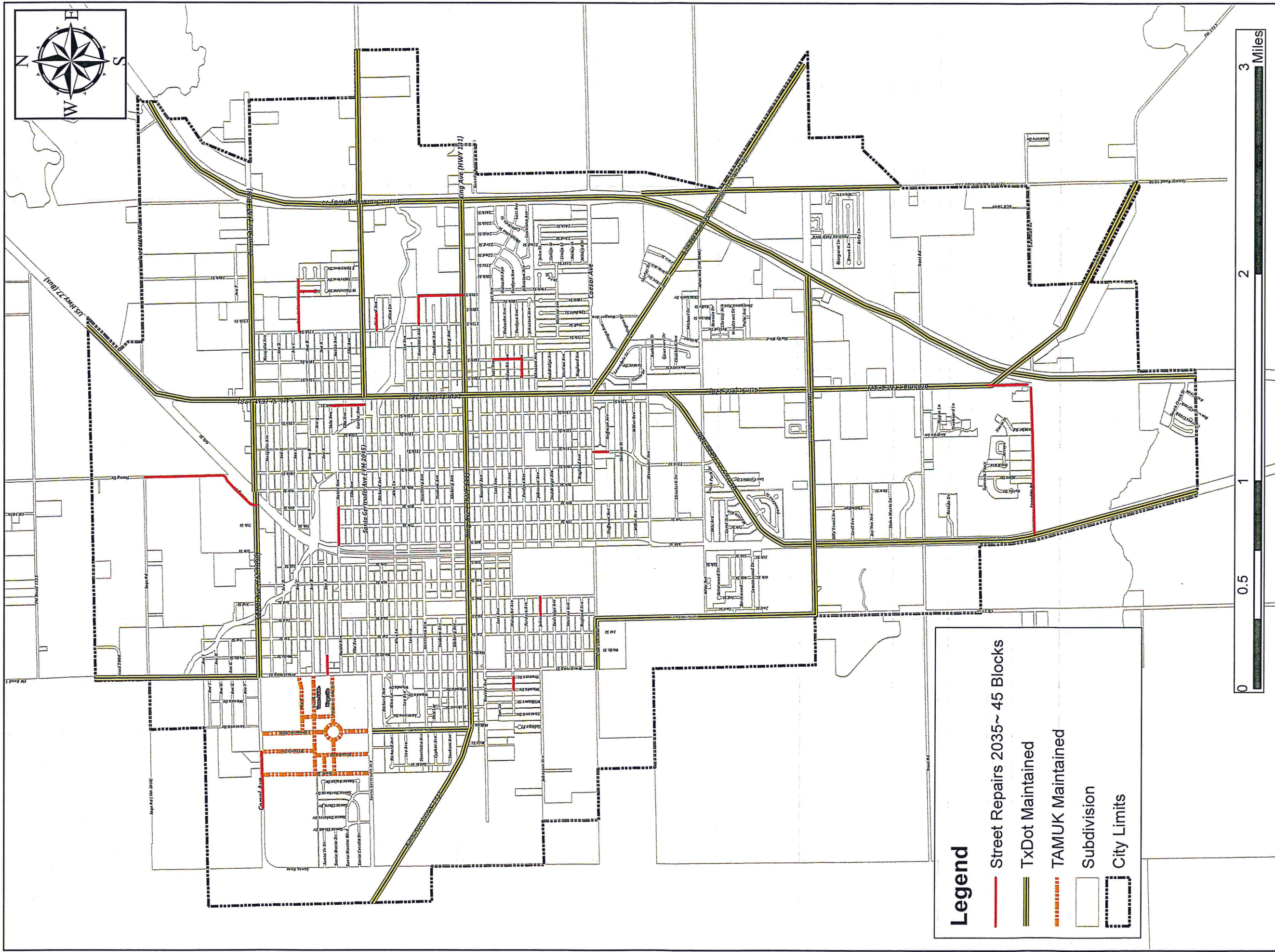


**Legend**

- Street Repairs 2034 ~ 51 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

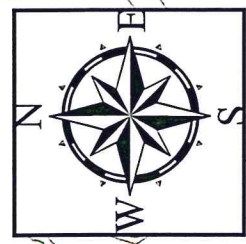
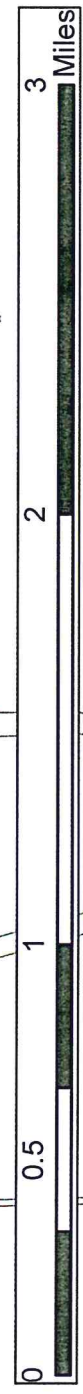
	<p><b>CITY OF KINGSVILLE</b>  <b>ENGINEERING DEPARTMENT</b>          200 E. Kleberg          Kingsville, Texas 78363          Office: 361 595 8005          Fax: 361 595 8035</p>
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<p>Drawn By: Engineering Department</p>	<p>Last Update: 12/14/2014</p> <p>Note:</p>
<p><b>5-27</b></p>	

# 2035 Street Repairs



**Legend**

- Street Repairs 2035~ 45 Blocks
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits



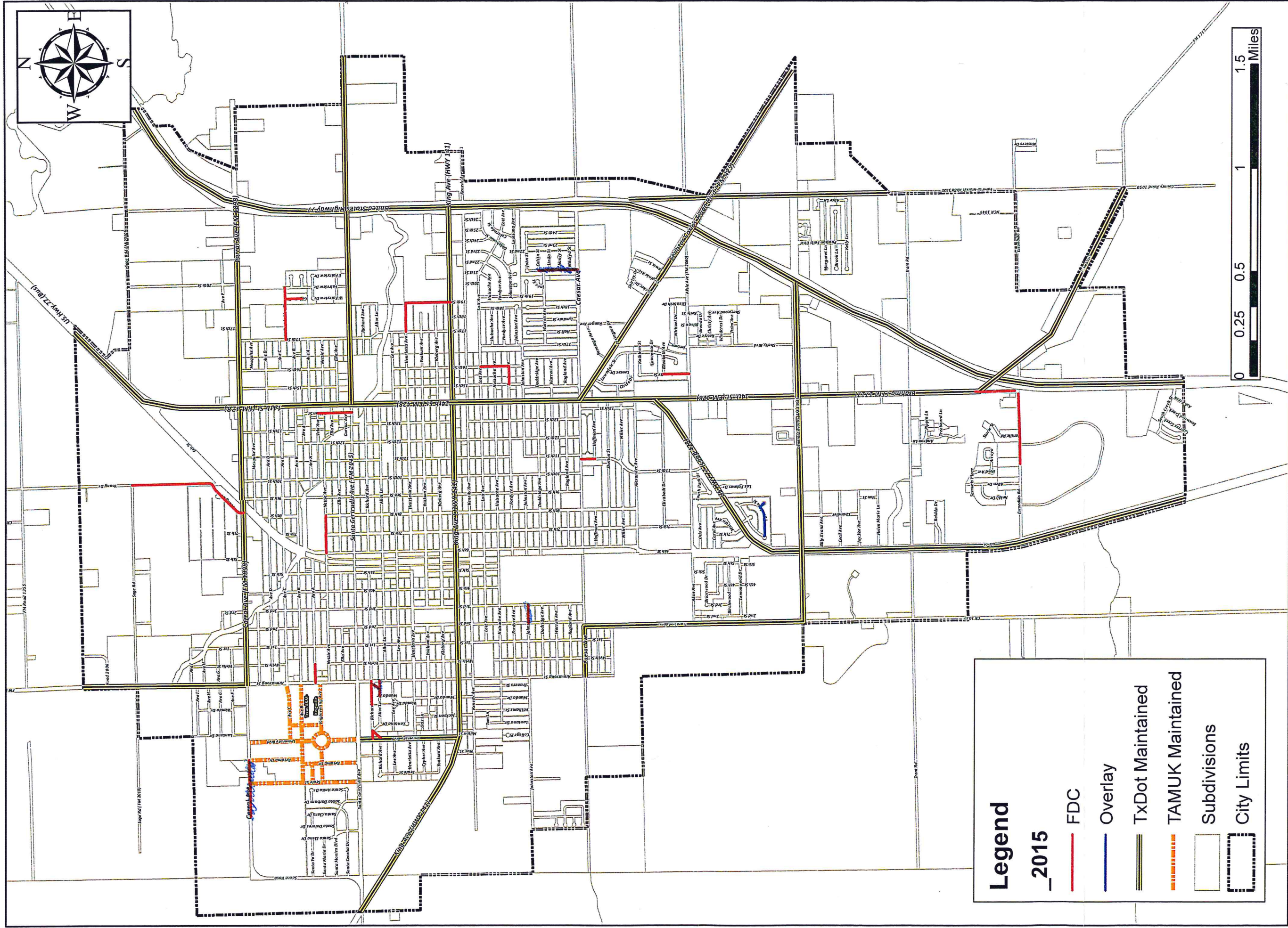
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Last Update: 12/14/2014  
Note:

# 2015 Street Construction



**Legend**

- \_2015**
- FDC
- Overlay
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

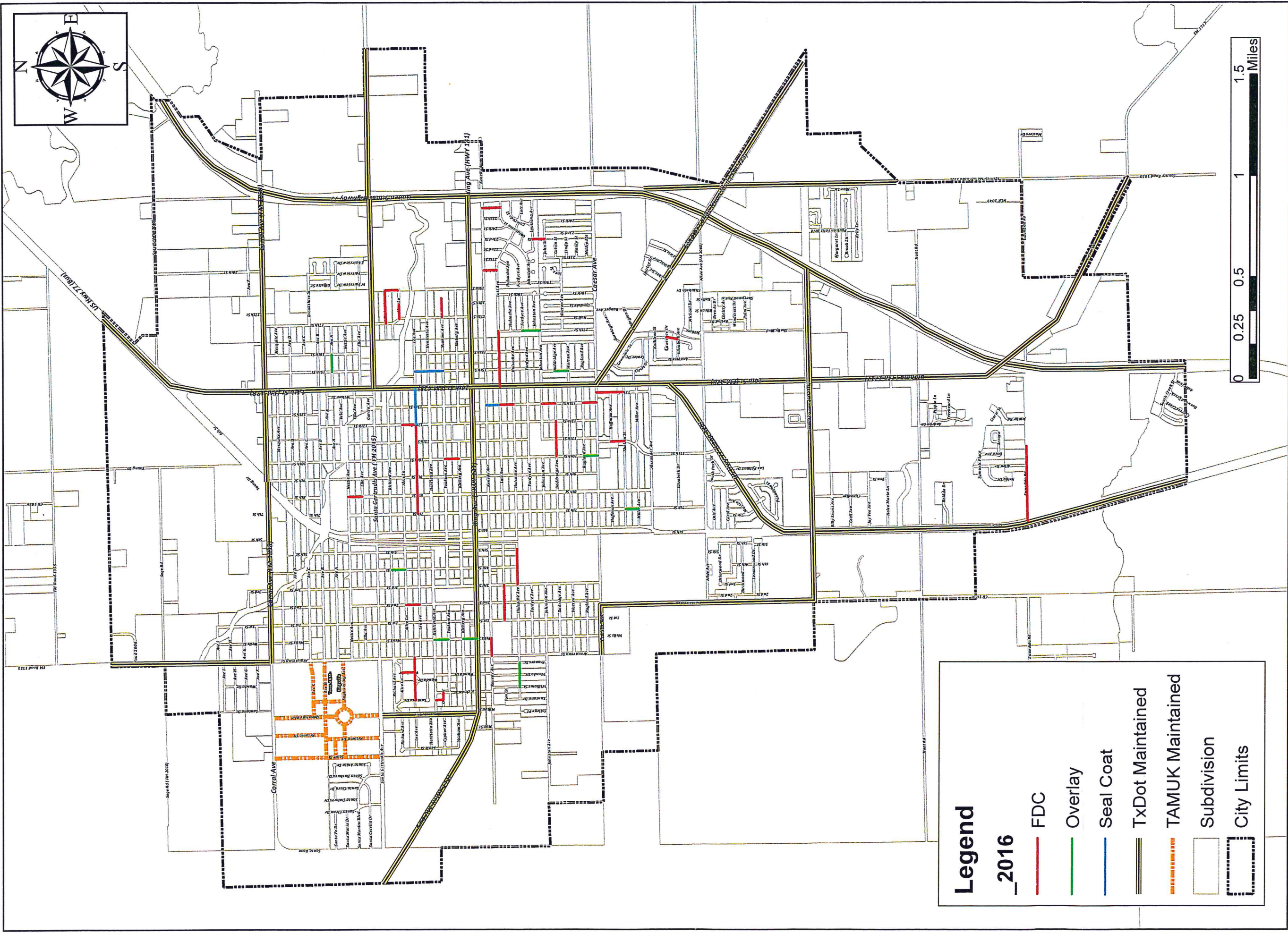
**5-1a**

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# 2016 Street Construction

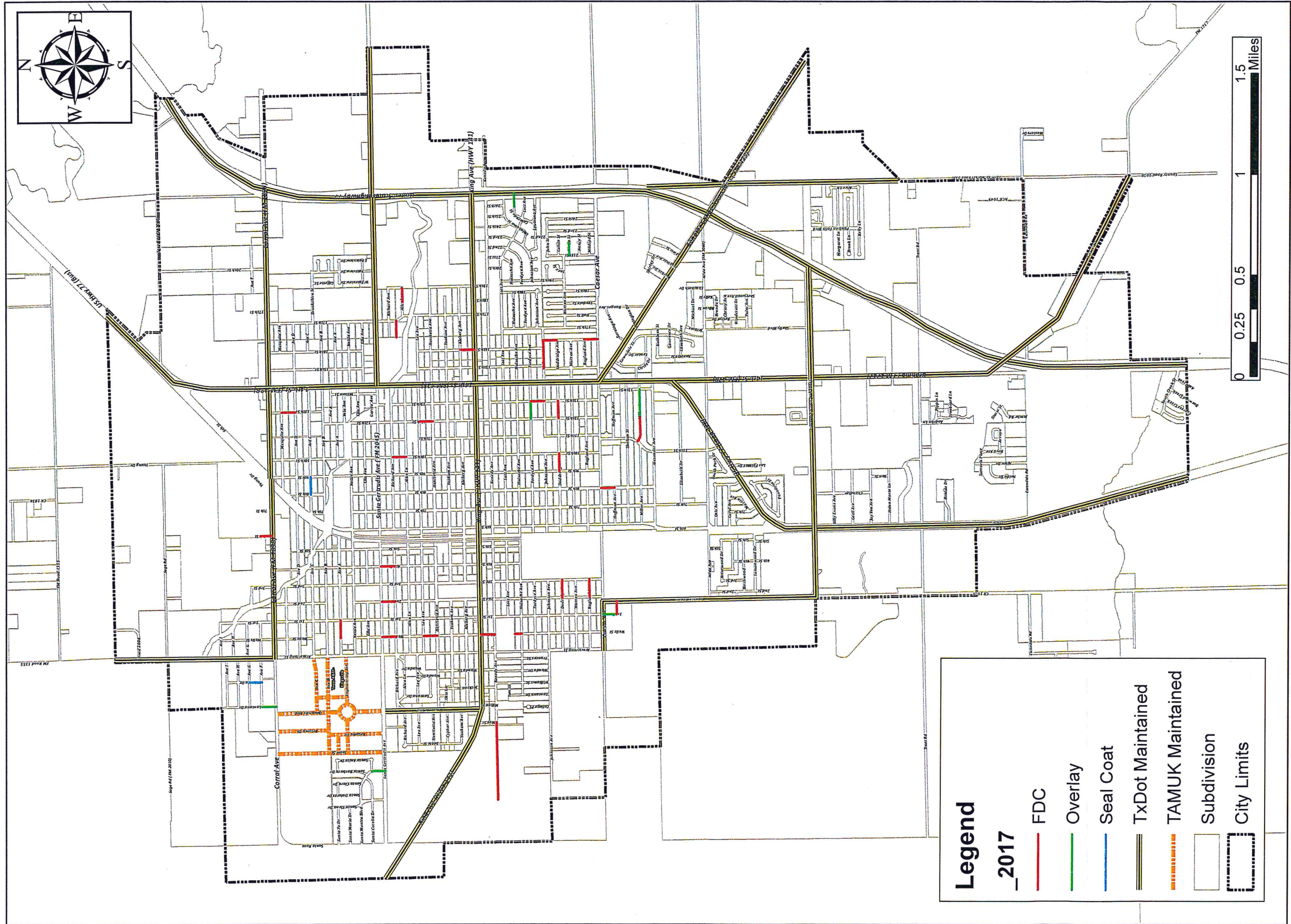


**Legend**

- FDC
- Overlay
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

	<p><b>CITY OF KINGSVILLE</b>  <b>ENGINEERING DEPARTMENT</b>          200 E. Kleberg          Kingsville, Texas 78363          Office: 361 595 8005          Fax: 361 595 8035</p>
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<p>Drawn By: Engineering Department</p>	<p>Last Update: 5/12/2015</p>
<p><b>5-2a</b></p>	

# 2017 Street Construction



**Legend**

- FDC
- Overlay
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivision
- City Limits

Drawn By: Engineering Department

Last Update: 5/12/2015

Note:

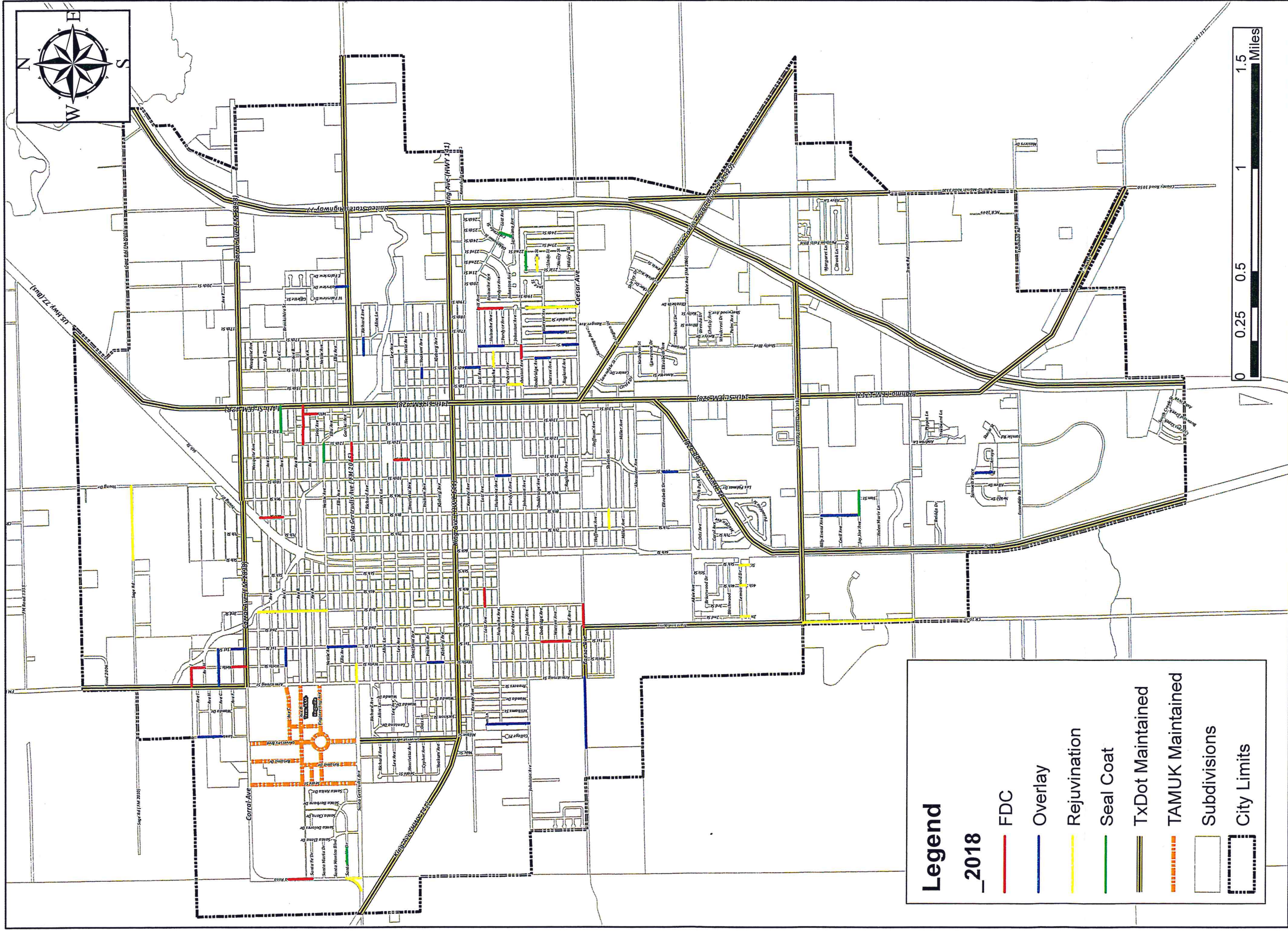
**5-3a**

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# 2018 Street Construction



**Legend**

- \_2018**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

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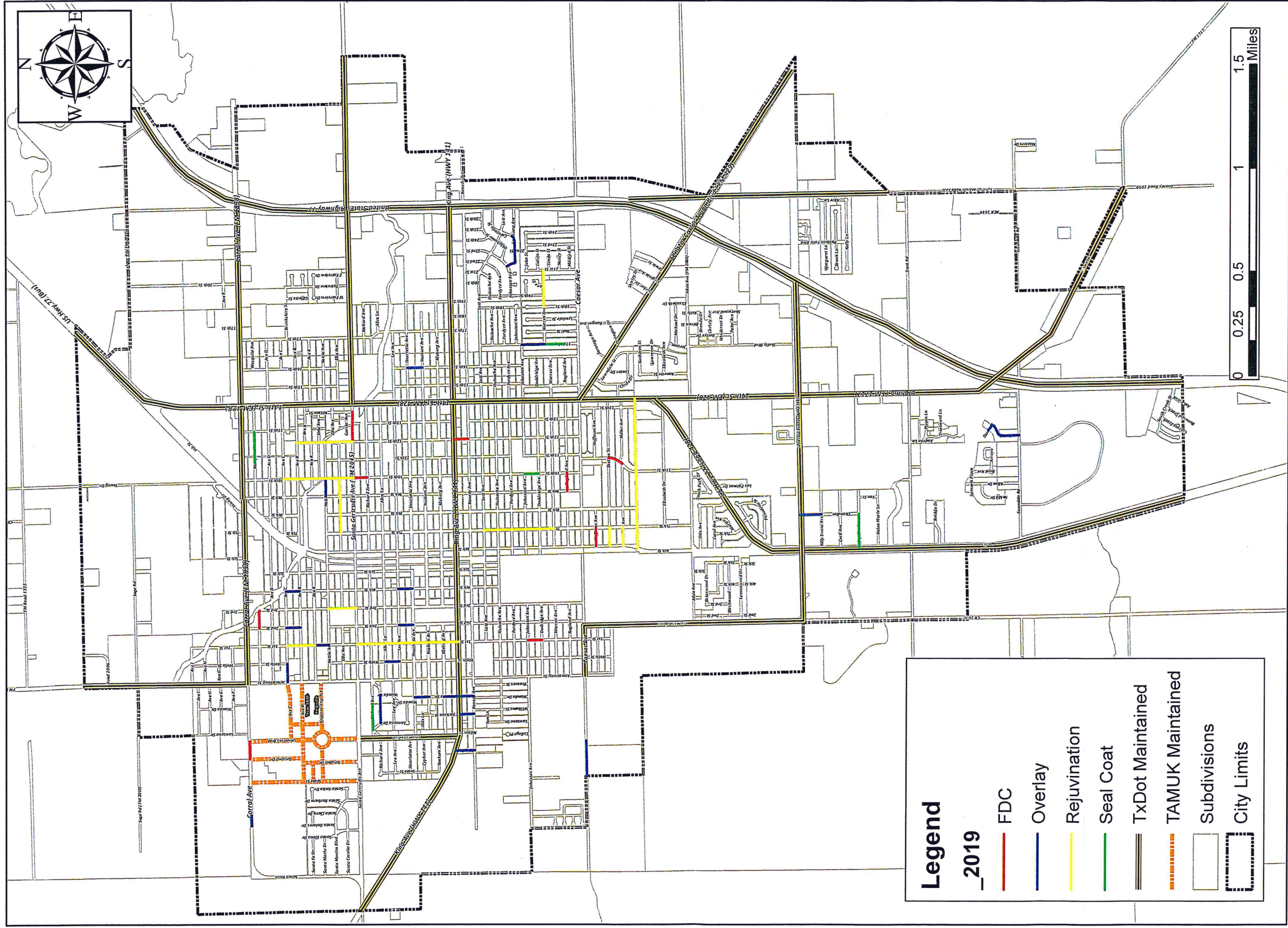
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Drawn By: Engineering Department

Last Update: 5/27/2015


Note:

# 2019 Street Construction

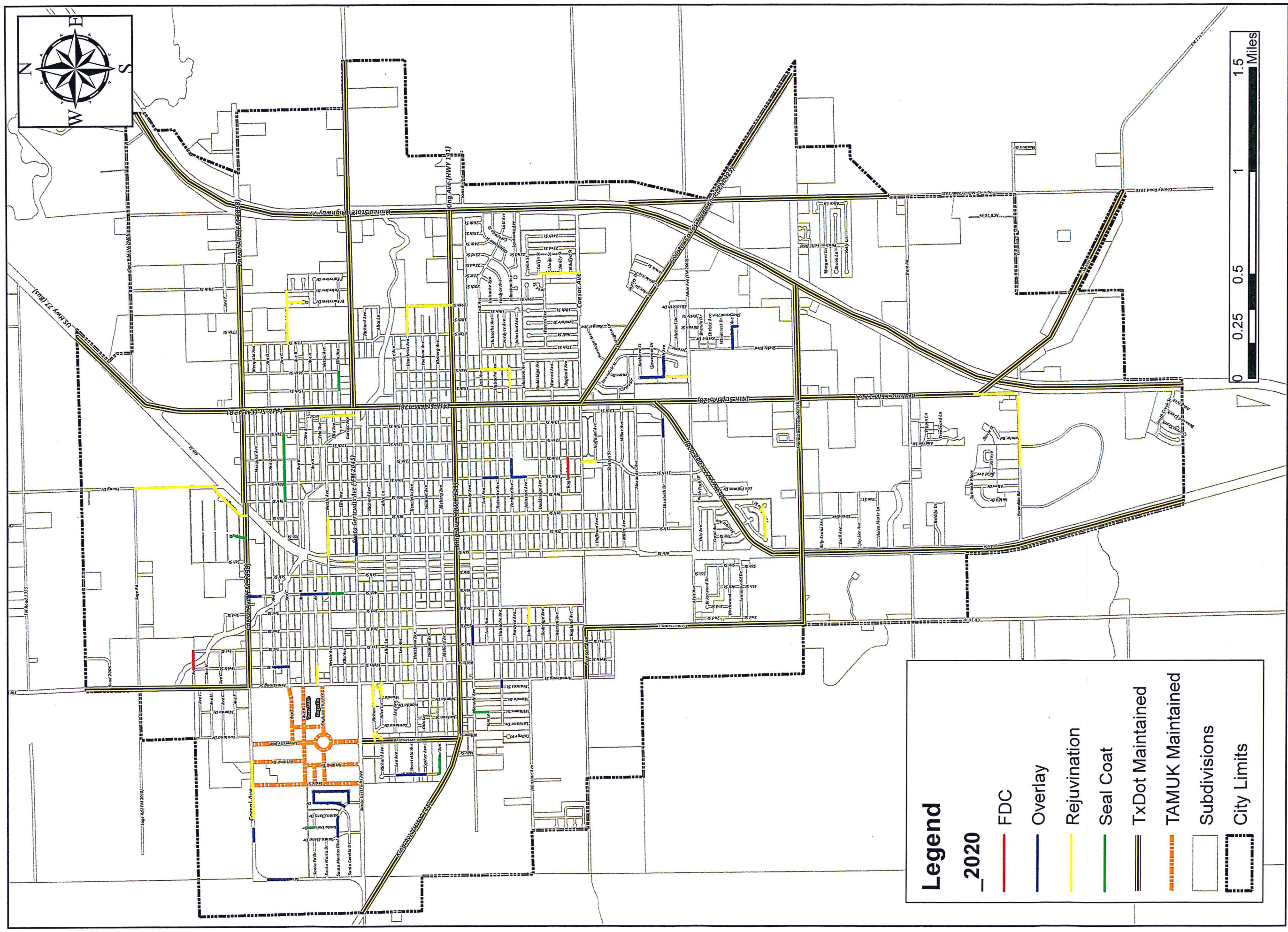


**Legend**

- \_2019**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits


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<p>Drawn By: Engineering Department</p> <p>Last Update: 5/27/2015</p> <p>Note:</p>	<p><b>5-5a</b></p>

# 2020 Street Construction



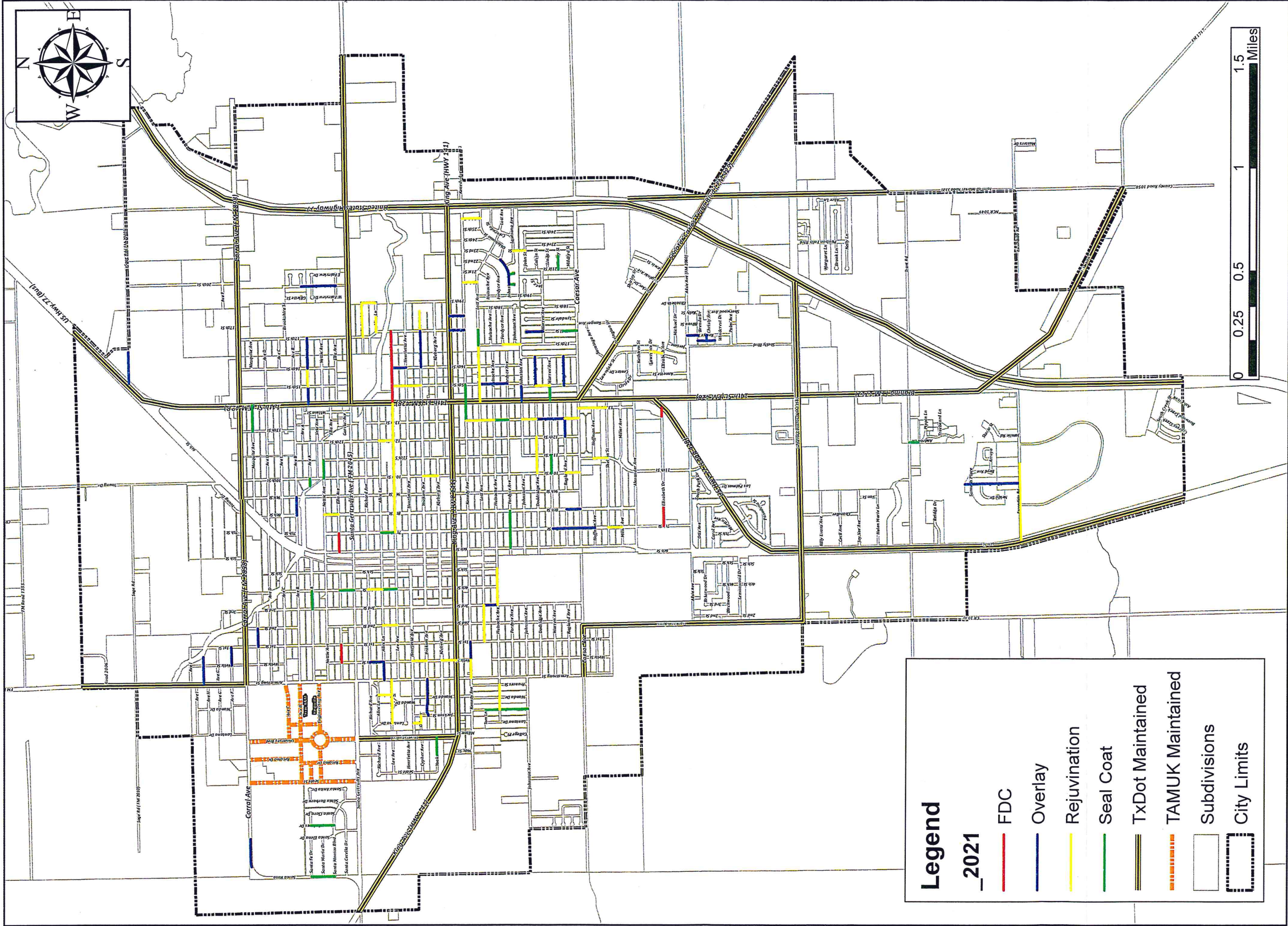
**Legend**

- \_2020**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

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<p>Drawn By: Engineering Department Last Update: 5/27/2015 Note:</p>	<p><b>5-6a</b></p>



# 2021 Street Construction



**Legend**

- \_2021**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

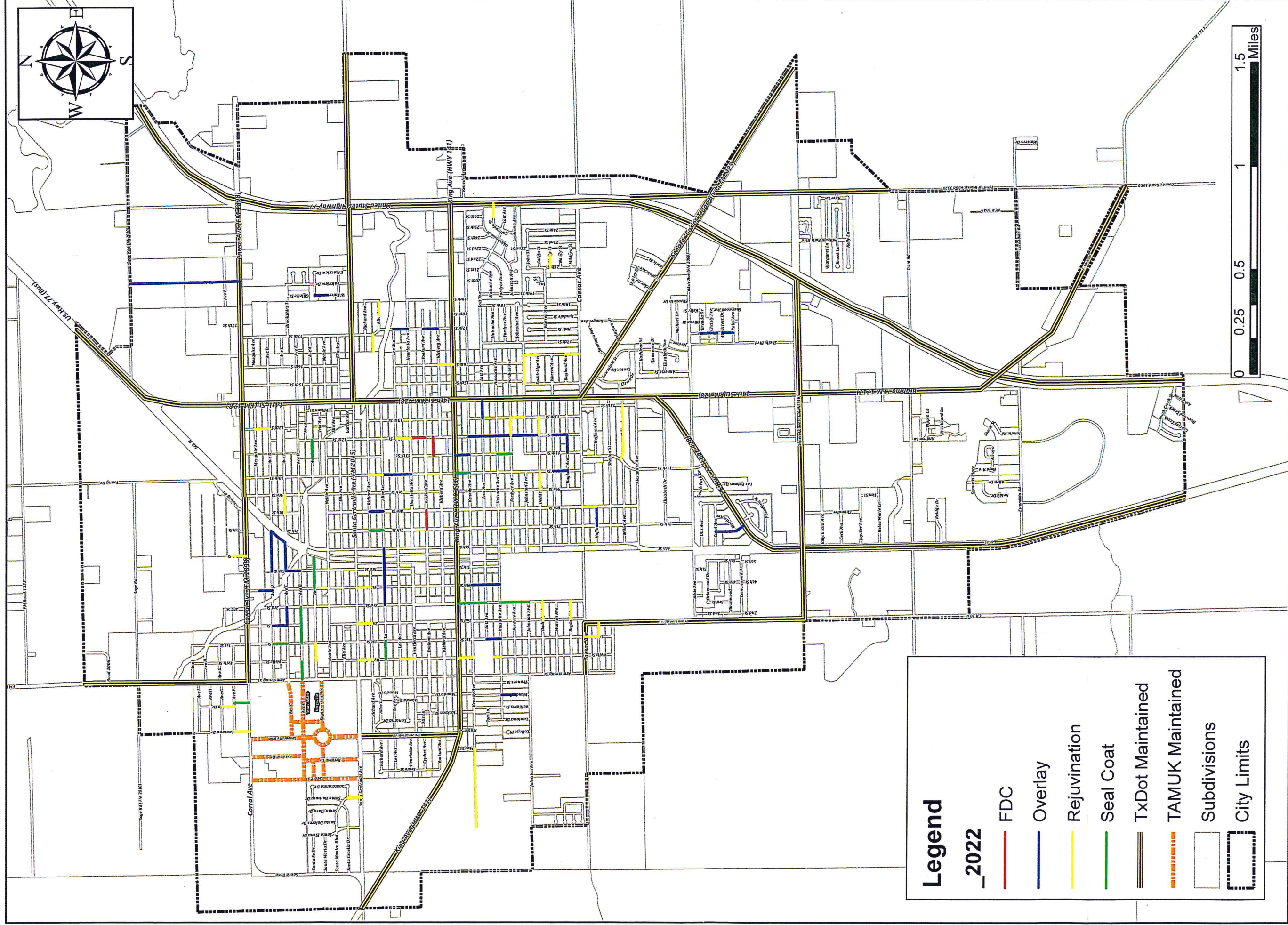
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 Last Update: 5/27/2015  
 Note:

**5-7a**

# 2022 Street Construction



**Legend**

- \_2022**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

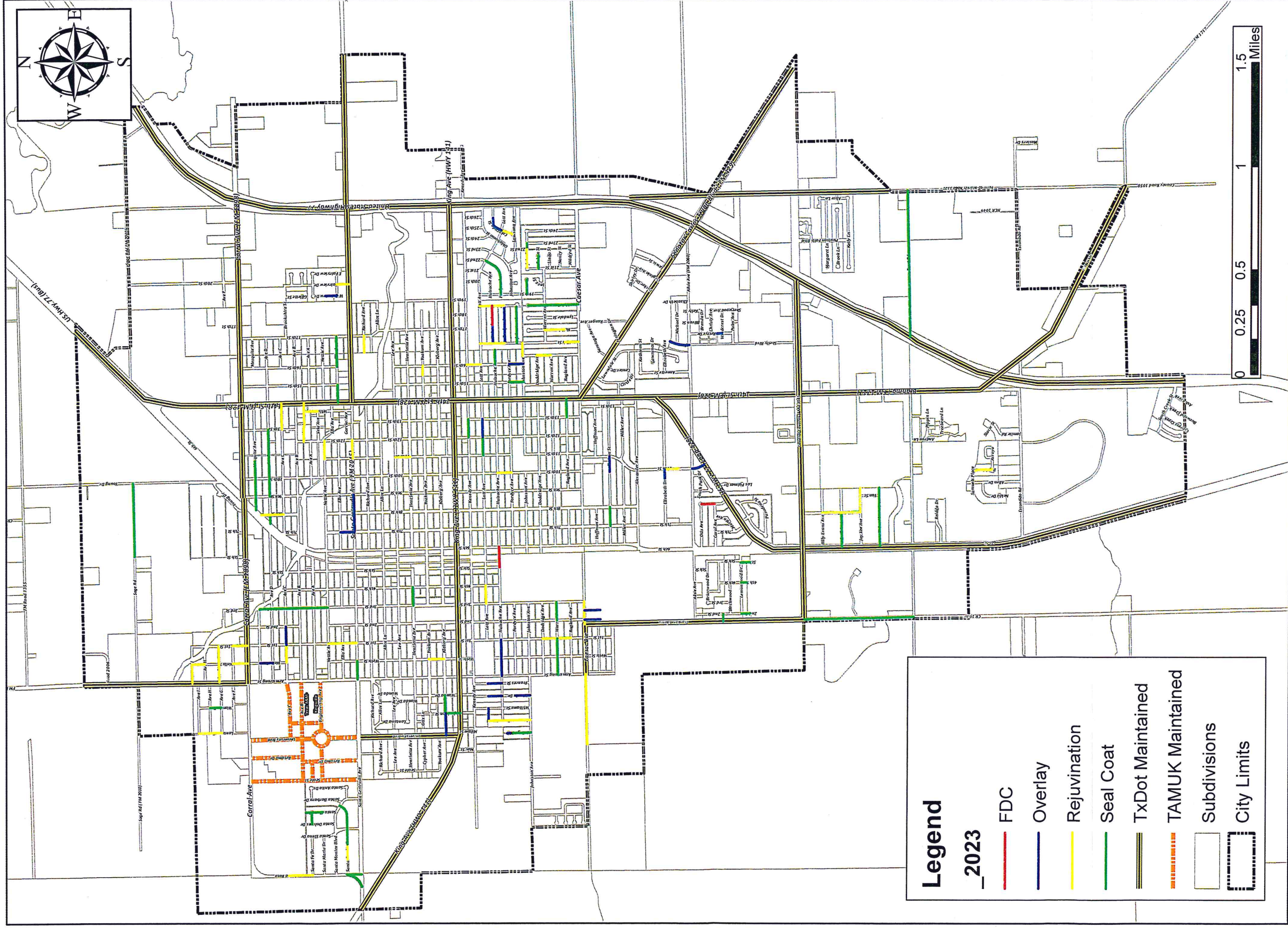
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 Last Update: 5/27/2015  
 Note:

# 2023 Street Construction



**Legend**

- \_2023**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department

Last Update: 5/27/2015

Note:

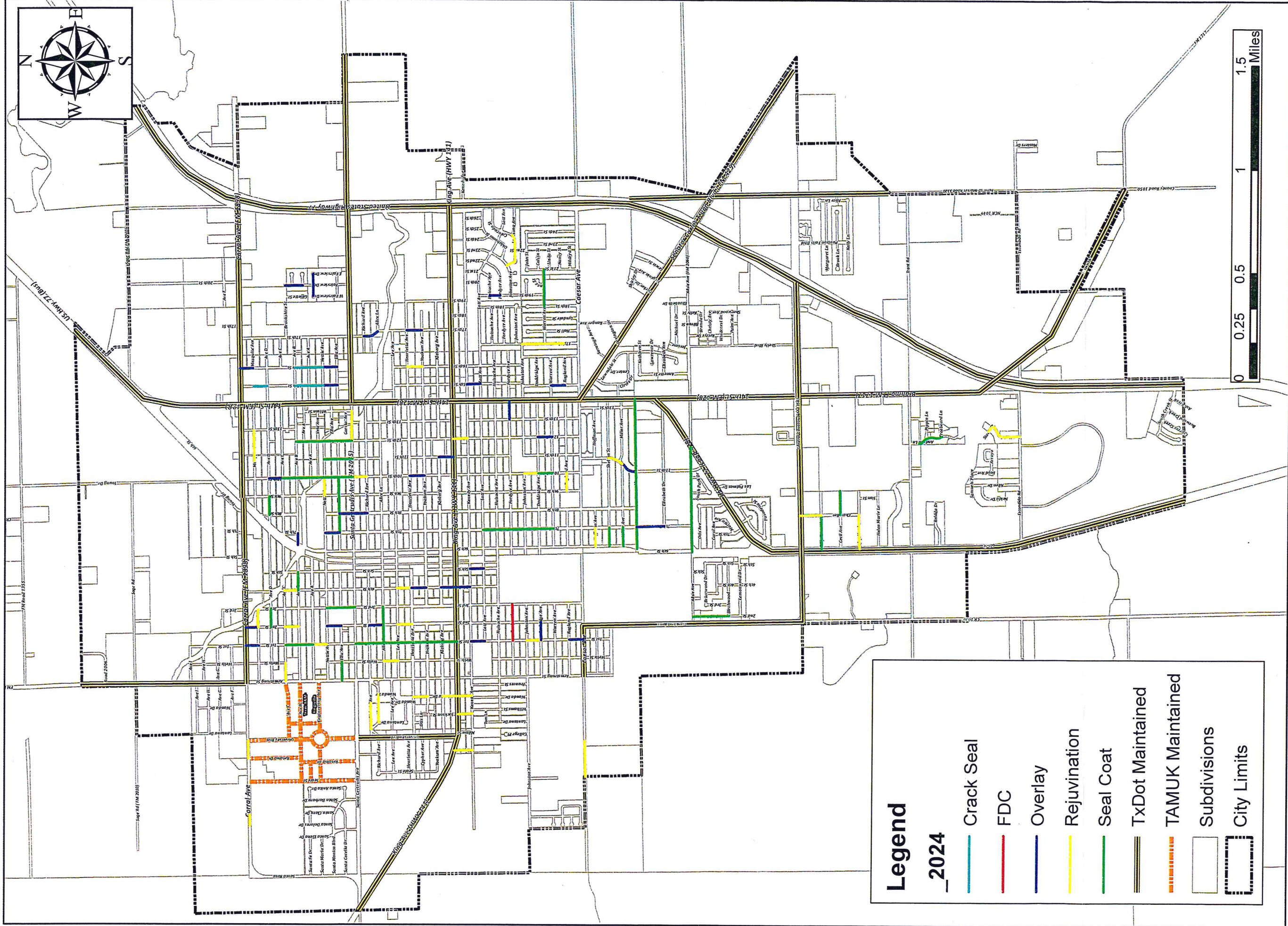
**5-9a**

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# 2024 Street Construction



**Legend**

**\_2024**

- Crack Seal
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

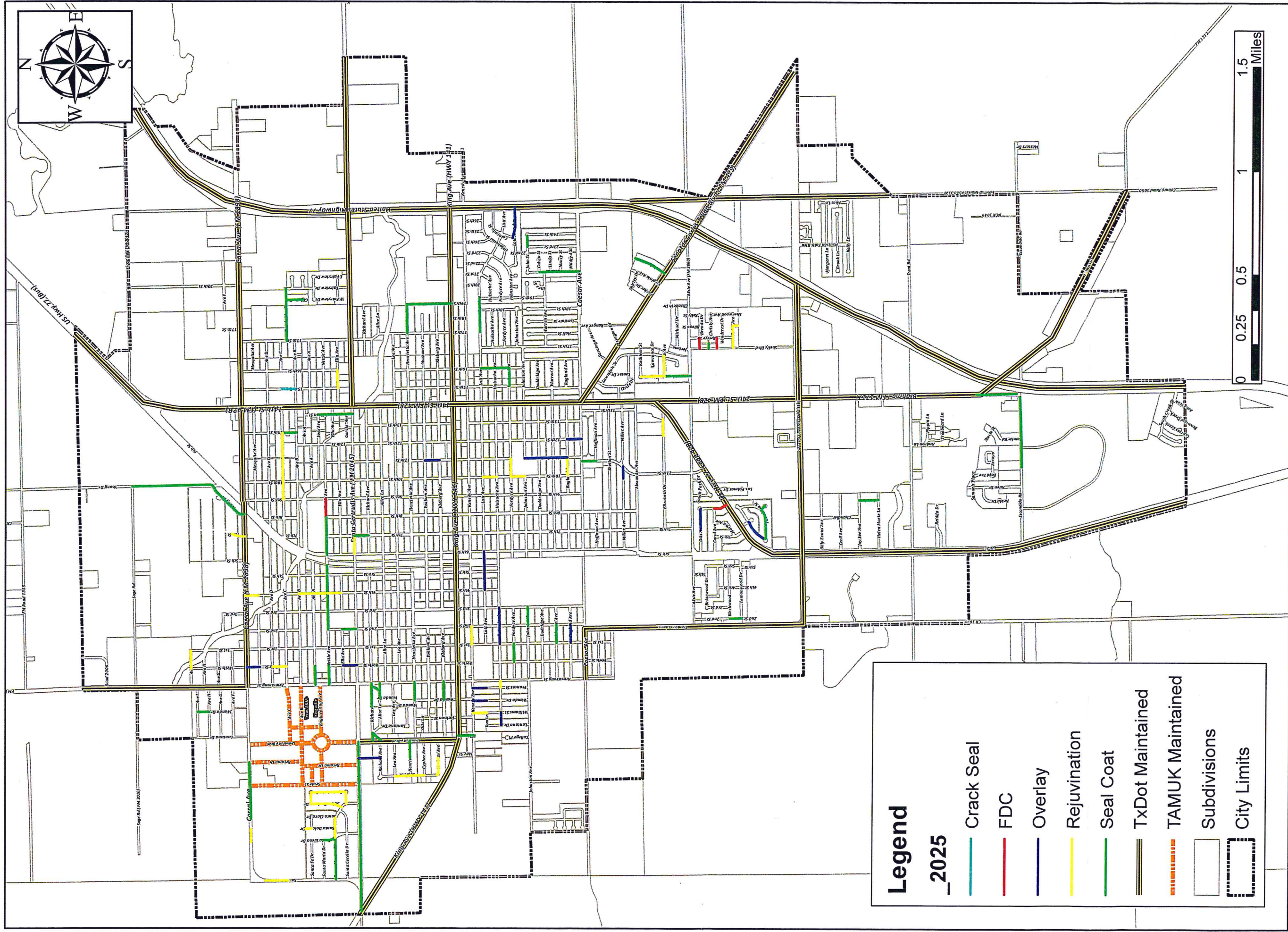
**5-10a**

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# 2025 Street Construction



**Legend**

**\_2025**

- Crack Seal
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

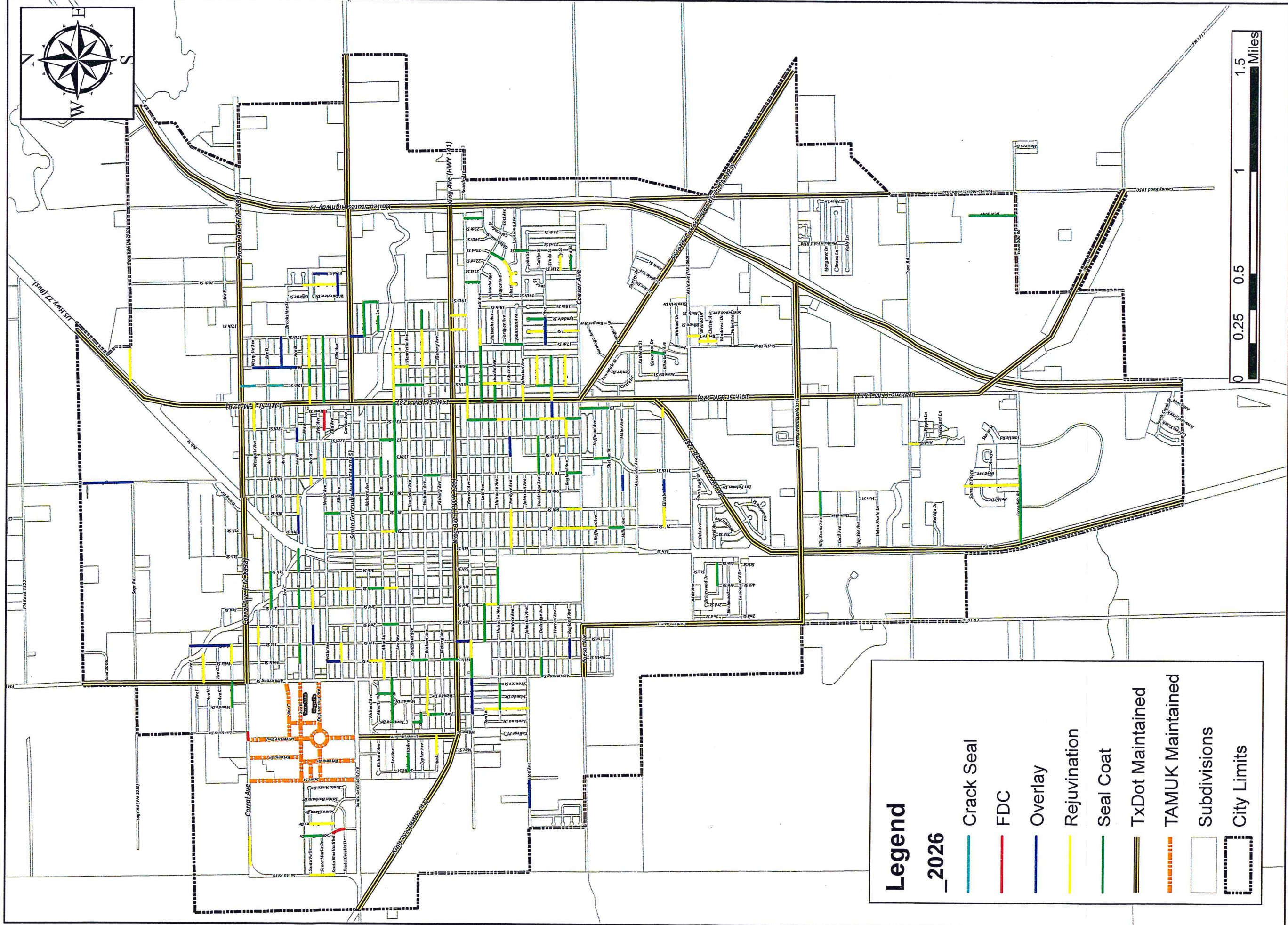
**5-11a**

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# 2026 Street Construction



**Legend**

**\_2026**

- Crack Seal
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

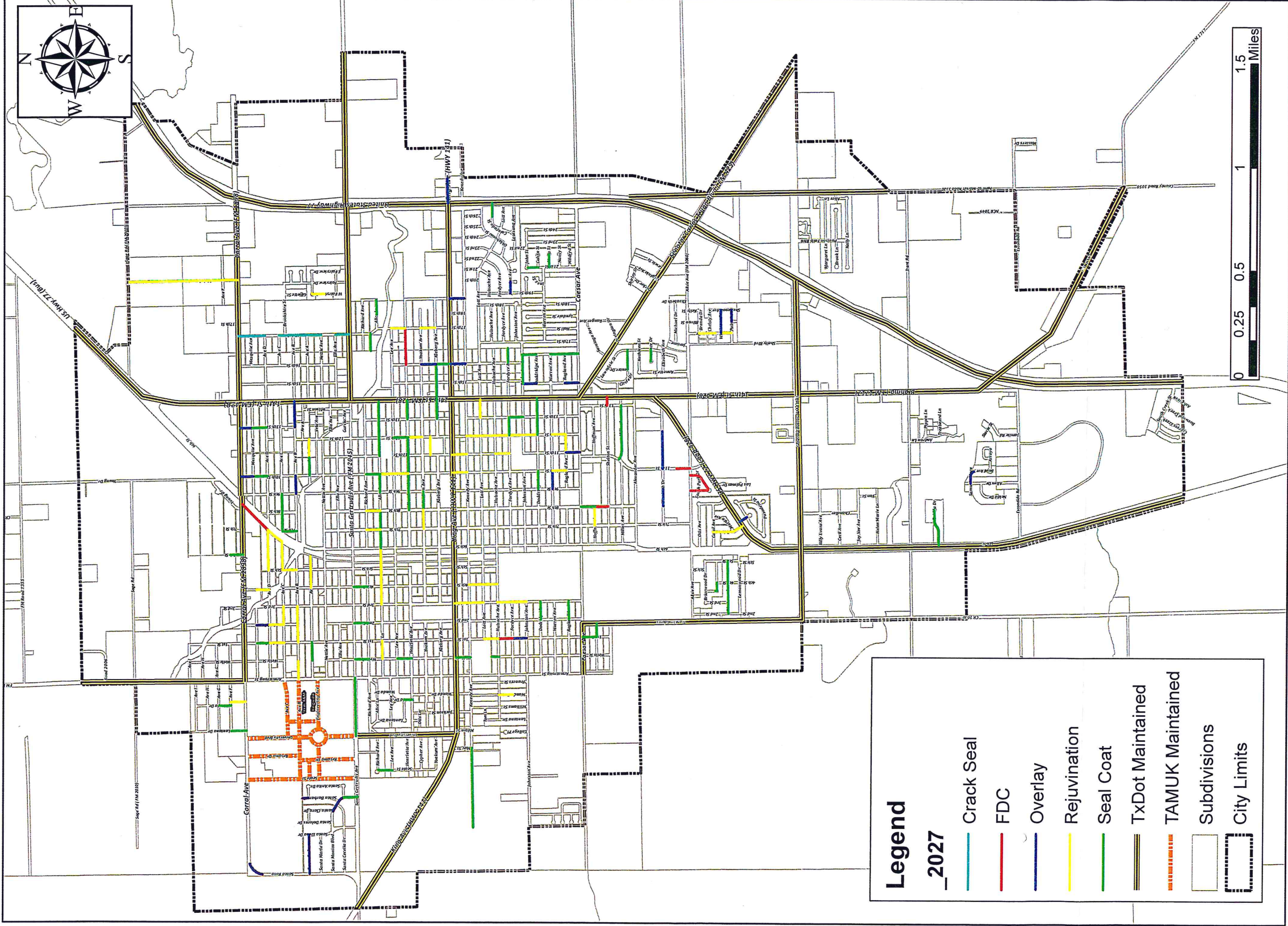
**5-12a**

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# 2027 Street Construction



**Legend**  
\_2027

-  Crack Seal
-  FDC
-  Overlay
-  Rejuvenation
-  Seal Coat
-  TxDot Maintained
-  TAMUK Maintained
-  Subdivisions
-  City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

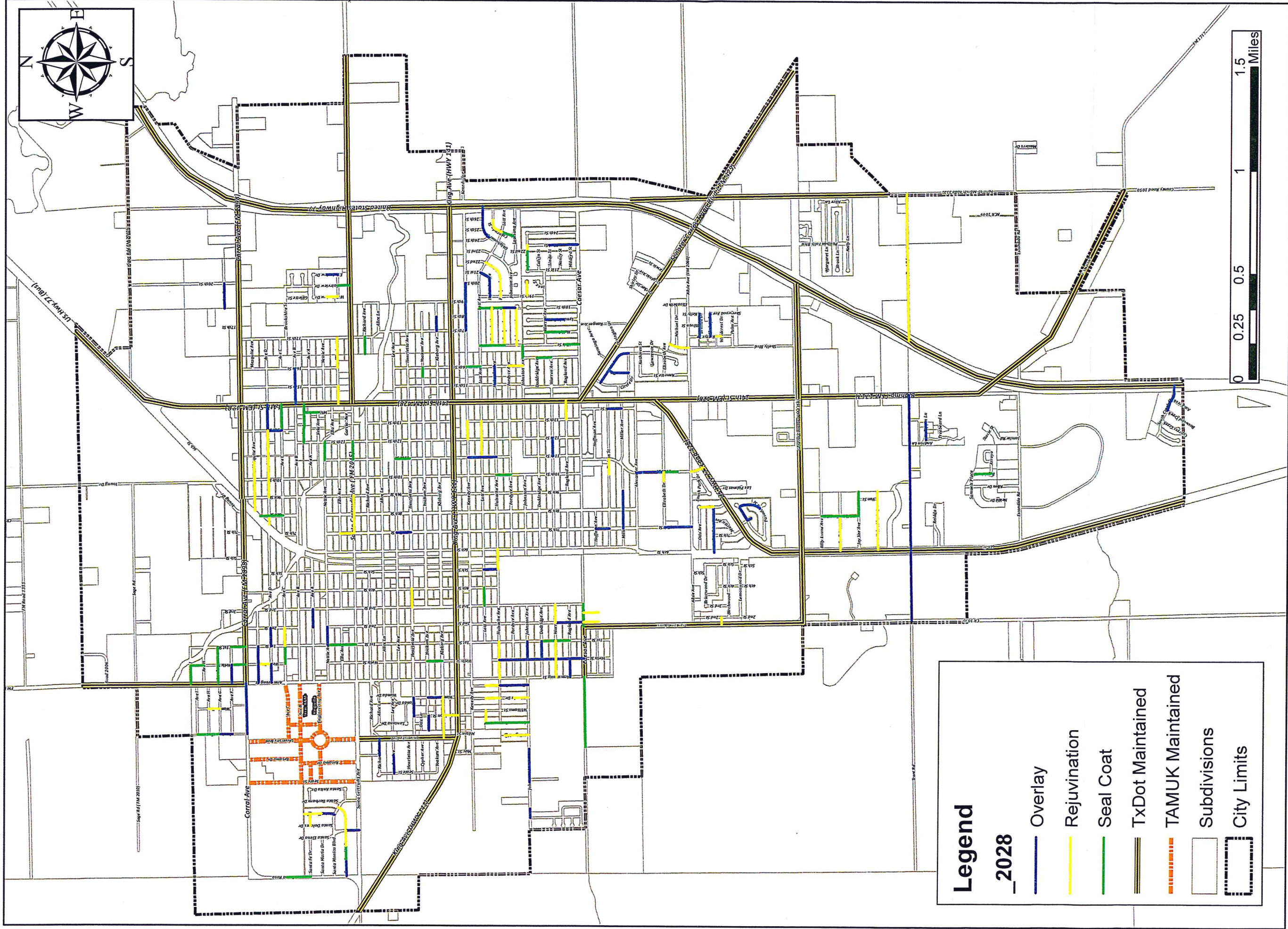
**5-13a**

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# 2028 Street Construction




**Legend**

- \_2028**
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

**5-14a**

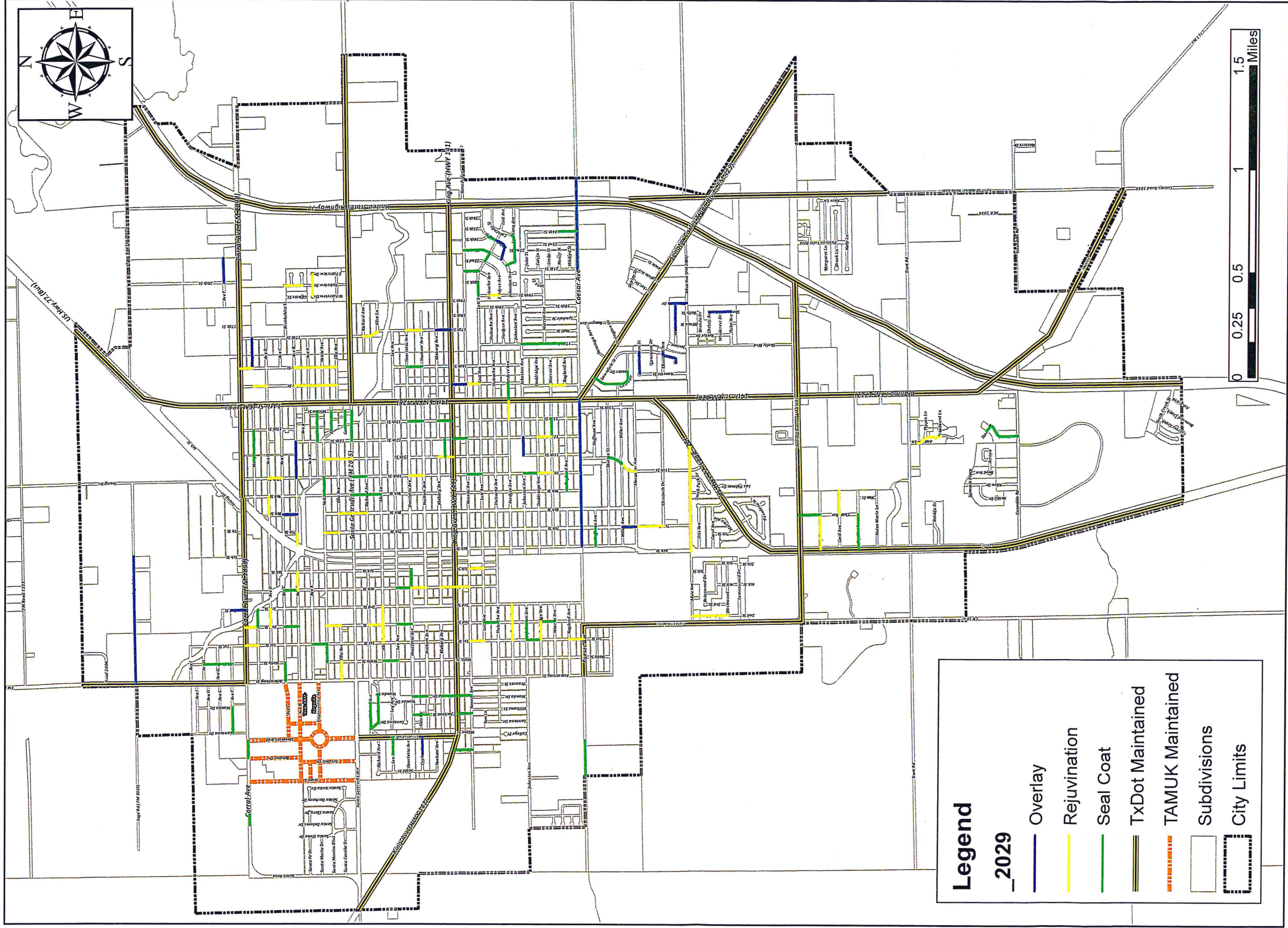
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# 2029 Street Construction



**Legend**

- 2029**
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

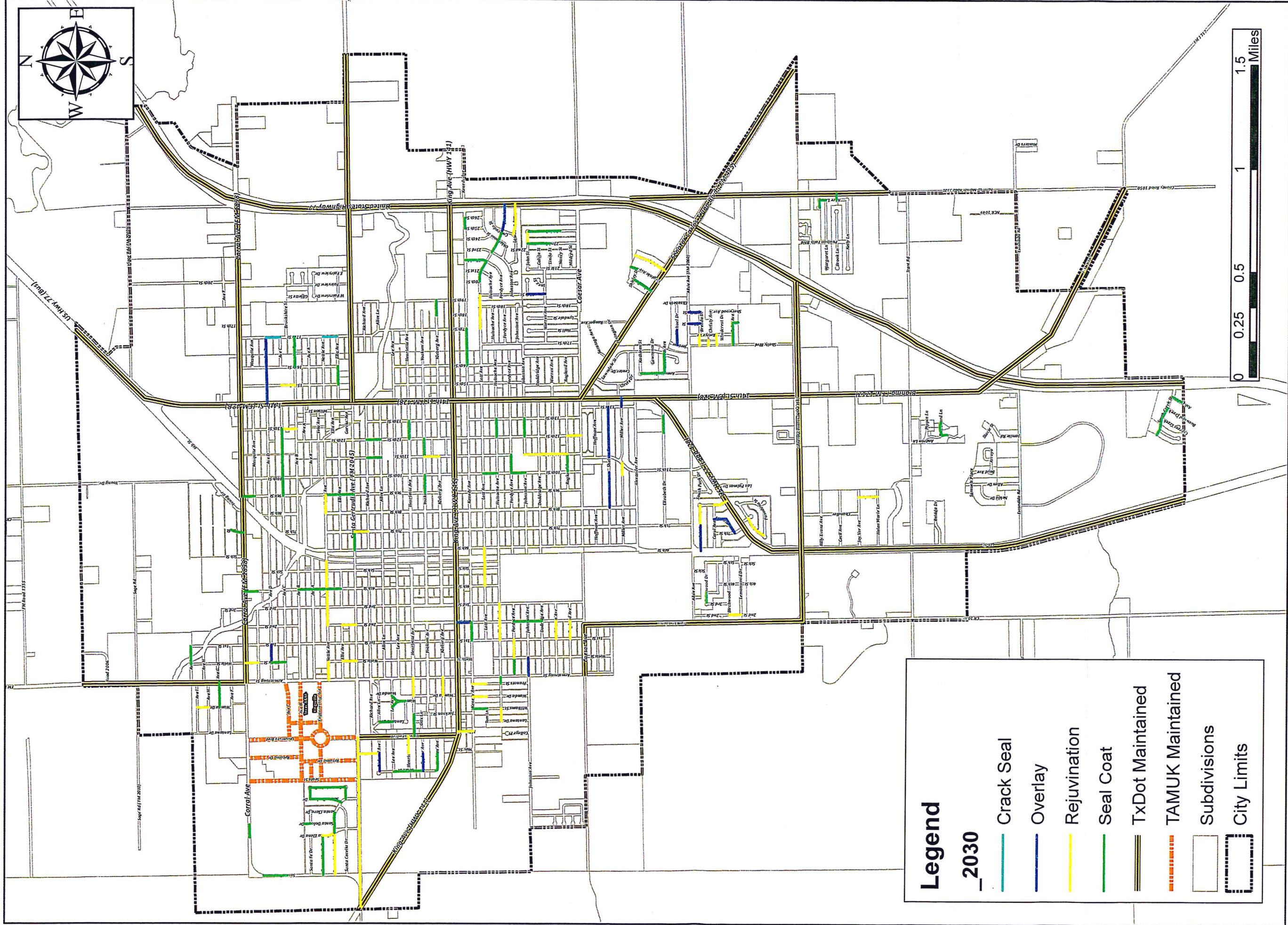
**5-15a**

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# 2030 Street Construction



**Legend**

**\_2030**

- Crack Seal
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

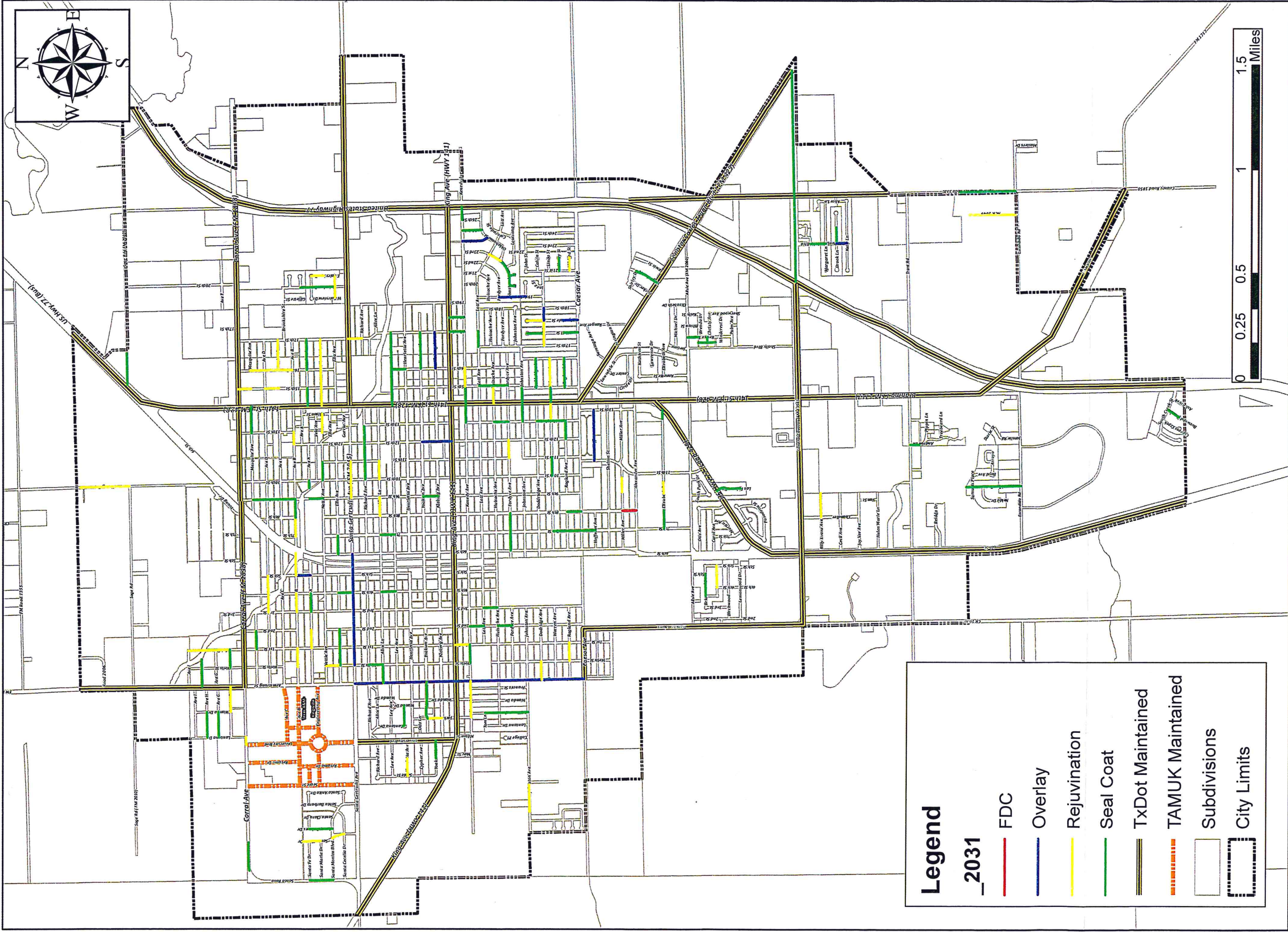
**5-16a**

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# 2031 Street Construction




**Legend**

	FDC
	Overlay
	Rejuvenation
	Seal Coat
	TxDot Maintained
	TAMUK Maintained
	City Limits

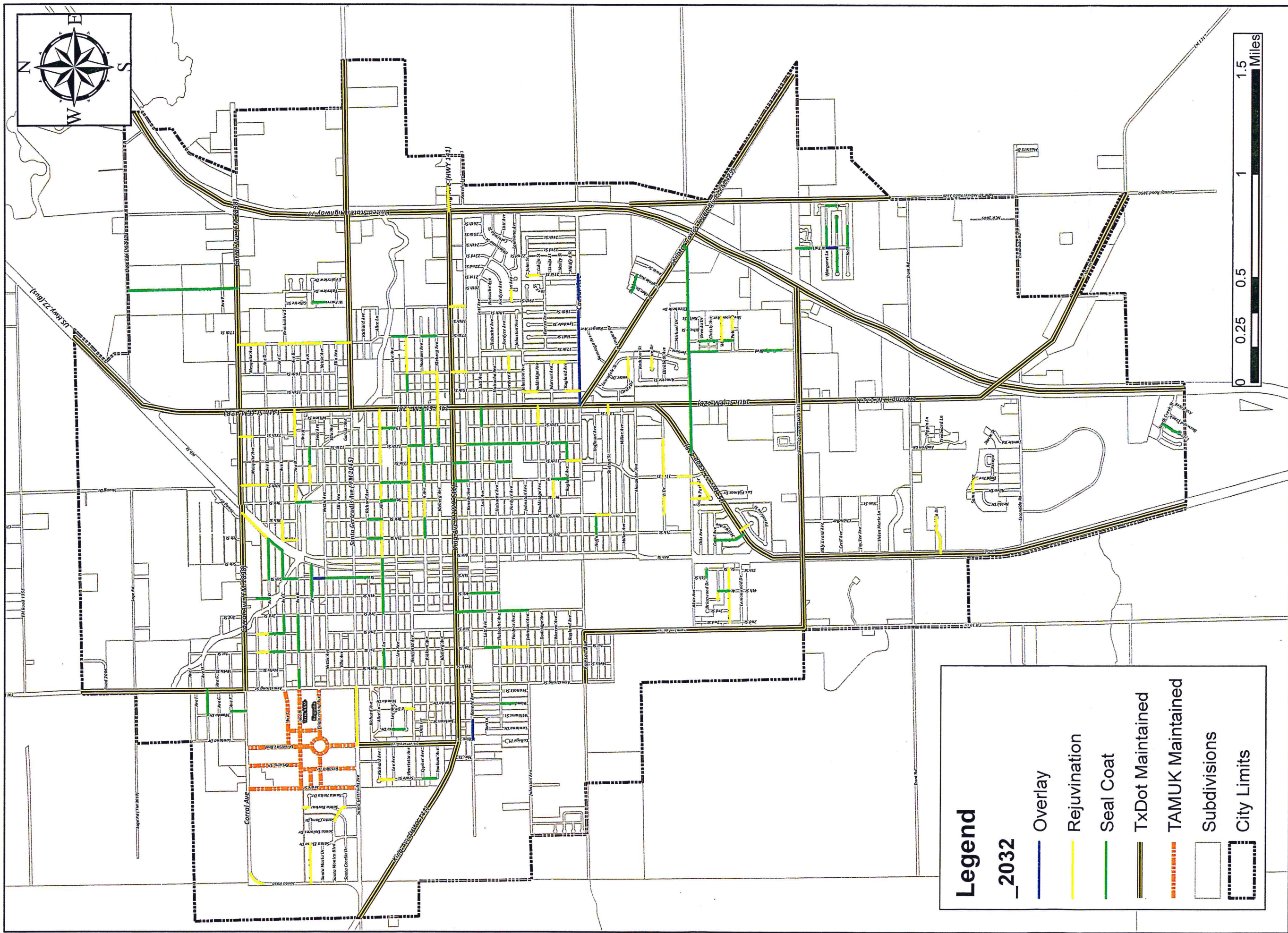
Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

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# 2032 Street Construction



**Legend**

- Overlay
- Rejuvination
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

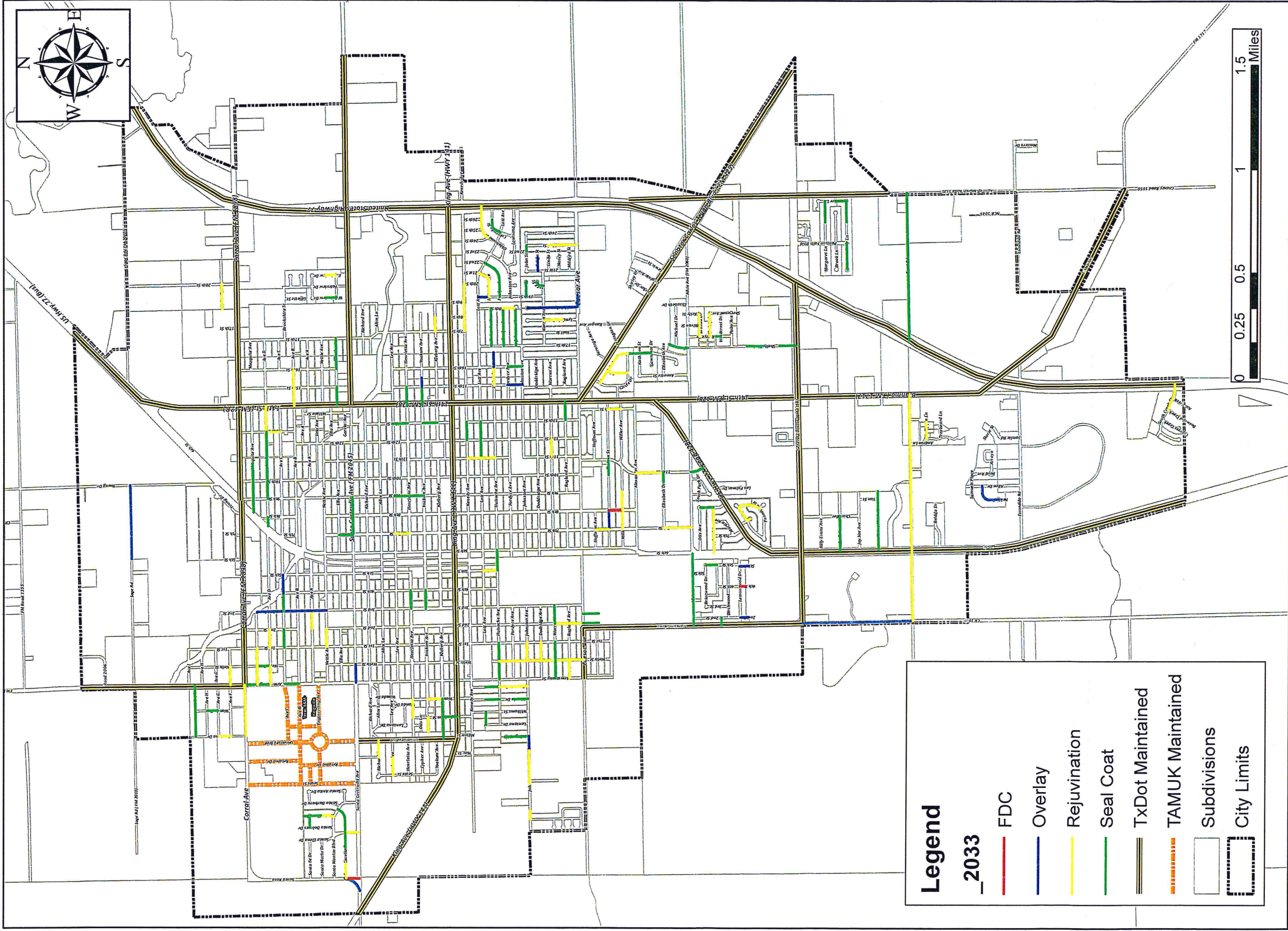
**5-18a**

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# 2033 Street Construction



**Legend**

- \_2033**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

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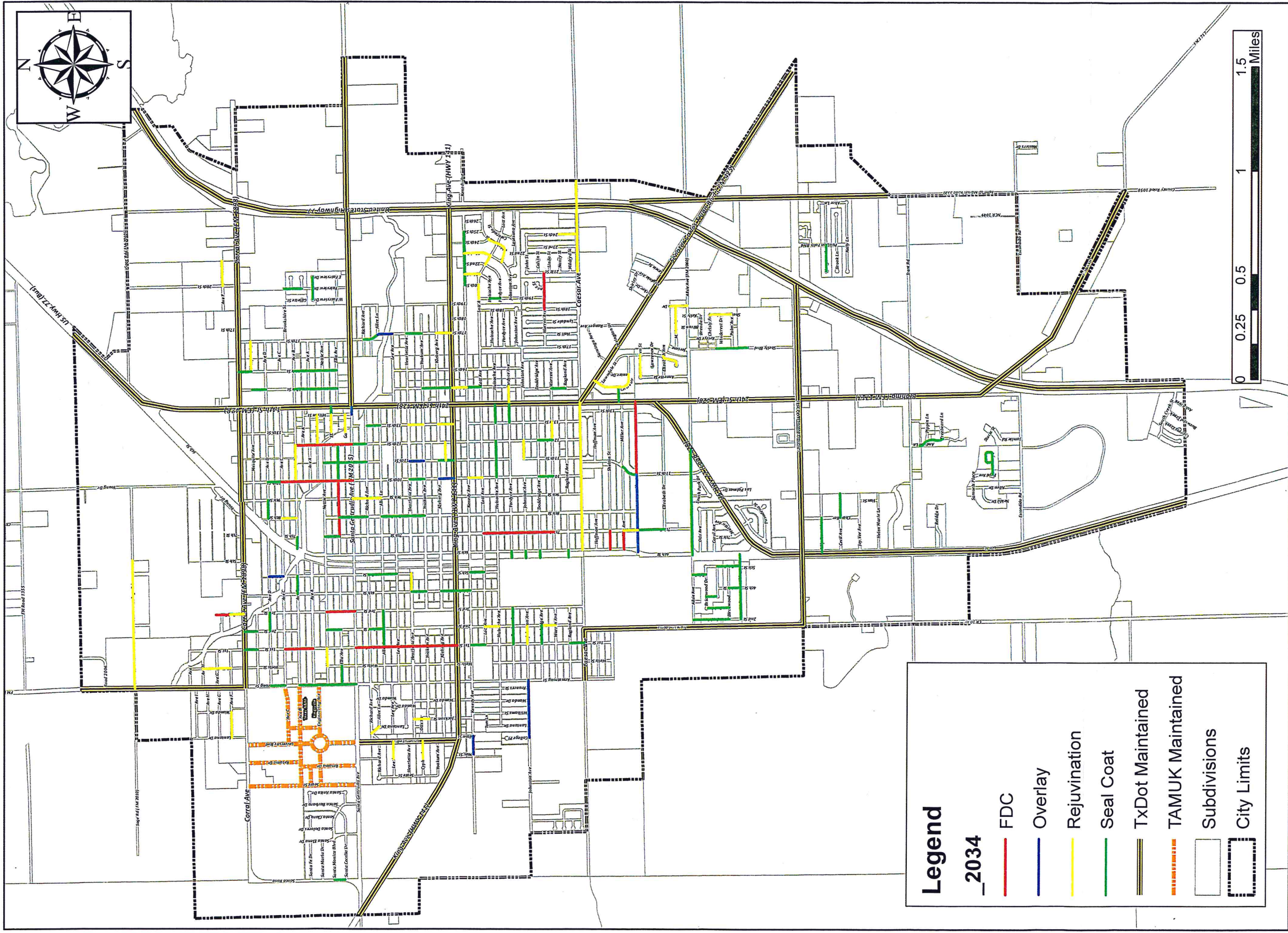


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Drawn By: Engineering Department  
 Last Update: 5/27/2015  
 Note:

**5-19a**

# 2034 Street Construction



**Legend**

- \_2034**
- FDC
- Overlay
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits

Drawn By: Engineering Department

Last Update: 5/27/2015

Note:

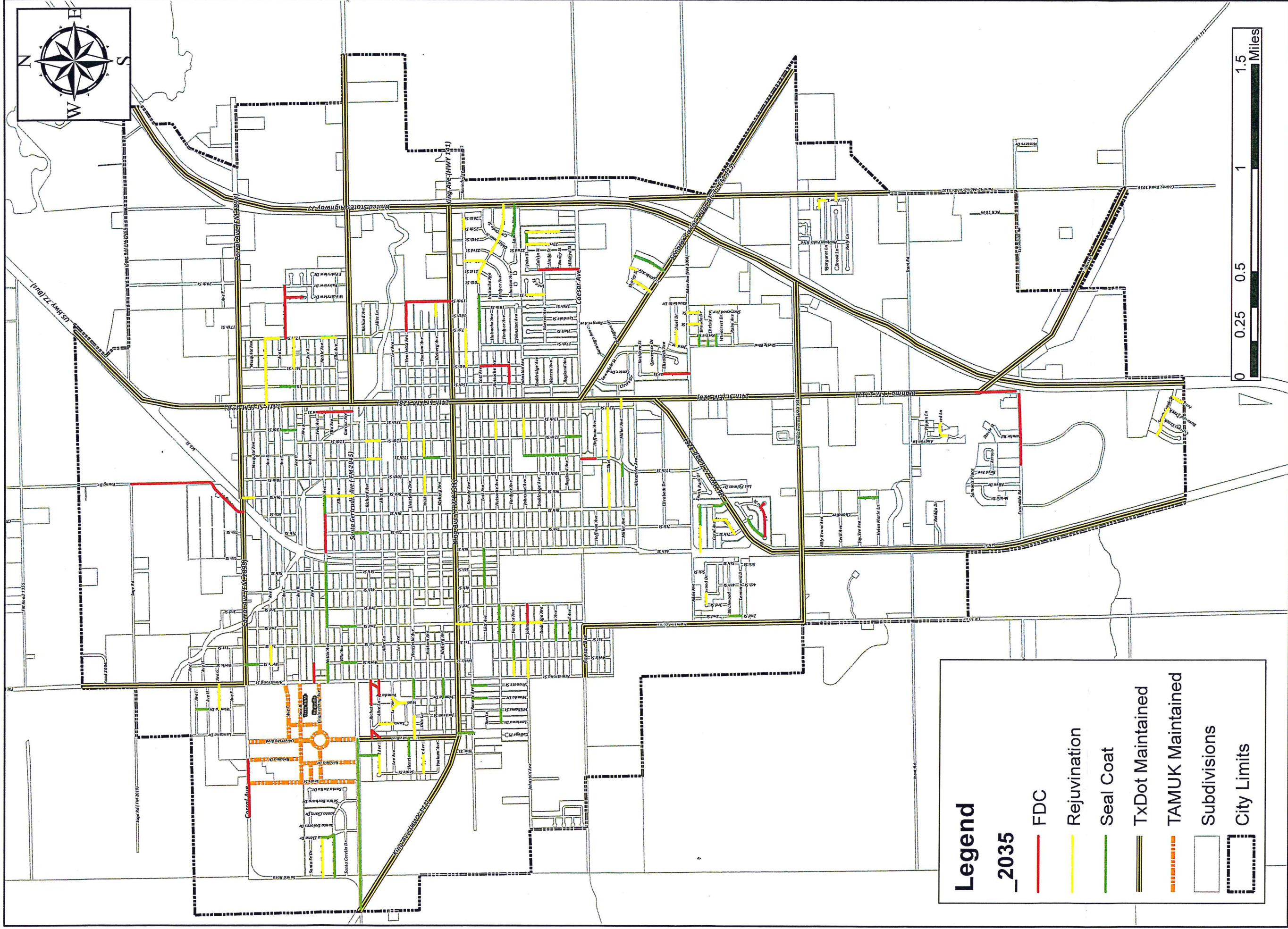
**5-20a**

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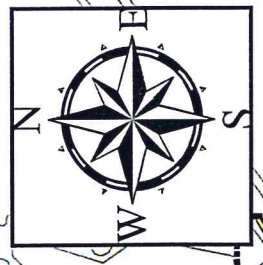
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# 2035 Street Construction



**Legend**

- 2035**
- FDC
- Rejuvenation
- Seal Coat
- TxDot Maintained
- TAMUK Maintained
- Subdivisions
- City Limits



Drawn By: Engineering Department

Last Update: 5/27/2015

Note:

**5-21a**

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## 6.) Plan Cost

	<u>page</u>
1. Yearly cost (graph)	6-1
2. Current cost estimates	6-2



**Preliminary Cost Estimate for 1 block of street construction/repair:**

Length 400 feet  
 Width 28  
 Area 11200 SF  
 Area 1244 SY

**1. Asphalt Pavement (Project done by the City crews)**

Items	Unit	Unit Price
Hot-Mix	110 lb/SY/in	
Crushed limestone	1.30 ton/cu. Yd (not compacted)	
Crushed limestone (compaction ratio)	1.30 (compacted/uncompacted)	
Primer-MC-30 (materials)	\$3.39 /gal	
Application Rate	0.15 gal/SY	
Primer- MC-30 (by City employee)	\$0.51 /SY	
Primer- MC-30 (by contractor)	\$1.50 /SY	
Binding materials -AC-5	\$2.46 /gal	
Binder Application Rate for #3 Rock	0.30 gal/SY	
Binder Application Rate for # 4 Rock	0.25 gal/SY	
Binder Application Ratefor # 5 Rock	0.30 gal/SY	
#3 Rock	17.11 Ton	
#4 Rock	13.69 Ton	
#5 Trap Rock	12.44 Ton	
Depth of Asphalt	2.0 inch	
Depth of Asphalt	1.5 inch	
Depth of Crushed Limestone	6.00 inch	
<p>For #3 Rock 1 Cu. Yd of Rock covers 80 SY of Surface, 1 Cu. Yd of Rock weighs about 1.1 Ton                      For #4 Rock 1 Cu. Yd of Rock covers 100 SY of Surface, 1 Cu. Yd of Rock weighs about 1.1 Ton                      For #5 Rock 1 Cu. Yd of Rock covers 100 SY of Surface, 1 Cu. Yd of Rock weighs about 1.2 Ton</p>		
<b>a. Hot-mix Pavement</b>		
6" limestone	350.52 Ton	\$22.75
Primer - MC 30	1,244 SY	\$1.50
Binding Materials - AC-5	373.33 gal	\$2.46
#5 Trap Rock	12.44 Tons	\$60.00
2" HMAC	136.89 Ton	\$77.19
1.5" HMAC	102.67 Ton	\$77.19
		\$7,974.30
		\$1,866.67
		\$918.40
		\$746.67
		\$10,566.45
		\$7,924.84
		\$12,433.12
		\$9,791.51
		\$20,407.42
		\$17,765.80
		<b>\$22,072.48</b>
2" - HMAC with Existing Flex. Base Materials plus Primer (\$10,566.45+1,866.67)		
1.5" - HMAC with Existing Flex. Base Materials plus Primer (\$7924.84+\$1,866.67)		
2" - HMAC with New 6" Flex. Base Materials plus Primer (\$10,566.45+\$7,062.95+\$1,866.67)		
1.5" - HMAC with 6" New Flex. Base Materials (\$7,924.84+\$7,062.95+\$1,866.67)		
<b>2.0" - HMAC with 6" New Flex. Base Materials plus #5 trap rock seal &amp; Primer</b>		
(\$10,566.45+\$7,062.95+\$1,866.67+\$918.40+\$746.67)		
<b>1.5" - HMAC with 6" New Flex. Base Materials plus #5 trap rock seal &amp; Primer</b>		
(\$7,924.84+\$7,062.95+\$1,866.67+\$918.40+\$746.67)		

\*Not Included:  
 6" lime stabilization for the Subgrade

**b. Seal Coat Pavement**

	Area -SY				
6" limestone	1244	350.52 Ton	\$22.75	\$7,974.30	
Primer - MC 30	1244	\$1.50 /SY		\$1,866.67	
#3 Rock	1244	17 Ton	60	\$1,026.67	
#4 Rock	1244	14 Ton	60	\$821.33	
AC-5 for 1st Course @ 0.30 gal/SY	1244	373 gal	2.46	\$918.40	
AC-5 for 2nd Course @ 0.25 gal/SY	1244	311 gal	2.46	\$765.33	
<b>2- Course Seal Coat with 6" Flex. Base Material &amp; Primer</b>				<b>\$13,372.70</b>	
Total Cost for Two Course Sealcoat				\$5,398.40	
Total Cost for One Course Sealcoat				\$3,606.40	

\*Not Included:

6" lime stabilization for the Subgrade

**2. Concrete Pavement (Construction done by the Contractor)**

	Area -SY				
6" limestone	1244	350.52 Ton	\$22.75	\$7,974.30	
6" -3600 psi Concrete with #4 bar @ 18" OCEI	1244		\$50.00	\$62,222.22	
<b>*Not Included:</b>					
6" lime stabilization for the Subgrade					
<b>Total</b>				<b>\$70,196.52</b>	

**3. Maintenance (by the Contractor)**

Fog Sealing Application (TRMISS)	\$2.00 /SY				
Hot Pour Crack Sealing	\$2.00 /LF				
Cost Fog Sealing			\$2,488.89		
Cost for Hot Pour Crack Sealing			\$800.00		

4. Construction and maintenance cost for 15 years per block of street

Year	Hot-mix Reconstruction	Seal-Coat Reconstruction	Concrete Reconstruction
0	\$19,430.87 New Road	\$13,372.70 New Road	70196.5
1			
2			
3			
4	\$3,288.89 Fog Seal Plus Crack seal	\$5,398.40 2-Course Seal Coat	
5			
6			
7			
8	\$3,288.89 Fog Seal Plus Crack seal	\$5,398.40 2-Course Seal Coat	
9			
10			
11			
12	\$3,288.89 Fog Seal Plus Crack seal	\$5,398.40 2-Course Seal Coat	
13			
14			
15	Reconstruction	Reconstruction	Reconstruction
<b>Total</b>	<b>\$29,298</b>	<b>\$29,568</b>	<b>\$70,197</b>

## 7.) Financing

page

Review Q&A

- Financing Options for City Streets 7-1
- Proposed Financing for Street Improvement Program (20 years) 7-3
- 2014 Property Tax Rates 7-4
- Tax Rate Comparison – Texas Cities 7-5
- 2012 Storm Water Fee Survey 7-9
- Debt Capacity Schedules 7-11
- General Fund Debt Subsidies and the Impact on Street Funding 7-13

# City of Kingsville, Texas

## Financing Options for City Streets

FY 2016 - FY 2035 (20 Fiscal Years)

### 1 Increased Property Tax

#### 2015 Computations:

Net Taxable Value	\$728,913,665
Net Taxable Value / \$100	\$7,289,137
Amount Collectible @ 98%	\$7,143,354
I&S Tax Rate	\$0.18662
M&O Tax Rate	<u>\$0.65558</u>
Total Tax Rate	\$0.84220
Total Expected Tax	\$6,016,133
Tax Revenue for Each One Cent (\$.01) of Tax Rate Increase	\$71,432

#### Selected Revenue and Tax Rate Increase Options

Total Tax Rate Increase for \$2,100,000 Increase Revenue	\$0.29399
Total Tax Rate Increase for \$2,100,000 x 75% = \$1,575,000 Increase Revenue	\$0.22049
Total Tax Rate Increase for \$2,100,000 x 50% = \$1,050,000 Increase Revenue	\$0.14699
Total Tax Rate Increase for \$2,100,000 x 50% = \$525,000 Increase Revenue	\$0.07350

Total Recommended Tax Rate Increase to Generate \$0 Increase Tax Revenue	\$0.00000
--	-----------

### 2 Increased Stormwater Fee

#### 2015 Computations:

Storm Water Fee Revenue - 6,914 Billed REU's @ \$15/REU/Year	\$102,843
Storm Water Fee Revenue - 8,079 Billed REU's @ \$15/REU/Year	<u>\$119,522</u>
Total Storm Water Revenue from All Customers - <i>legally Restricted for Debt</i>	\$222,364

#### Selected Revenue and Stormwater Fee Increase Options

Total Increase for each (14,993) REU/Year for \$2,100,000 Increase Revenue	\$140.07
Total Increase for each (14,993) REU/Year for \$1,575,000 Increase Revenue	\$105.05
Total Increase for each (14,993) REU/Year for \$1,050,000 Increase Revenue	\$70.03
Total Increase for each (14,993) REU/Year for \$525,000 Increase Revenue	\$35.02

Total Recommended Annual Storm Water Fee Increase to Generate \$1,400,000 increase in Storm Water Revenue	\$93.38
---	---------

**3 Increased Revenue Using Solid Waste Fee Swaps**

Special Landfill Closure and Postclosure Care Fee Swap

Excess closure & postclosure care fee swapped for street improvements:

<u>Desc.</u>	<u>Accts.</u>	<u>Fee/Mo.</u>	<u># Months</u>	<u>Total Annual</u>	
Closure	7926	\$0.90	12	\$100,000	n/a
Street	7926	\$2.10	12	<u>\$200,000</u>	<u>\$200,000</u>
Total	7926	\$3.00	12	\$300,000	\$200,000

Note: A \$3 special fee for landfill closure and postclosure care was established more than a decade ago to ensure that the City has sufficient cash reserves to pay for legally mandated closure and post-closure care costs at the date of landfill closure (estimated 2060) and for thirty years thereafter (2090). Due to error by the engineering firm retained in 1999 to perform the Landfill Solid Waste Closure and Post Closure and Service Fee Report, the \$3 special fee was overstated and, accordingly, the City accumulated excess reserves, which was discovered when the City retained Naismith Engineering to update this Report in 2013. Rather than reduce the fee from \$3/mo. to \$0.90/mo., the City should consider converting this fee for street improvements, which will not cost residents any more than they are currently paying (\$3/mo). The special \$3/mo fee is included in the City's monthly utility billing and is assessed to all residential and commercial utility accounts/customers.

Special Solid Waste Debt Service Fee Swap

Excess landfill tipping fee used for debt service swapped for street improvements:

<u>Desc.</u>	<u>Accts.</u>	<u>Fee/Mo.</u>	<u># Months</u>	<u>Total Annual</u>	
Residential	6800	\$4.30	12	=	\$350,880
?	?	?	12	=	<u>\$131,320</u>
Total			12	=	\$482,200

Total Recommended Additional Revenue from Fee Swaps as Follows:

<u>Yr.</u>	<u>Annual Revenue</u>	<u>Annual Debt Serv</u>	<u>Dollars Remaining</u>	<u>Total by Year</u>
16	\$482,200	\$352,650	\$129,550	\$129,550
17	\$482,200	\$122,750	\$359,450	\$359,450
18	\$482,200	\$124,450	\$357,750	\$357,750
19	\$482,200	\$56,000	\$426,200	\$426,200
20	\$482,200	\$59,125	\$423,075	\$423,075
21	\$482,200	\$57,026	\$425,174	\$425,174
22	\$482,200	\$0	\$482,200	\$482,200

**4 Other?**

# Proposed Financing for Street Improvement Program

FY 2016 - FY 2035 (20 Fiscal Years)

No.	Year	Solid Waste				
		Storm Water	Fee Swaps		Transfer	Total
			Excess Landfill Closure	Excess Sanitation Debt		
1	2016	\$1,400,000	\$200,000	\$129,550	\$370,450	\$2,100,000
2	2017	1,400,000	200,000	359,450	140,550	2,100,000
3	2018	1,400,000	200,000	357,750	142,250	2,100,000
4	2019	1,400,000	200,000	426,200	73,800	2,100,000
5	2020	1,400,000	200,000	423,075	76,925	2,100,000
6	2021	1,400,000	200,000	425,174	74,826	2,100,000
7	2022	1,400,000	200,000	482,200	17,800	2,100,000
8	2023	1,400,000	200,000	482,200	17,800	2,100,000
9	2024	1,400,000	200,000	482,200	17,800	2,100,000
10	2025	1,400,000	200,000	482,200	17,800	2,100,000
11	2026	1,400,000	200,000	482,200	17,800	2,100,000
12	2027	1,400,000	200,000	482,200	17,800	2,100,000
13	2028	1,400,000	200,000	482,200	17,800	2,100,000
14	2029	1,400,000	200,000	482,200	17,800	2,100,000
15	2030	1,400,000	200,000	482,200	17,800	2,100,000
16	2031	1,400,000	200,000	482,200	17,800	2,100,000
17	2032	1,400,000	200,000	482,200	17,800	2,100,000
18	2033	1,400,000	200,000	482,200	17,800	2,100,000
19	2034	1,400,000	200,000	482,200	17,800	2,100,000
20	2035	1,400,000	200,000	482,200	17,800	2,100,000

# 2014 Governing Body Summary #1A\*

## Benchmark 2014 Tax Rates

### CITY OF KINGSVILLE

Date: 08/05/2014 03:08 PM

DESCRIPTION OF TAX RATE	TAX RATE PER \$100	THIS YEAR'S TAX LEVY**	ADDITIONAL TAX LEVY
Effective Tax Rate	\$0.82758	\$6,032,344 ✓	
One Percent \$100 Tax Increase***	\$0.83586	\$6,092,698	\$60,354
One Cent per \$100 Tax Increase***	\$0.837580	\$6,105,235	\$72,891
Notice & Hearing Limit****	\$0.82758	\$6,032,344	\$0
Rollback Tax Rate	\$0.89258	\$6,506,138	\$473,794
Last Year's Tax Rate	\$0.842200	\$6,138,911 ✓	\$106,567
Proposed Tax Rate	\$0.84220	\$6,138,911	\$106,567

\*These figures are provided as estimates of possible outcomes resulting from varying the tax rate. Please be aware that these are only estimates and should not be used alone in making budgetary decisions.

\*\*Tax levies are calculated using line 19 of the Effective Tax Rate Worksheet and this year's frozen tax levy on homesteads of the elderly or disabled.

\*\*\*Tax increase compared to effective tax rate.

\*\*\*\*The Notice and Hearing Limit is the highest tax rate that may be adopted without notices and a public hearing. It is the lower of the rollback tax rate the effective tax rate. 0



# Tax Rate Comparison - Texas Cities (1)

Issuer Alice (7)	M&O Rate (2)	I&S Rate (2)	Total Tax	Taxable Assessed Valuation (2)	Population (3)	Rating (4)	Percent of Principal Paid in 5 Years (5)	Sales Taxes (6)			Administrative Charge / Overhead Allocation / Fee in Lieu of Taxes / Subsidization (5)			General Fund Expenses Per Capita \$
								Receipts	Tax Rate	Per Capita Sales Tax	Receipts	Tax Rate	Equivalent	
Alice (7)	\$0.1216	\$0.1673	\$0.2889	\$1,078,712,567	19,104	A1 / A+	22.84%	\$14,510,444	1.3452	\$ 759.55	\$ 230,622	\$0.0214	799	
Alvin	0.7140	0.1300	0.8440	914,783,133	24,774	Aa3 / A+	38.53%	1,820,594	0.1990	73.49	499,502	0.0546	455	
Angleton (8)	0.5736	0.1499	0.7235	687,097,063	19,280	Aa3 / A+	47.70%	3,035,171	0.4417	157.43	141,913	0.0207	459	
Balch Springs	0.6619	0.1181	0.7800	644,517,323	24,211	A-/Baa2 / A-	33.70%	4,047,618	0.6280	167.18	15,546	0.0024	446	
Belton	0.5397	0.1153	0.6550	766,775,269	18,665	AA-	9.52%	2,323,032	0.3030	124.46	322,100	0.0420	527	
Big Spring	0.7047	0.2448	0.9495	546,733,594	27,364	A1 / A+	30.04%	5,914,200	1.0817	216.13	905,250	0.1656	577	
Brownwood	0.5972	0.1480	0.7452	717,255,045	20,402	Baa1	51.64%	4,360,548	0.6079	213.73	2,000,000	0.2788	751	
Denison	0.5710	0.0823	0.6534	1,018,556,350	22,784	A1 / A+ / AA-	22.15%	3,746,744	0.3678	164.45	902,066	0.0886	959	
Eagle Pass (9)	0.3250	0.1230	0.4480	1,097,861,872	27,183	A2 / A+	24.64%	4,157,872	0.3787	152.96	5,455,587	0.4969	545	
<b>Kingsville (8)</b>	<b>0.6380</b>	<b>0.2050</b>	<b>0.8420</b>	<b>684,774,265</b>	<b>26,322</b>	<b>A1 / A+</b>	<b>52.13%</b>	<b>4,582,312</b>	<b>0.6692</b>	<b>174.09</b>	<b>1,250,000</b>	<b>0.1825</b>	<b>544</b>	
Marshall (9)	0.4040	0.0720	0.4760	1,046,866,200	24,089	A1 / AA-	55.70%	5,962,546	0.5696	247.52	1,219,000	0.1164	719	
Orange	0.5830	0.1620	0.7450	723,231,143	19,613	Aa3 / A+	62.70%	2,306,088	0.3189	117.58	-	0.0000	846	
Pampa	0.5280	0.0930	0.6210	584,484,955	18,180	A1 / A+	33.81%	3,685,594	0.6306	202.73	35,150	0.0060	605	
Plainview	0.5790	0.0000	0.5790	748,293,643	22,343	Aa3 / AA-	27.78%	3,688,408	0.4929	165.08	506,451	0.0677	443	
Portland	0.4970	0.1170	0.6140	786,654,859	18,500	AA-	31.61%	2,520,683	0.3204	136.25	904,684	0.1150	459	
San Benito	0.6478	0.0403	0.6881	584,440,167	25,391	A1 / A / A+	27.84%	2,760,389	0.4723	108.72	106,385	0.0182	355	
Watauga	0.4120	0.1770	0.5890	964,172,471	24,350	Aa3 / A / AA-	55.96%	5,626,261	0.5835	231.06	457,160	0.0474	444	

(1) Comparable Cities were determined as cities with a TAV not less than and not greater than 50% of the City of Kingsville's TAV, and with a population of not less than 18,000 and not greater than 35,000.

(2) 2011 Tax Rate and Valuation as provided by the Municipal Advisory Council of Texas. As of July 12, 2012.

(3) Source: Texas Municipal League. As of July, 2012.

(4) Source: Municipal Advisory Council of Texas. As of July 12, 2012.

(5) Source: Municipal Advisory Council of Texas. As of Fiscal Year September 30, 2011.

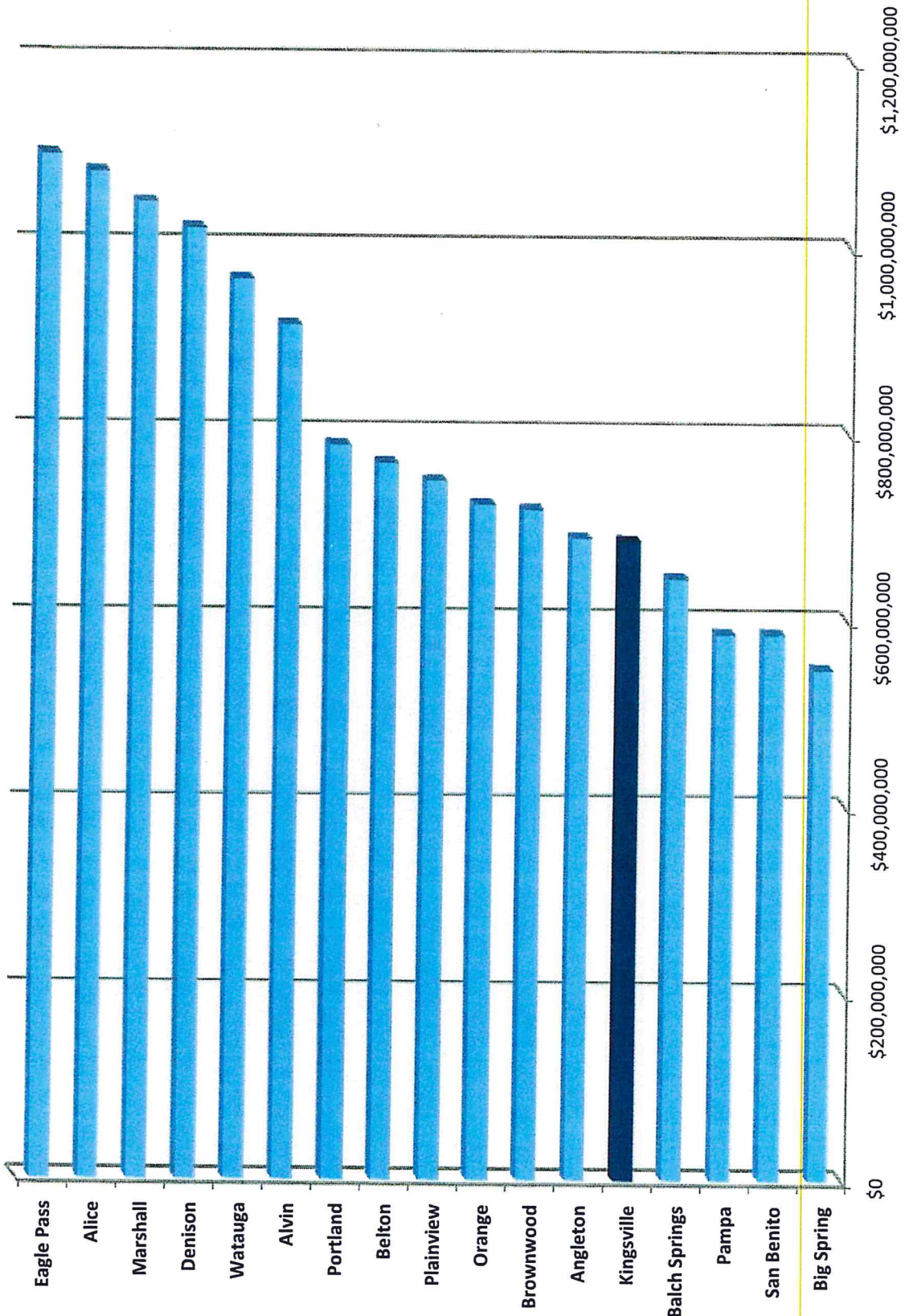
(6) Financial information as of FYE 2011.

(7) The City of Alice's sales tax receipts are substantially higher than the listed cities; however, the City of Alice has been included for comparative purposes.

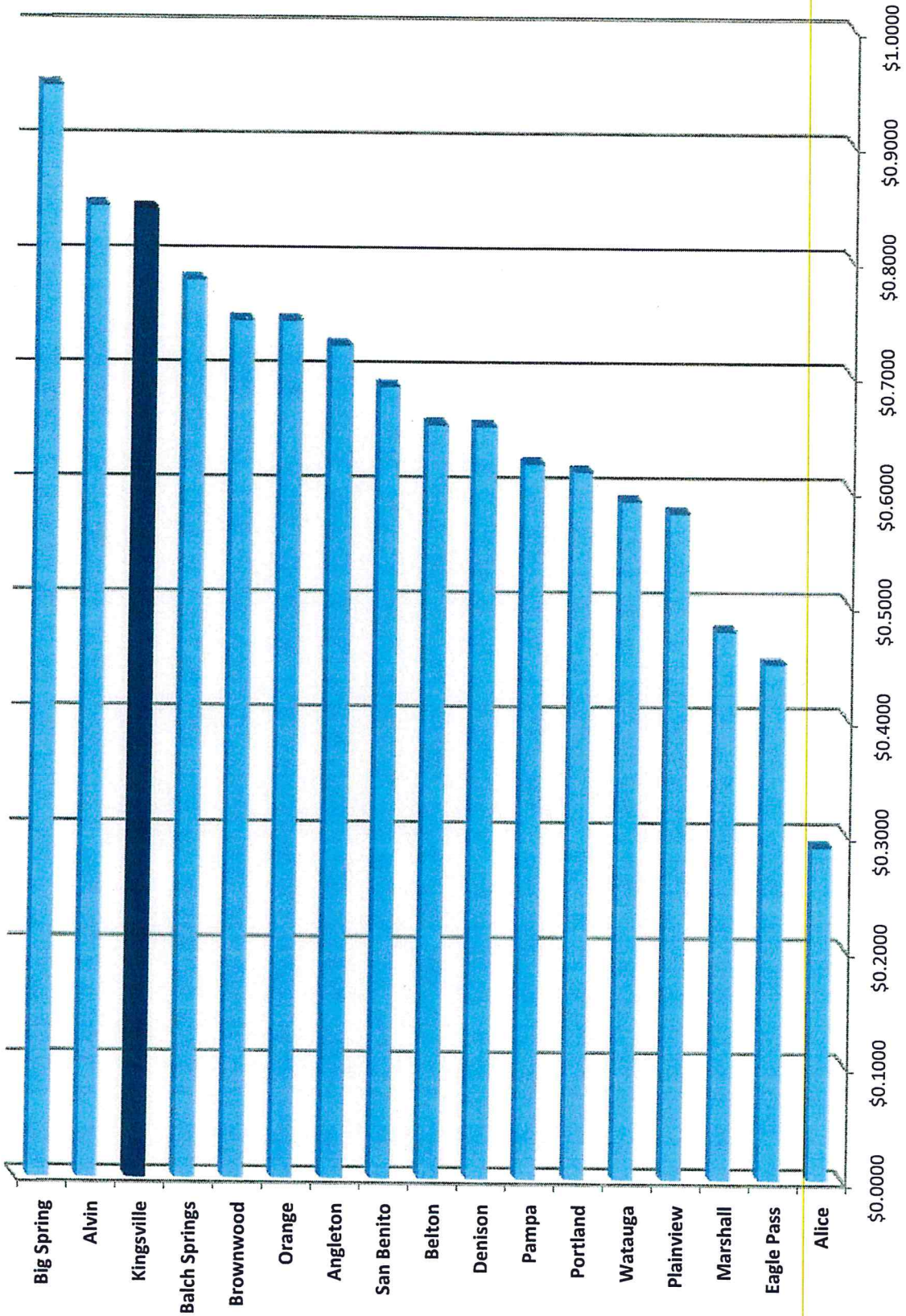
(8) Financial information in unaudited as of FYE 2011.

(9) Financial information as of FYE 2010, FYE 2011 audit and unaudited information not available.

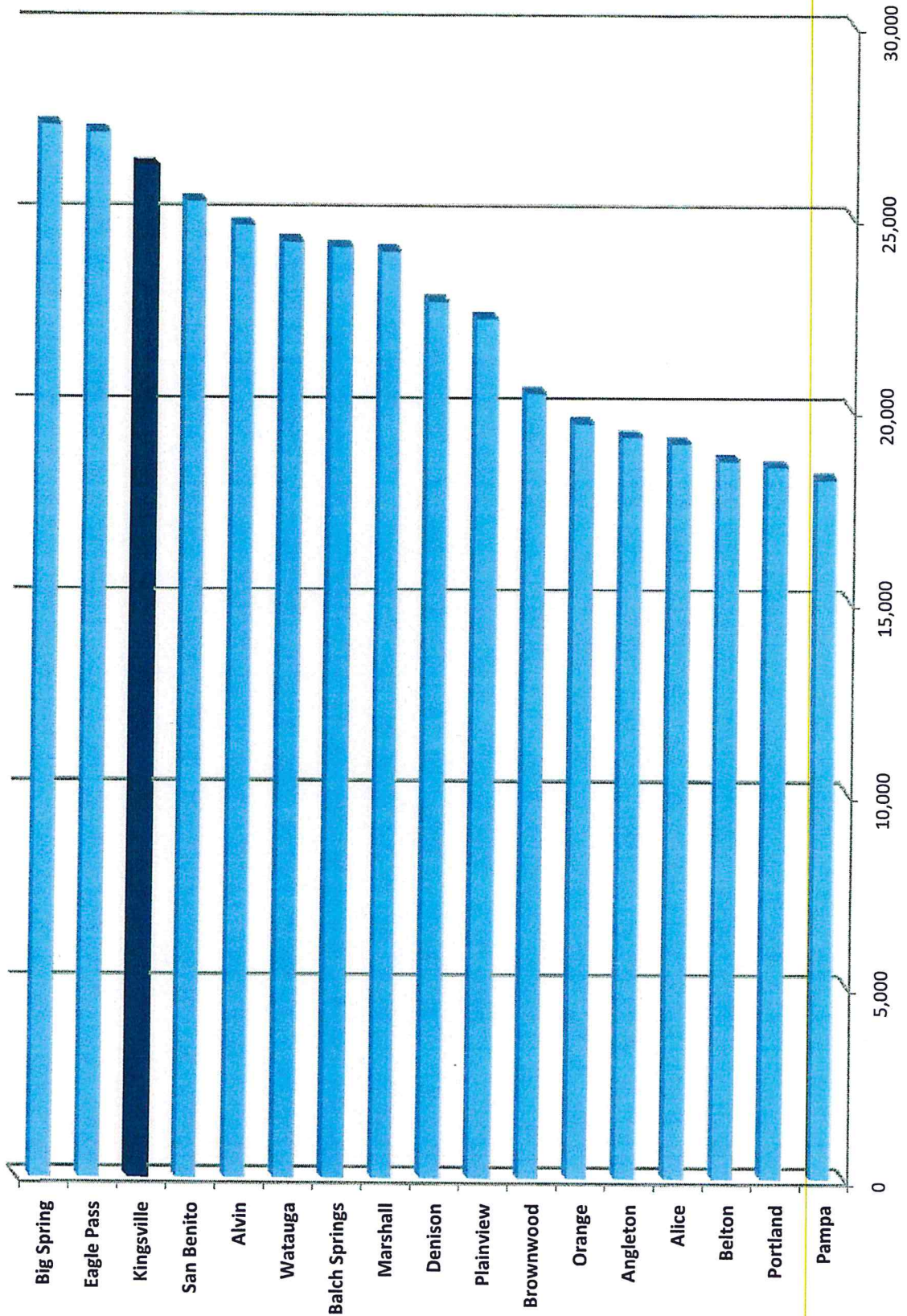
# Taxable Assessed Valuation



# Total Tax Rate



# Population



2012 Stormwater Fee Survey

*Per Month*

City	Population	\$/ERU
Abilene	115,930	\$2.45
Allen	76,600	\$3.00
Amarillo	185,525	\$2.51
Arlington	367,197	\$4.25
Austin	709,893	\$7.75
Azle	11,164	\$3.00
Baytown	68,714	\$1.50
Bedford	49,902	\$3.50
Belton	18,277	\$3.00
Benbrook	22,850	\$6.50
Bryan	67,266	\$2.80
Burkburnett	10,927	\$1.50
Cibolo	18,000	\$4.00
Cleburne	30,400	\$2.75
College Station	84,000	\$5.00
Colleyville	23,210	\$7.00
Coppell	39,460	\$1.00
Corinth	19,556	\$6.00
Crowley	12,920	\$1.75
Dallas	1,280,500	\$7.77
Denton	109,561	\$5.45
Desoto	47,600	\$6.00
Dickinson	18,000	\$4.00
Eagle Pass	27,183	\$3.00
EL Paso	592,099	\$2.97
Eules	53,400	\$2.50
Flower Mound	65,851	\$3.90
Fort Worth	686,850	\$5.40
Fredericksburg	11,305	\$1.00
Frisco	116,969	\$2.00
Gainesville	16,569	\$3.50
Galveston	57,500	\$7.00
Garland	224,750	\$2.88
Georgetown	45,342	\$4.75
Grand Prairie	161,550	\$3.76
Grapevine	48,583	\$4.00
Haltom City	40,132	\$4.89
Harker Heights	26,700	\$6.00
Hewitt	13,588	\$2.75
Highland Village	15,738	\$4.91
Houston	2,099,642	\$9.00
Hurst	38,996	\$4.00
Irving	205,600	\$4.00

	<u>Month</u>	<u>Year</u>
Kingsville now	= \$1.25	\$15.00
Addition Proposed		
93.38/yr. / 12 months =	7.78	93.38
Total Proposed	<u>\$9.03</u>	<u>\$108.38</u>

Keller	37,700	\$8.00
Kennedale	7,284	\$2.50
Killeen	102,003	\$6.00
Lancaster	36,225	\$7.97
Laredo	215,484	\$6.50
Leon Valley	11,020	\$3.68
Little Elm	25,898	\$3.35
Live Oak	14,503	\$5.50
Lubbock	212,365	\$12.00
Mansfield	51,300	\$3.50
McKinney	112,000	\$2.75
Mesquite	136,750	\$3.00
Mission	77,058	\$1.50
New Braunfels	51,066	\$4.59
North Richland Hills	64,050	\$2.58
Plano	255,700	\$3.30
Portland	18,500	\$3.00
Prosper	10,500	\$4.00
Richardson	103,201	\$3.75
Richland Hills	8,300	\$9.50
Round Rock	92,392	\$2.75
Rowlett	54,869	\$5.50
Saginaw	18,950	\$3.00
San Angelo	93,200	\$4.00
San Antonio	1,306,900	\$4.25
San Marcos	50,371	\$6.74
Schertz	30,000	\$3.80
Sealy	6,150	\$2.00
Selma	5,540	\$4.12
Southlake	26,224	\$8.00
Stephenville	17,050	\$3.00
Sunset Valley	468	\$4.00
Taylor	15,191	\$1.00
Temple	60,000	\$3.00
The Colony	40,206	\$2.50
Trophy Club	7,832	\$6.00
Universal City	18,530	\$3.08
University Park	24,182	\$4.85
Watagua	24,150	\$12.00
Webster	9,930	\$1.24
White Settlement	16,543	\$4.62
Wichita Falls	103,876	\$3.55
<b>Average</b>		<b>\$4.29</b>
<b>Minimum</b>		<b>\$1.00</b>
<b>Maximum</b>		<b>\$12.00</b>

**City of Kingsville, Texas**

**Tax Supported Improvements - Maximum Bank Qualified Bond Issuance Analysis**

**Projected Series 2016 Through Series 2019 - LEVEL DEBT STRUCTURE**

*Preliminary - For Purposes of Discussion Only*



<b>-Projected, \$11,800,000 Bank Qualified Issuance Capacity-</b>										
FYE 9/30	Assessed Valuation <sup>(1)</sup>	Est. AV Growth <sup>(2)</sup>	Existing Tax- Supported Debt Service	Series 2016	Series 2017	Series 2018	Series 2019	Projected Total Debt Service	Est. I&S Tax Rate <sup>(4)(5)</sup>	
				\$1,850,000	\$3,100,000	\$4,500,000	\$2,350,000			
				8/1/2016	8/1/2017	8/1/2018	8/1/2019			
				@ 4.00% <sup>(3)</sup>	@ 4.25% <sup>(3)</sup>	@ 4.50% <sup>(3)</sup>	@ 4.75% <sup>(3)</sup>			
2014	\$ 714,248,395		\$ 1,454,103					\$ 1,454,103	0.18816	
2015	728,913,665	2.05%	1,445,882					1,445,882	0.18662	
2016	728,913,665	0.00%	1,444,038					1,444,038	0.18662	
2017	728,913,665	0.00%	1,198,514	\$ 132,000				1,330,514	0.18721	
2018	728,913,665	0.00%	977,315	135,200	\$ 225,875			1,338,390	0.18832	
2019	728,913,665	0.00%	633,840	132,600	229,950	\$ 336,250		1,332,640	0.18751	
2020	728,913,665	0.00%	457,340	135,000	230,488	336,925	\$ 175,813	1,335,565	0.18792	
2021	728,913,665	0.00%	463,215	132,200	230,813	340,400	165,925	1,332,553	0.18750	
2022	728,913,665	0.00%	458,615	134,400	225,925	338,425	178,075	1,335,440	0.18791	
2023	728,913,665	0.00%	463,790	131,400	226,038	336,225	179,513	1,336,965	0.18812	
2024	728,913,665	0.00%	345,640	133,400	225,938	338,800	185,713	1,229,490	0.17300	
2025	728,913,665	0.00%	355,940	135,200	230,625	335,925	181,438	1,239,128	0.17436	
2026	728,913,665	0.00%	355,540	131,800	229,888	337,825	182,163	1,237,215	0.17409	
2027	728,913,665	0.00%	349,740	133,400	228,938	339,275	182,650	1,234,003	0.17363	
2028	728,913,665	0.00%	358,510	134,800	227,775	340,275	182,900	1,244,260	0.17508	
2029	728,913,665	0.00%	276,270	131,000	226,400	340,825	182,913	1,157,408	0.16286	
2030	728,913,665	0.00%	278,045	132,200	229,813	340,925	182,688	1,163,670	0.16374	
2031	728,913,665	0.00%	269,325	133,200	227,800	340,575	182,225	1,153,125	0.16225	
2032	728,913,665	0.00%	275,400	134,000	230,575	339,775	181,525	1,161,275	0.16340	
2033	728,913,665	0.00%	70,650	134,600	227,925	338,525	180,588	952,288	0.13399	
2034	728,913,665	0.00%		135,000	230,063	336,825	184,413	886,300	0.12471	
2035	728,913,665	0.00%		135,200	226,775	339,675	182,763	884,413	0.12444	
2036	728,913,665	0.00%		135,200	228,275	336,850	180,875	881,200	0.12399	
2037	728,913,665	0.00%			229,350	338,575	183,750	751,675	0.10577	
2038	728,913,665	0.00%				339,625	181,150	520,775	0.07328	
2039	728,913,665	0.00%					183,313	183,313	0.02579	
2040	728,913,665	0.00%					-	-	-	
2041	728,913,665	0.00%					-	-	-	
2042	728,913,665	0.00%					-	-	-	
2043	728,913,665	0.00%					-	-	-	
2044	728,913,665	0.00%					-	-	-	
				<u>\$ 11,931,712</u>	<u>\$ 2,671,800</u>	<u>\$ 4,569,225</u>	<u>\$ 6,772,500</u>	<u>\$ 3,620,388</u>	<u>\$ 29,565,624</u>	

(1) FY 2014 and FY 2015 assessed valuation and FY 2014 I&S tax rate provided by the Kleberg County Appraisal District.

(2) Assumes 0% growth in Taxable Assessed Valuation.

(3) August 1 Dated and Delivery Date with First Interest payment on February 1.

(4) Est. tax collections rate: **97.50%**

(5) FYE 2015 Tax Rate provided by City Officials.

Scenario 1

9/26/2014

**City of Kingsville, Texas**  
**Tax Supported Improvements - Maximum Bank Qualified Bond Issuance Analysis**  
**Projected Series 2016 Through Series 2019 - WRAP STRUCTURE**  
**Preliminary - For Purposes of Discussion Only**



**-Projected: \$15,500,000 Bank Qualified Issuance Capacity-**

FYE 9/30	Assessed Valuation <sup>(1)</sup>	Est. AV Growth <sup>(2)</sup>	Existing Tax- Supported Debt Service	Series 2016	Series 2017	Series 2018	Series 2019	Projected Total Debt Tax Rate <sup>(4)</sup>	Est. I&S Tax Rate <sup>(5)</sup>
				\$3,000,000 8/1/2016 @ 4.00% <sup>(3)</sup>	\$4,500,000 8/1/2017 @ 4.25% <sup>(3)</sup>	\$6,000,000 8/1/2018 @ 4.50% <sup>(3)</sup>	\$2,000,000 8/1/2019 @ 4.75% <sup>(3)</sup>		
2014	\$ 714,248,395		\$ 1,454,103					\$ 1,454,103	0.18816
2015	728,913,665	2.05%	1,445,882					1,445,882	0.18662
2016	741,051,054	1.67%	1,444,038					1,444,038	0.18662
2017	753,390,547	1.67%	1,198,514	\$ 180,000				1,378,514	0.18767
2018	765,935,509	1.67%	977,315	265,200	\$ 125,625			1,368,140	0.18320
2019	778,689,361	1.67%	633,840	329,200	274,975	\$ 175,000		1,413,015	0.18611
2020	791,655,582	1.67%	457,340	310,400	276,363	308,200	\$ 87,500	1,439,803	0.18654
2021	804,837,708	1.67%	463,215	292,000	272,538	311,400	133,100	1,472,253	0.18762
2022	818,239,334	1.67%	458,615	279,000	273,713	334,375	131,200	1,476,903	0.18513
2023	831,864,115	1.67%	463,790	261,200	324,675	341,225	139,300	1,530,190	0.18866
2024	845,715,767	1.67%	345,640	248,800	383,300	432,625	141,925	1,552,290	0.18825
2025	859,798,068	1.67%	355,940	241,600	384,163	459,750	134,313	1,575,765	0.18797
2026	874,114,858	1.67%	355,540	229,400	379,600	470,300	146,938	1,581,778	0.18560
2027	888,670,041	1.67%	349,740	212,400	379,825	479,950	188,850	1,610,765	0.18590
2028	903,467,588	1.67%	358,510	200,800	379,625	543,700	188,625	1,671,260	0.18973
2029	918,511,534	1.67%	276,270	189,400	379,000	564,075	218,163	1,626,908	0.18167
2030	933,805,981	1.67%	278,045	178,200	382,950	587,875	211,038	1,638,108	0.17992
2031	949,355,102	1.67%	269,325	107,200	381,263	569,875	158,913	1,486,575	0.16060
2032	965,163,136	1.67%	275,400	103,800	379,150	571,875	158,925	1,489,150	0.15825
2033	981,234,395	1.67%	70,650	105,400	381,613	572,975	158,700	1,289,338	0.13477
2034	997,573,262	1.67%		106,800	383,438	573,175	158,238	1,221,650	0.12560
2035	1,014,184,194	1.67%		108,000	379,625	572,475	157,538	1,217,638	0.12314
2036	1,031,071,719	1.67%		104,000	380,388	570,875	156,600	1,211,863	0.12055
2037	1,048,240,445	1.67%			380,513	573,375	155,425	1,109,313	0.10854
2038	1,065,695,052	1.67%				574,750	159,013	733,763	0.07062
2039	1,083,440,303	1.67%					157,125	157,125	0.01487
2040	1,101,481,035	1.67%						-	-
2041	1,119,822,170	1.67%						-	-
2042	1,138,468,710	1.67%						-	-
2043	1,157,425,739	1.67%						-	-
2044	1,176,698,428	1.67%						-	-
				<u>\$ 11,931,712</u>	<u>\$ 4,052,800</u>	<u>\$ 6,882,338</u>	<u>\$ 9,587,850</u>	<u>\$ 3,141,425</u>	<u>\$ 35,596,124</u>

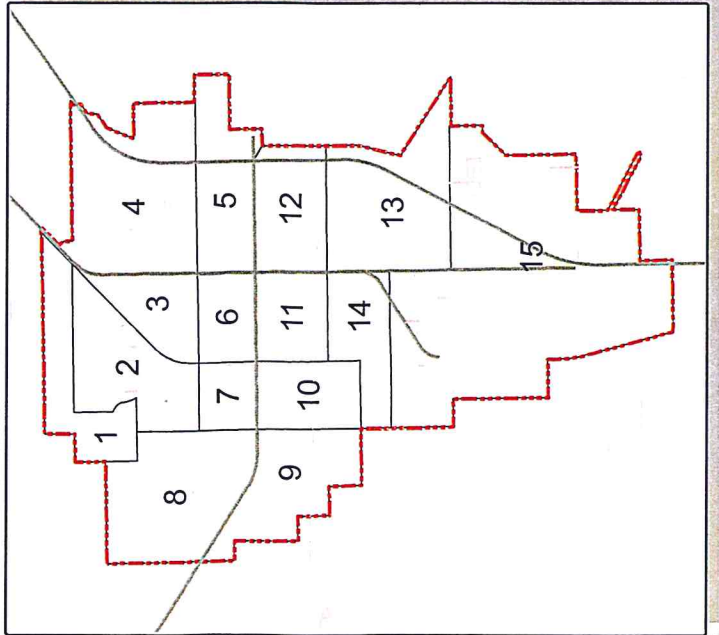
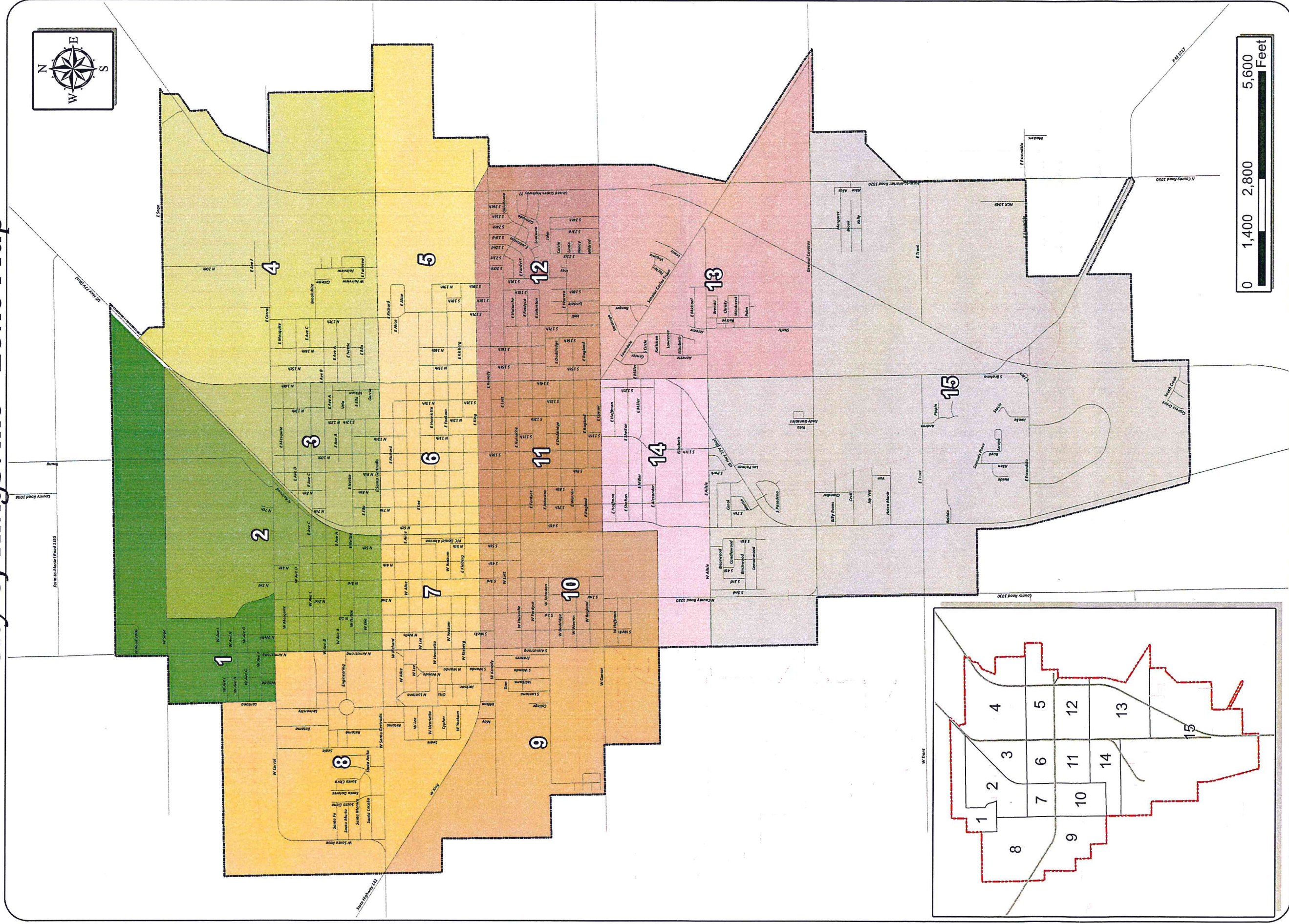
(1) FY 2014 and FY 2015 assessed valuation and FY 2014 I&S tax rate provided by the Kleberg County Appraisal District.  
(2) Estimated assessed valuation growth in Fiscal Year 2015 and thereafter equal to the average tax base growth in Fiscal Years 2011 through 2015.  
(3) August 1 Dated and Delivery Date with First Interest payment on February 1.  
(4) Est. tax collections rate: 97.50%  
(5) FYE 2015 Tax Rate provided by City Officials.

Scenario 4

9/26/2014



# City of Kingsville - Zone Map



Drawn By: Paul Laraway

Last Update: 8/2/2013

Note:

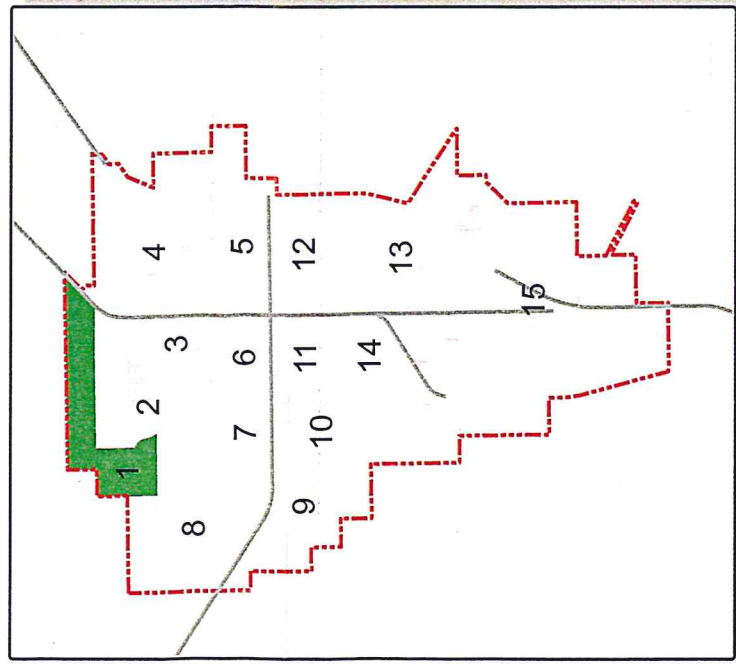
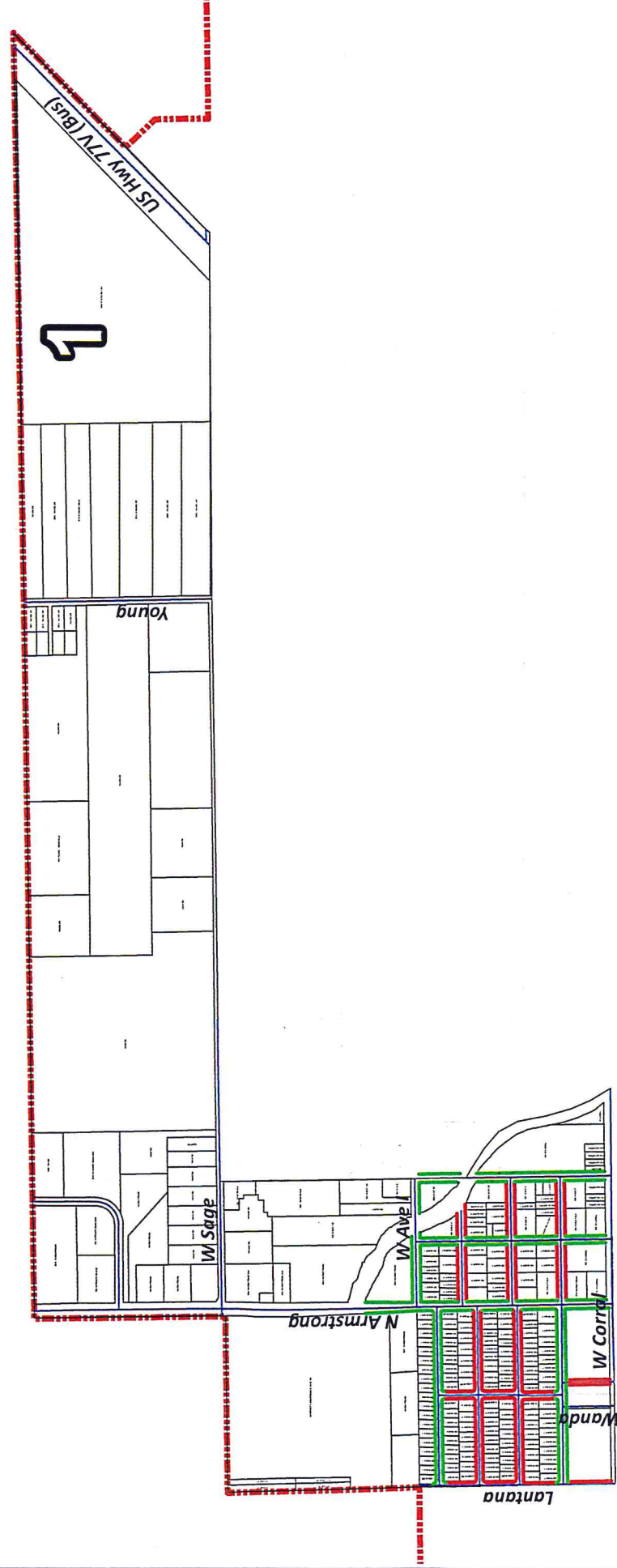
Page  
**8-1**

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OR ANY OTHER PURPOSE.



**CITY OF KINGSVILLE**  
**ENGINEERING DEPARTMENT**  
200 East Kleberg  
Kingsville, Texas 78363  
Office: 361-595-8005  
Fax: 361-595-8035

# City of Kingsville - Zone 1



Blocks = 34  
Curb and Gutter = 27,265 linear ft. ~ 5.16 miles

**Legend**

- Curb\_Gutter Inventory Zone 1 Fair
- Curb\_Gutter Inventory Zone 1 Poor



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Drawn By: D. Herrera

Last Update: 1/9/2015

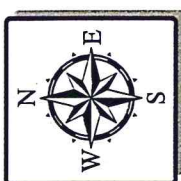
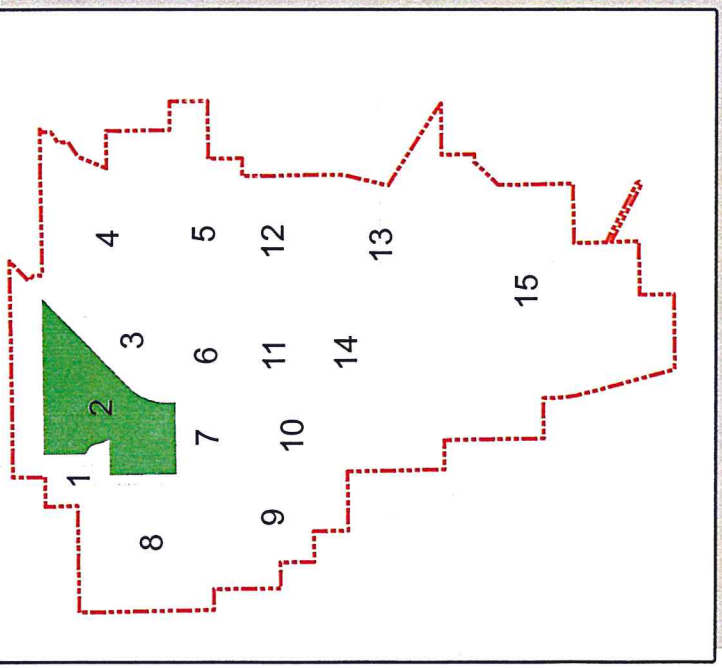
Note:



**CITY OF KINGSVILLE**  
**ENGINEERING DEPARTMENT**  
200 East Kleberg  
Kingsville, Texas 78363  
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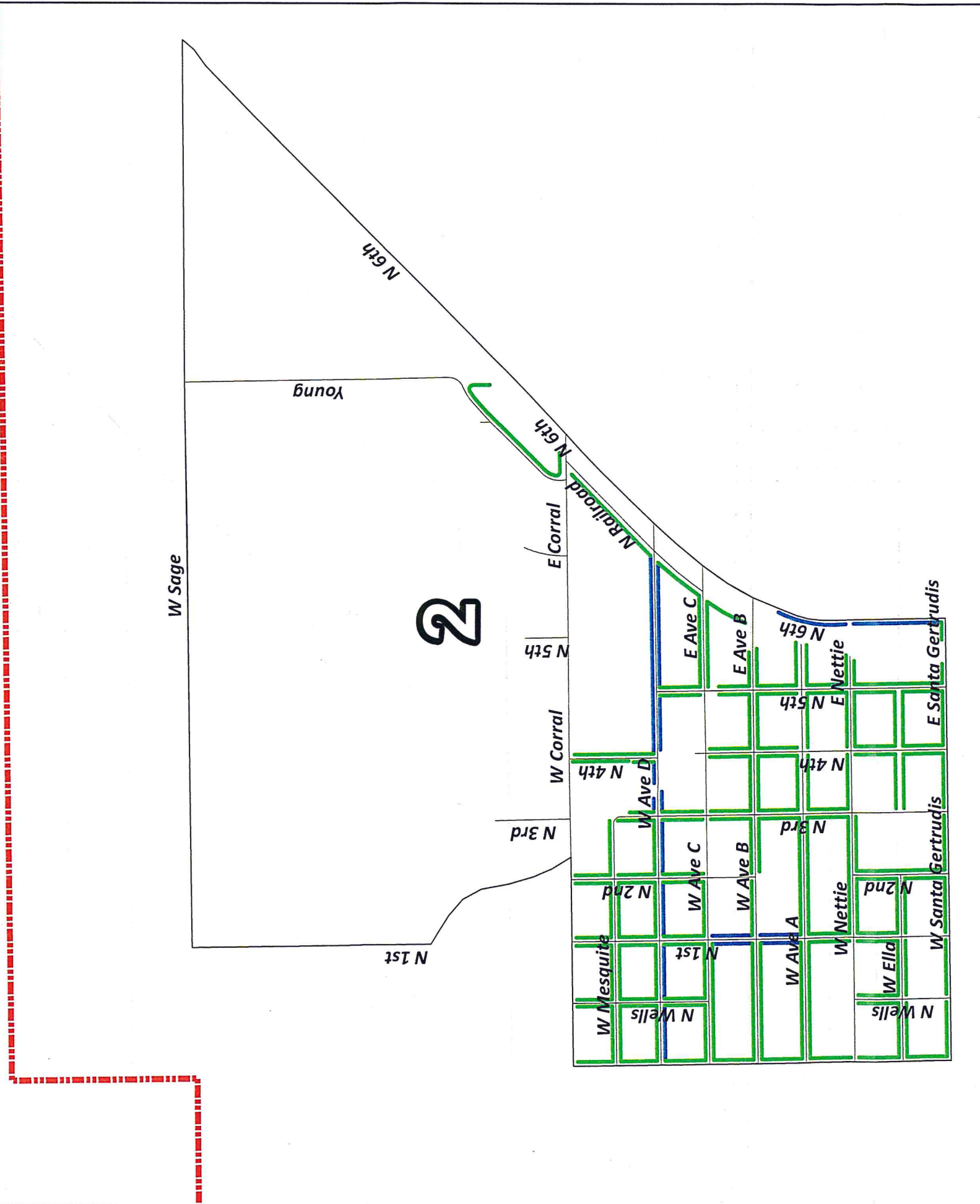
# City of Kingsville - Zone 2



**Legend**

- Curb\_Gutter Inventory Zone 2 Fair
- Curb\_Gutter Inventory Zone 2 Good

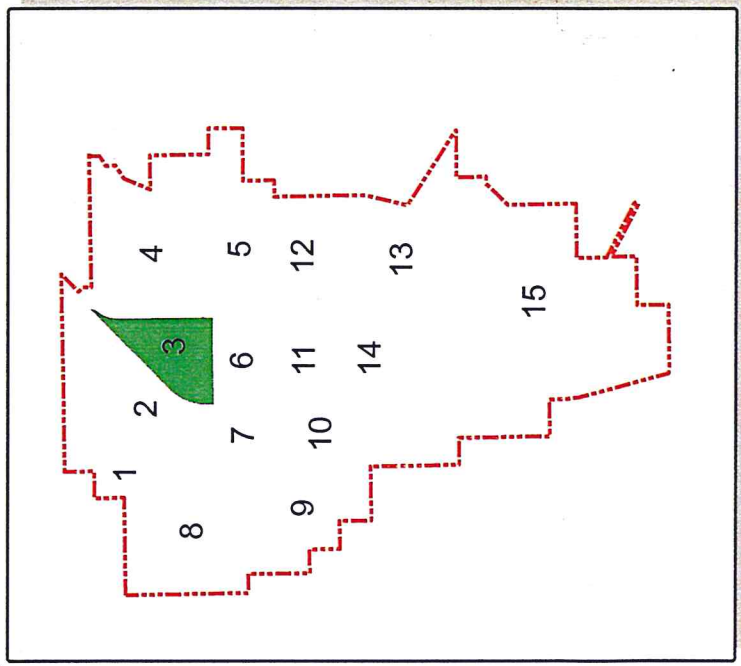
Blocks = 89  
 Curb and Gutter = 70,901 linear ft. ~ 13.43 miles



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Page <b>8-3</b>	Drawn By: D. Herrera	Note:	DISCLAIMER THIS MAP IS FOR VISUAL PURPOSES ONLY. THE INFORMATION ON THIS SHEET MAY CONTAIN INACCURACIES OR ERRORS. THE CITY OF KINGSVILLE IS NOT RESPONSIBLE IF THE INFORMATION CONTAINED HEREIN IS USED FOR ANY DESIGN, CONSTRUCTION, PLANNING, BUILDING, OR ANY OTHER PURPOSE.
	Last Update: 1/9/2015		
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# City of Kingsville - Zone 3



## Legend

- Curb\_Gutter Inventory Zone 3 Poor
- Curb\_Gutter Inventory Zone 3 Fair
- Curb\_Gutter Inventory Zone 3 Good

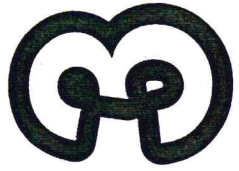


N 14th

N 6th

Blocks = 86

Curb and Gutter = 68,462 linear ft. ~ 12.97 miles



E Corral

E Mesquite

E Ave D

N 8th

E Ave C

E Ave B

N 7th

E Ave A

E Ave A

E Nettie

N 8th

N 11th

Vela

Wilson

Garcia

N 14th

N 12th S 12th N 12th

E Ella

N 9th

E Santa Gertrudis

N 7th



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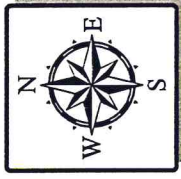
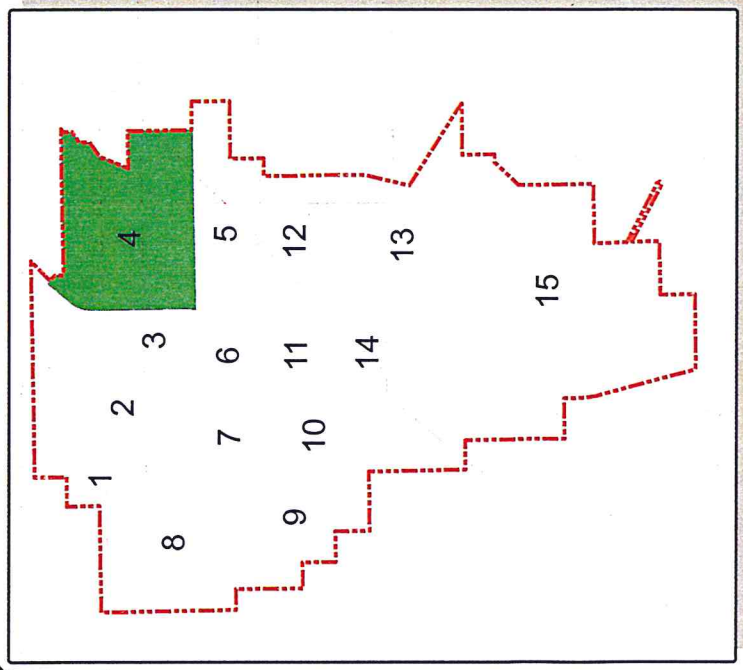
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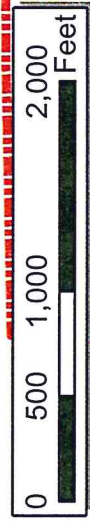
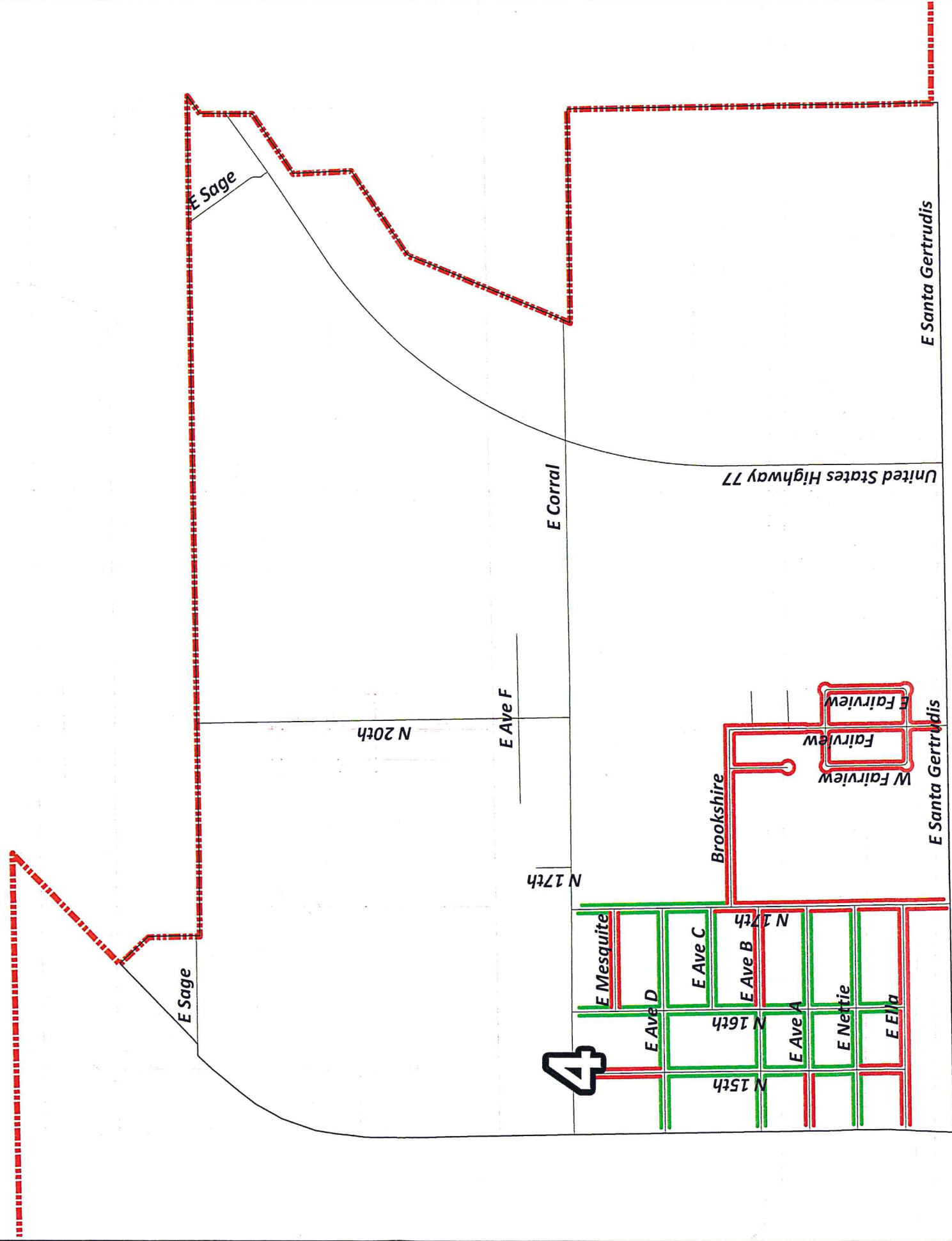
# City of Kingsville - Zone 4



## Legend

- Curb\_Gutter Inventory Zone 4 Poor
- Curb\_Gutter Inventory Zone 4 Fair

Blocks = 57  
 Curb and Gutter = 42,136 linear ft. ~ 7.98 miles



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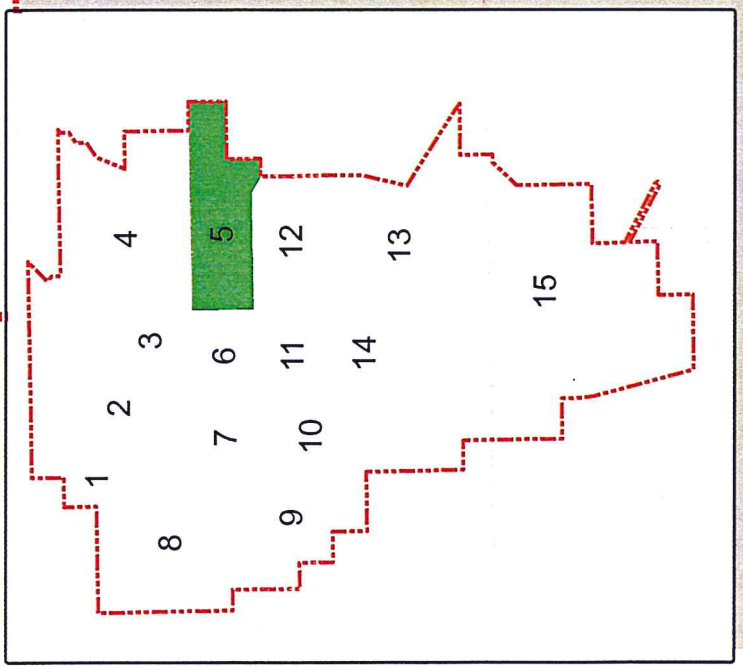
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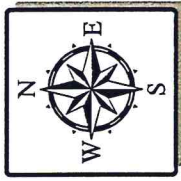
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# City of Kingsville - Zone 5

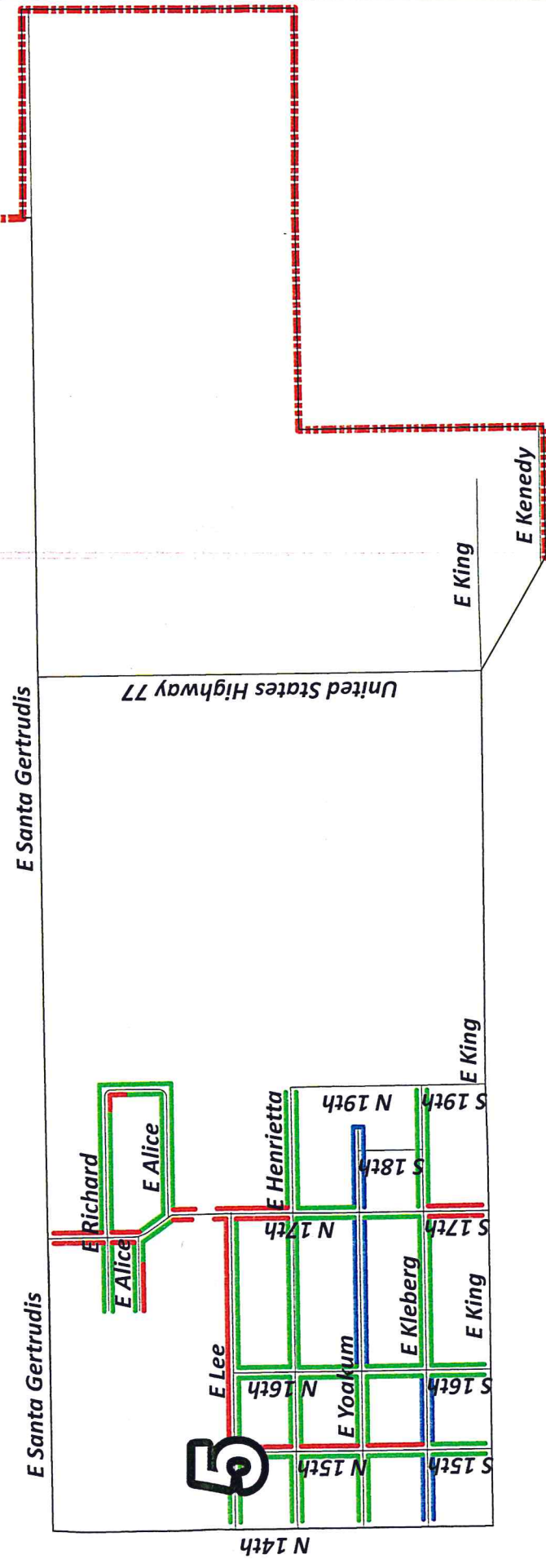


**Legend**


- Curb\_Gutter Inventory Zone 5 Poor
- Curb\_Gutter Inventory Zone 5 Fair
- Curb\_Gutter Inventory Zone 5 Good



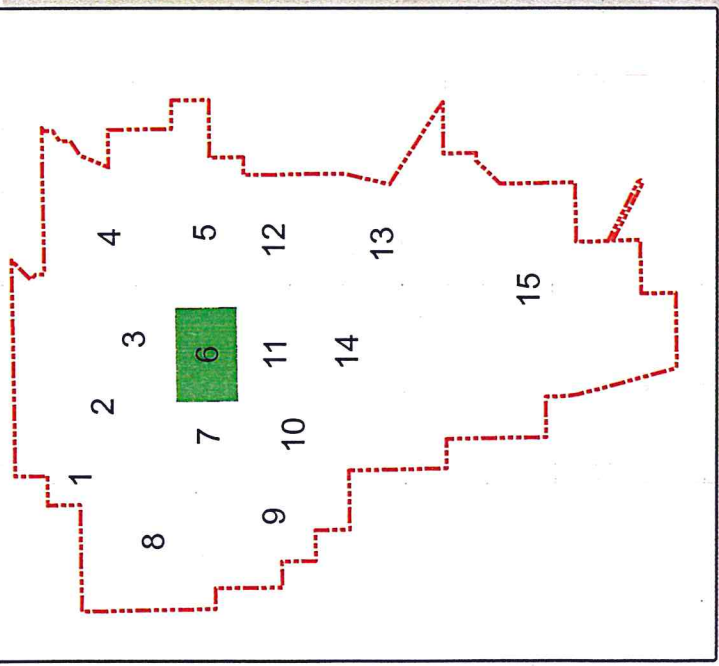
Blocks = 38  
 Curb and Gutter = 30,468 linear ft. ~ 5.77 miles



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# City of Kingsville - Zone 6

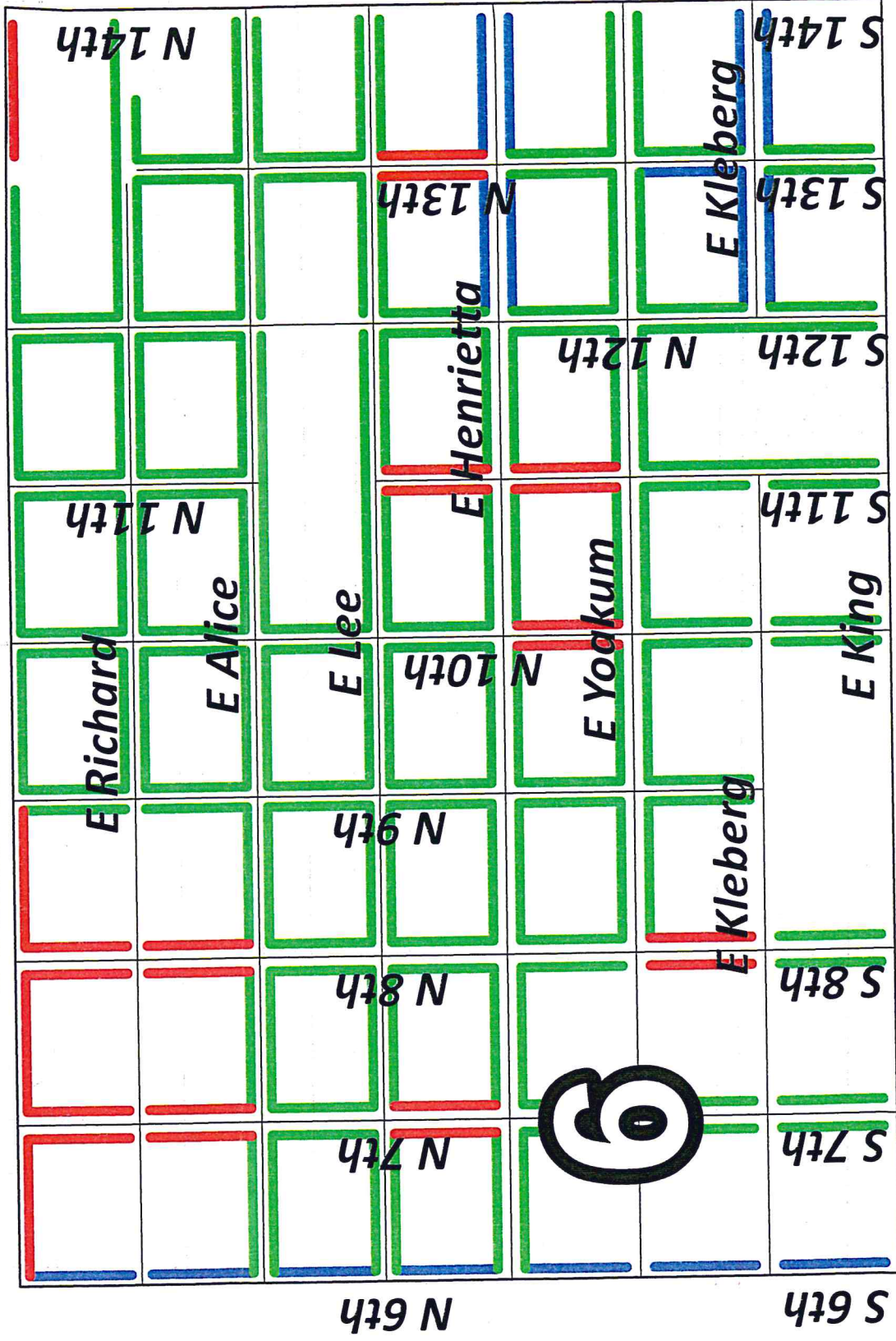


**Legend**

- Curb\_Gutter Inventory Zone 6 Poor
- Curb\_Gutter Inventory Zone 6 Fair
- Curb\_Gutter Inventory Zone 6 Good

Blocks = 84  
 Curb and Gutter = 67,316 linear ft. ~ 12.75 miles

## E Santa Gertrudis



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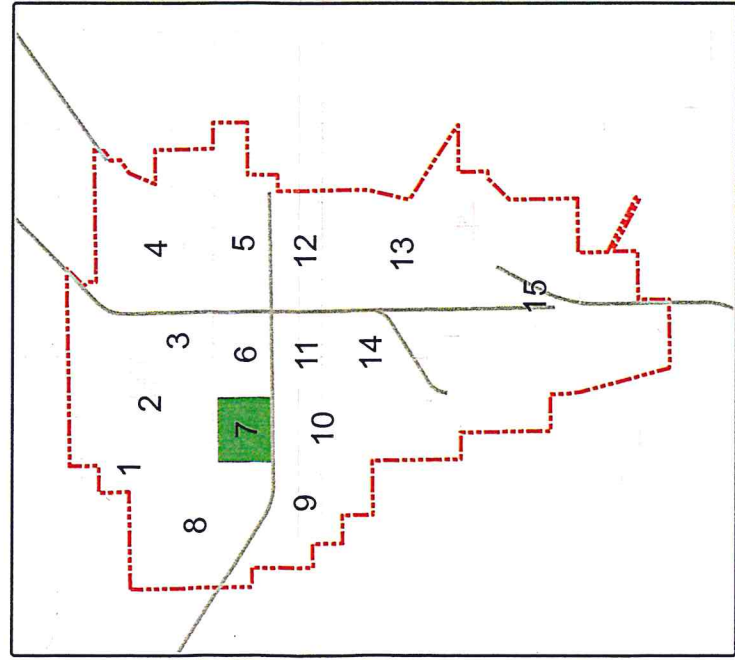
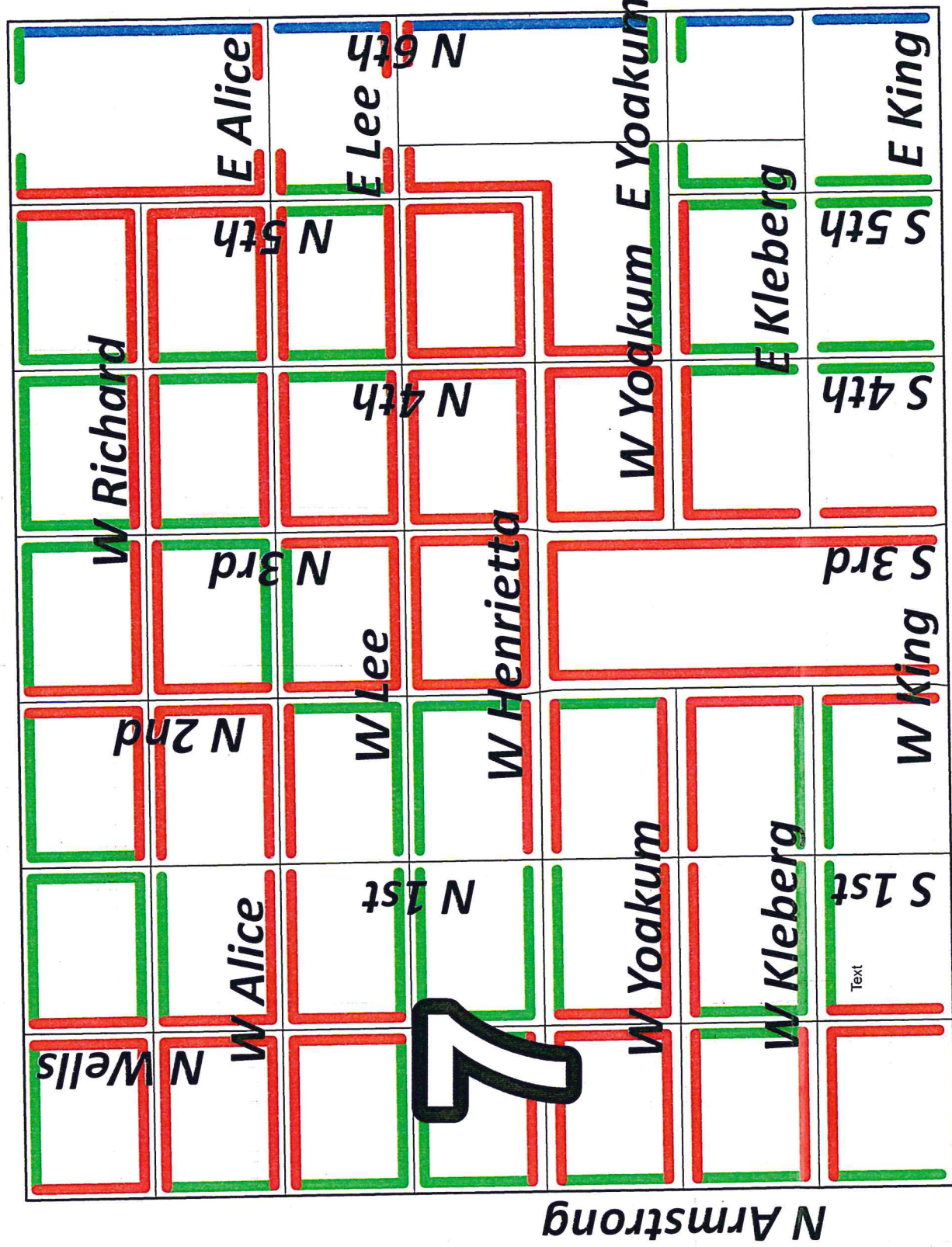
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# City of Kingsville - Zone 7



E Santa Gertrudis



Blocks = 81  
Curb and Gutter = 65,119 linear ft. ~ 12.33 miles

**Legend**

- Curb\_Gutter Inventory Zone 7 Poor
- Curb\_Gutter Inventory Zone 7 Fair
- Curb\_Gutter Inventory Zone 7 Good



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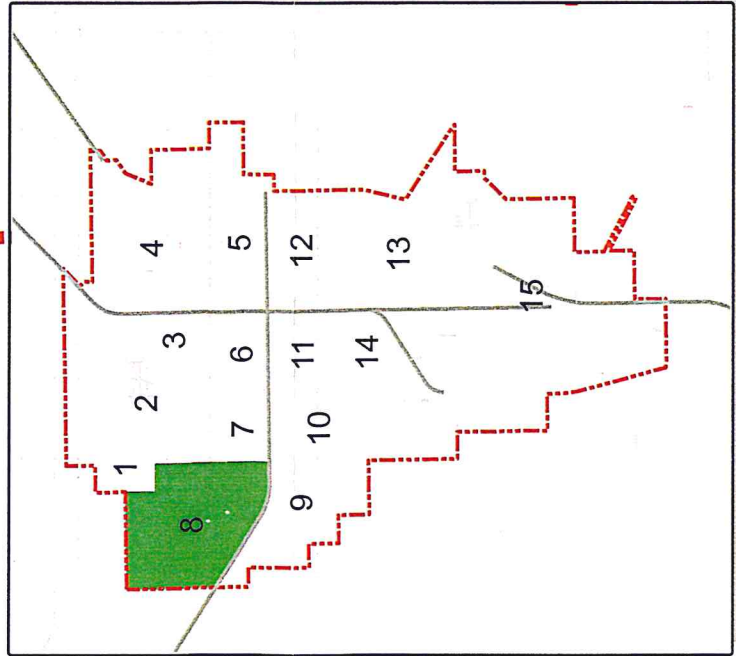
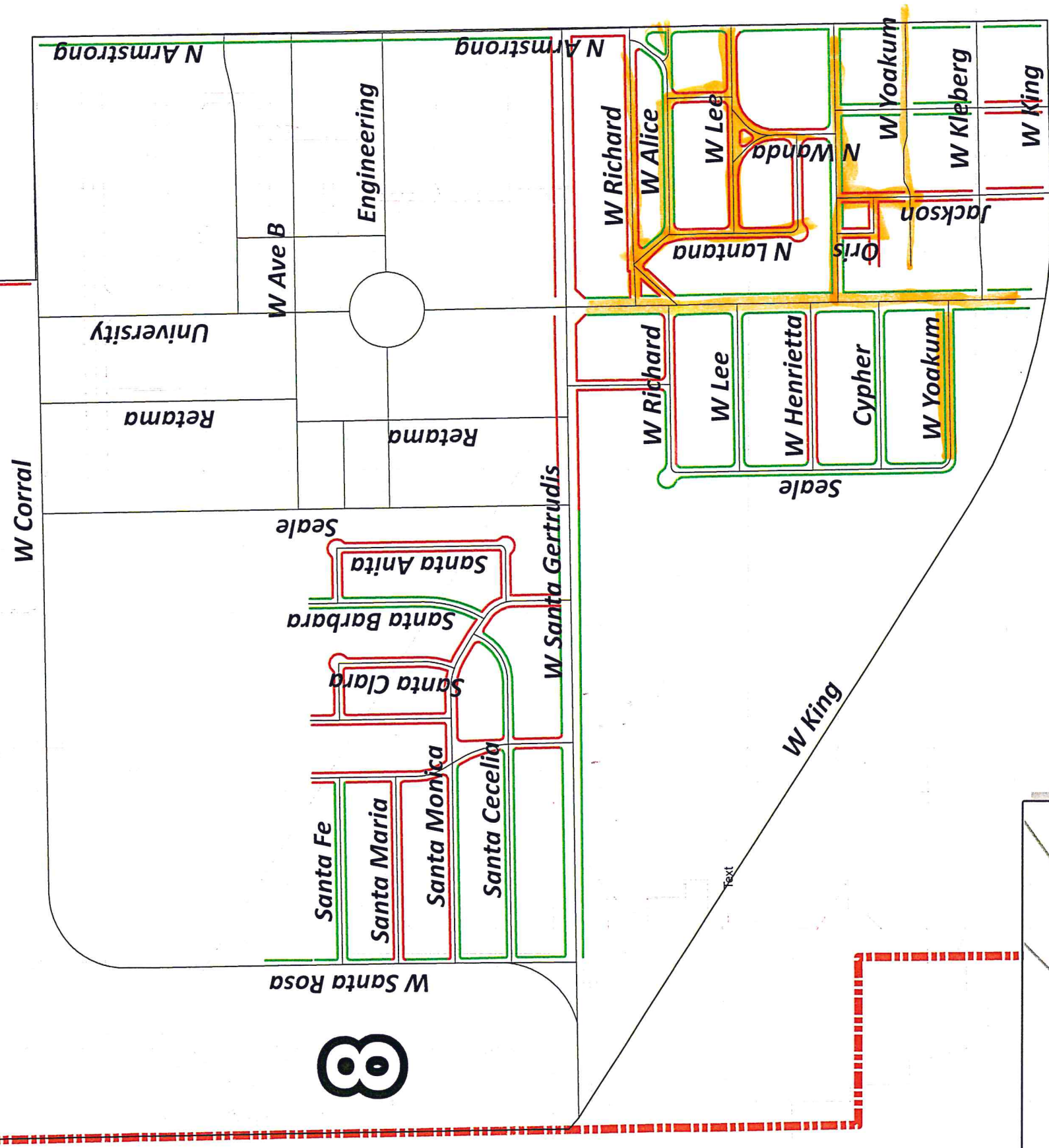


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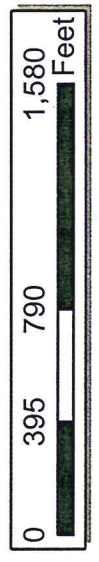
# City of Kingsville - Zone 8



Blocks = 113  
Curb and Gutter = 90,816 linear ft. ~ 17.2 miles

**Legend**

- Curb\_Gutter Inventory Zone 8 Poor
- Curb\_Gutter Inventory Zone 8 Fair



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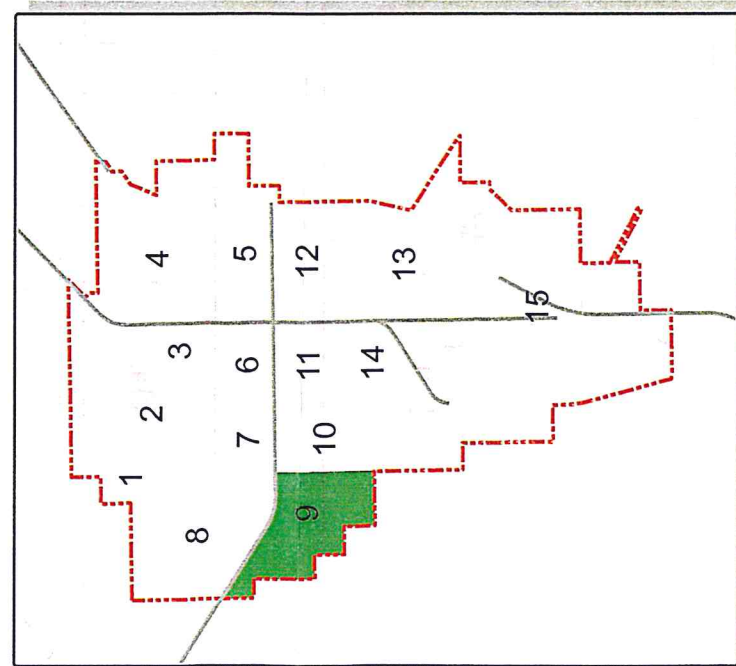
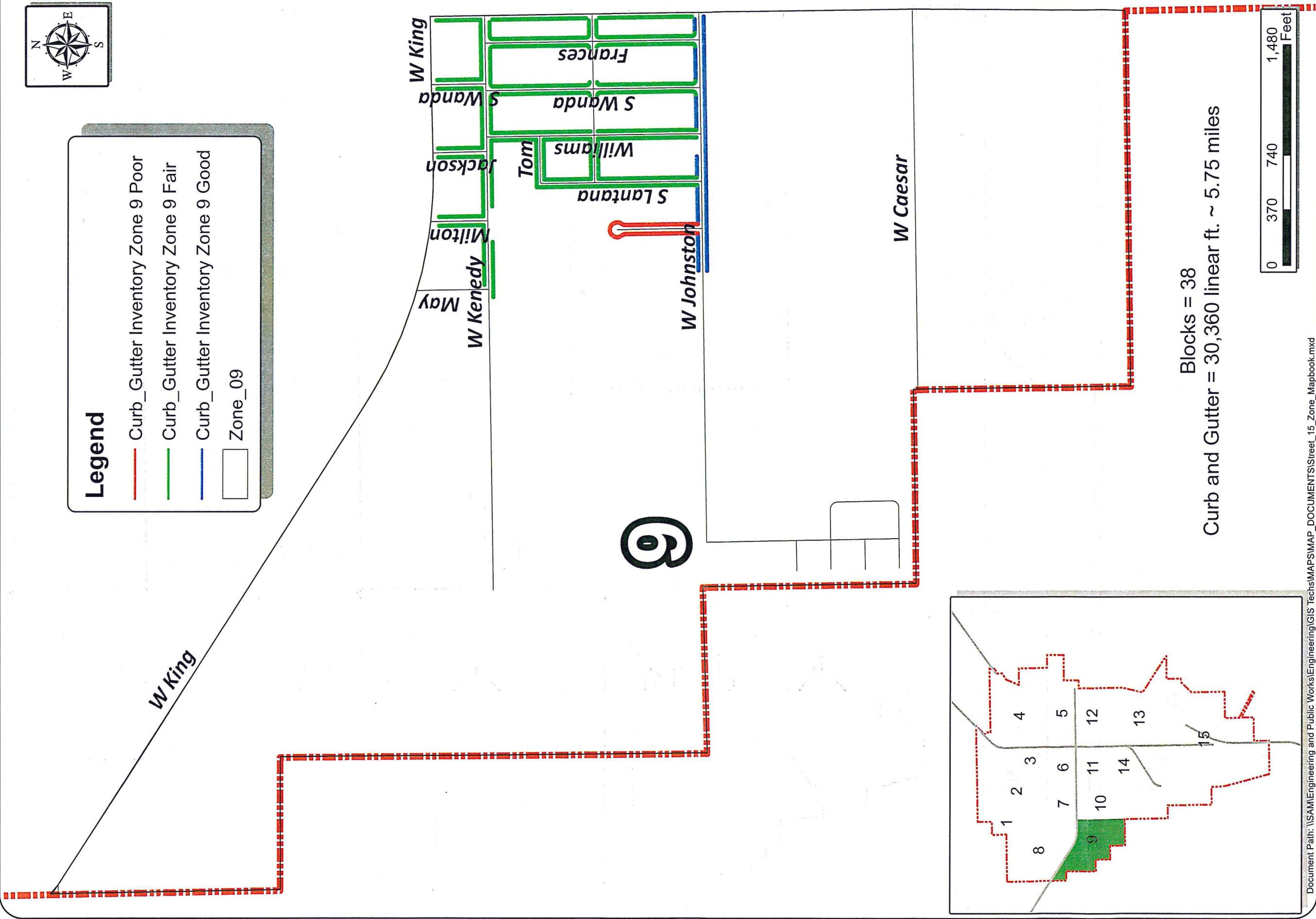
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# Zone 9



**Legend**

- Curb\_Gutter Inventory Zone 9 Poor
- Curb\_Gutter Inventory Zone 9 Fair
- Curb\_Gutter Inventory Zone 9 Good
- Zone\_09



Blocks = 38  
 Curb and Gutter = 30,360 linear ft. ~ 5.75 miles

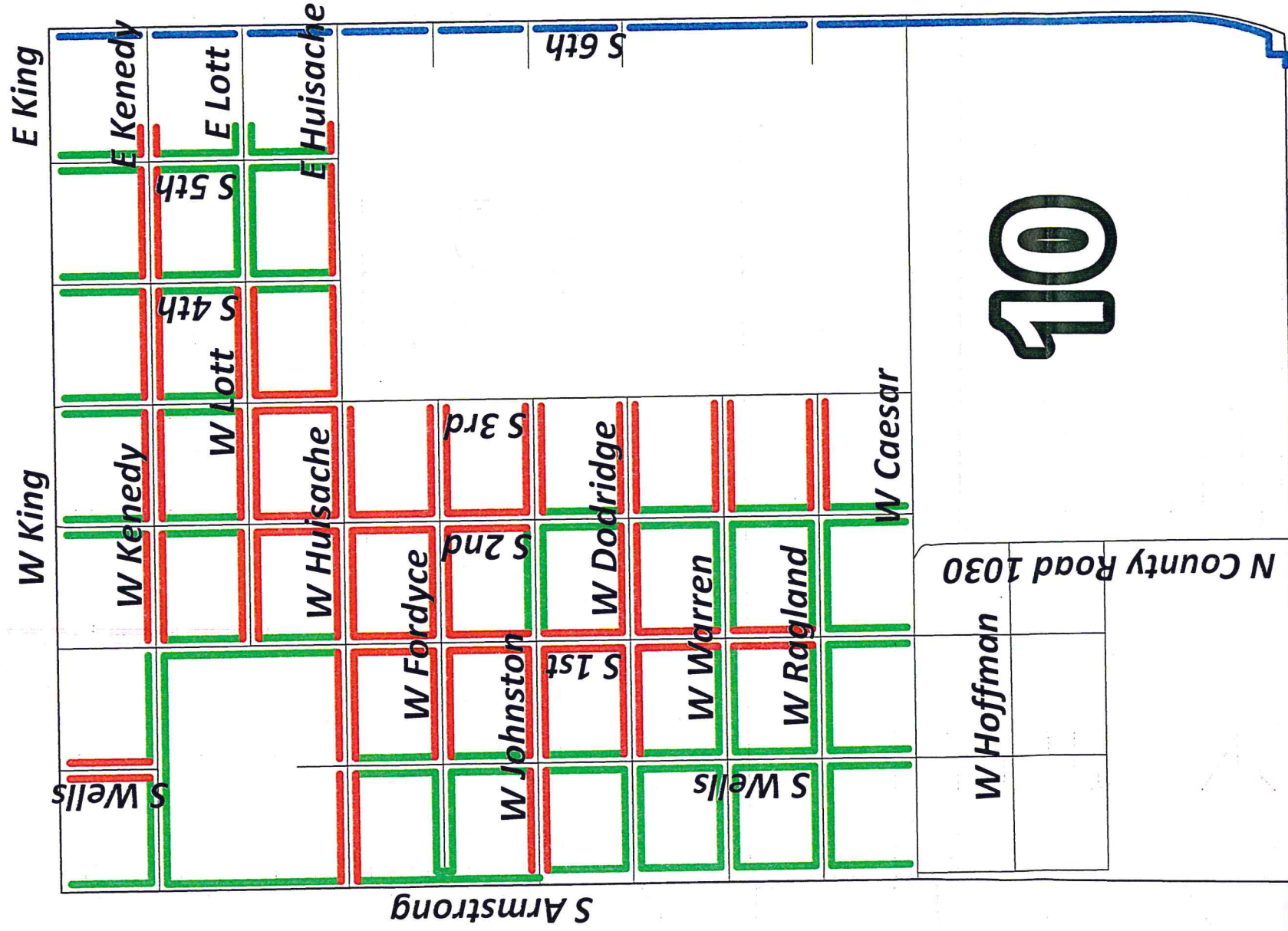
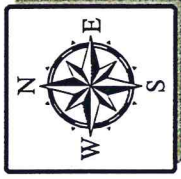


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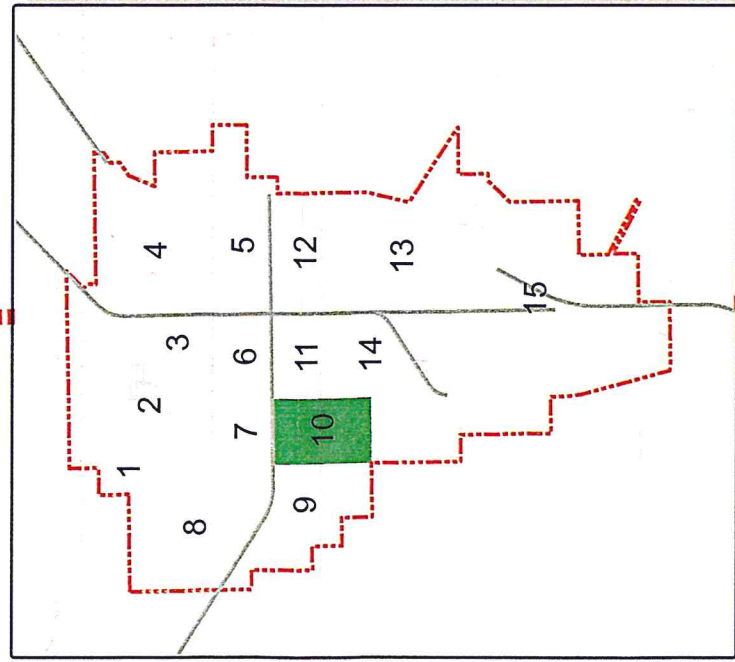
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# Zone 10



Blocks = 72  
Curb and Gutter = 57,816 linear ft. ~ 10.95 miles



### Legend

- Curb\_Gutter Inventory Zone 10 Poor
- Curb\_Gutter Inventory Zone 10 Fair
- Curb\_Gutter Inventory Zone 10 Good
- Zone\_10



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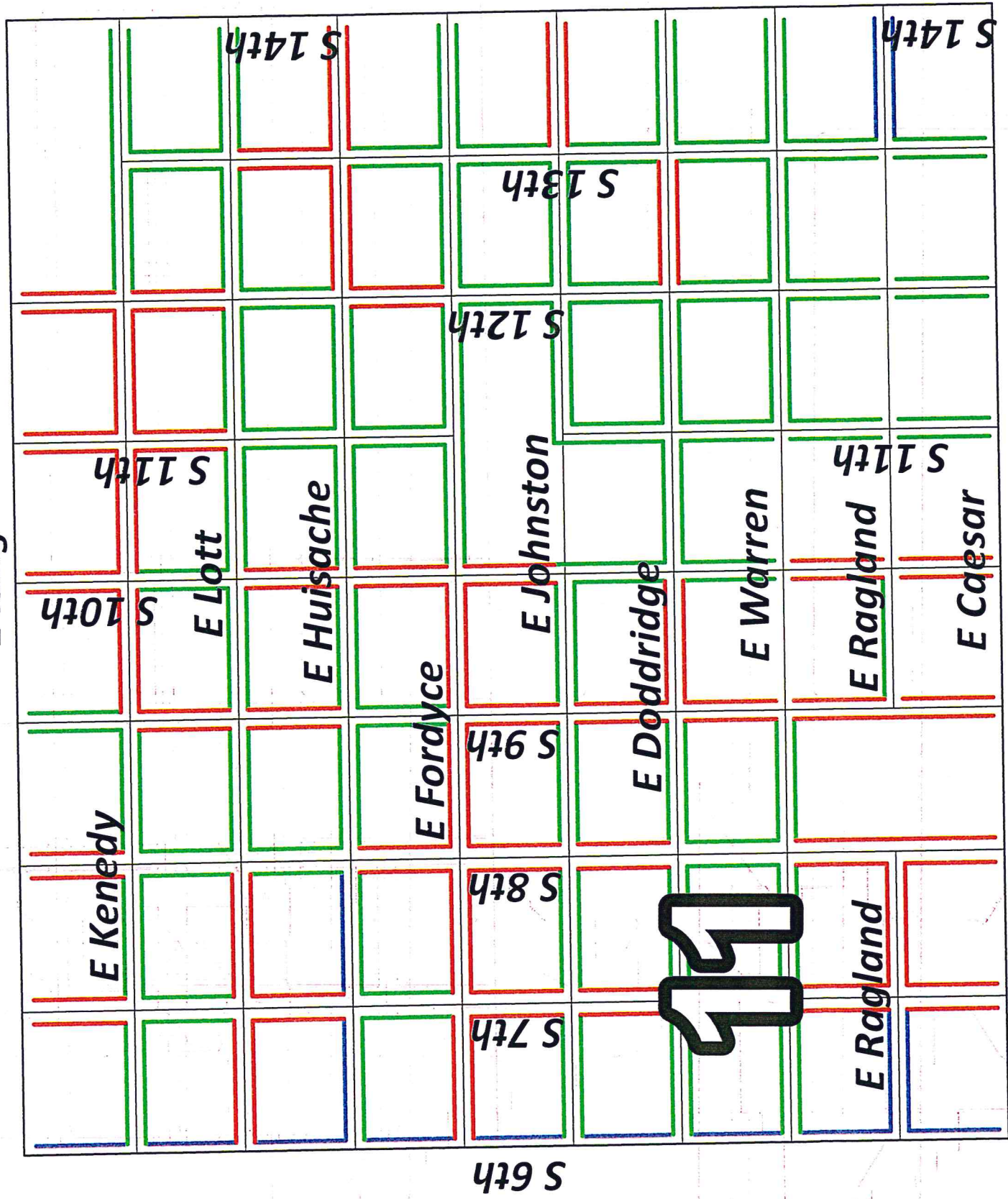
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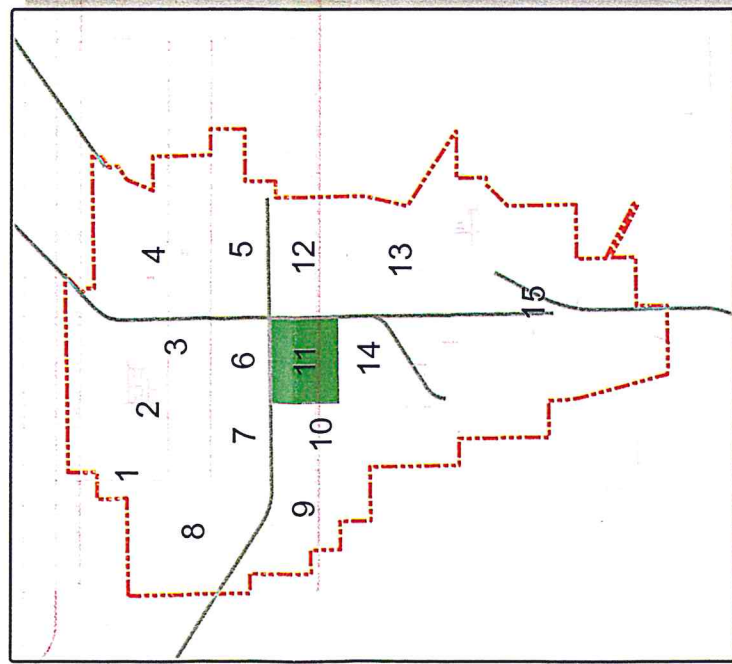
# Zone 11



E King



Blocks = 109  
Curb and Gutter = 86,856 linear ft. ~ 16.45 miles



### Legend

- Curb\_Gutter Inventory Zone 11 Poor
- Curb\_Gutter Inventory Zone 11 Fair
- Curb\_Gutter Inventory Zone 11 Good
- Zone\_11



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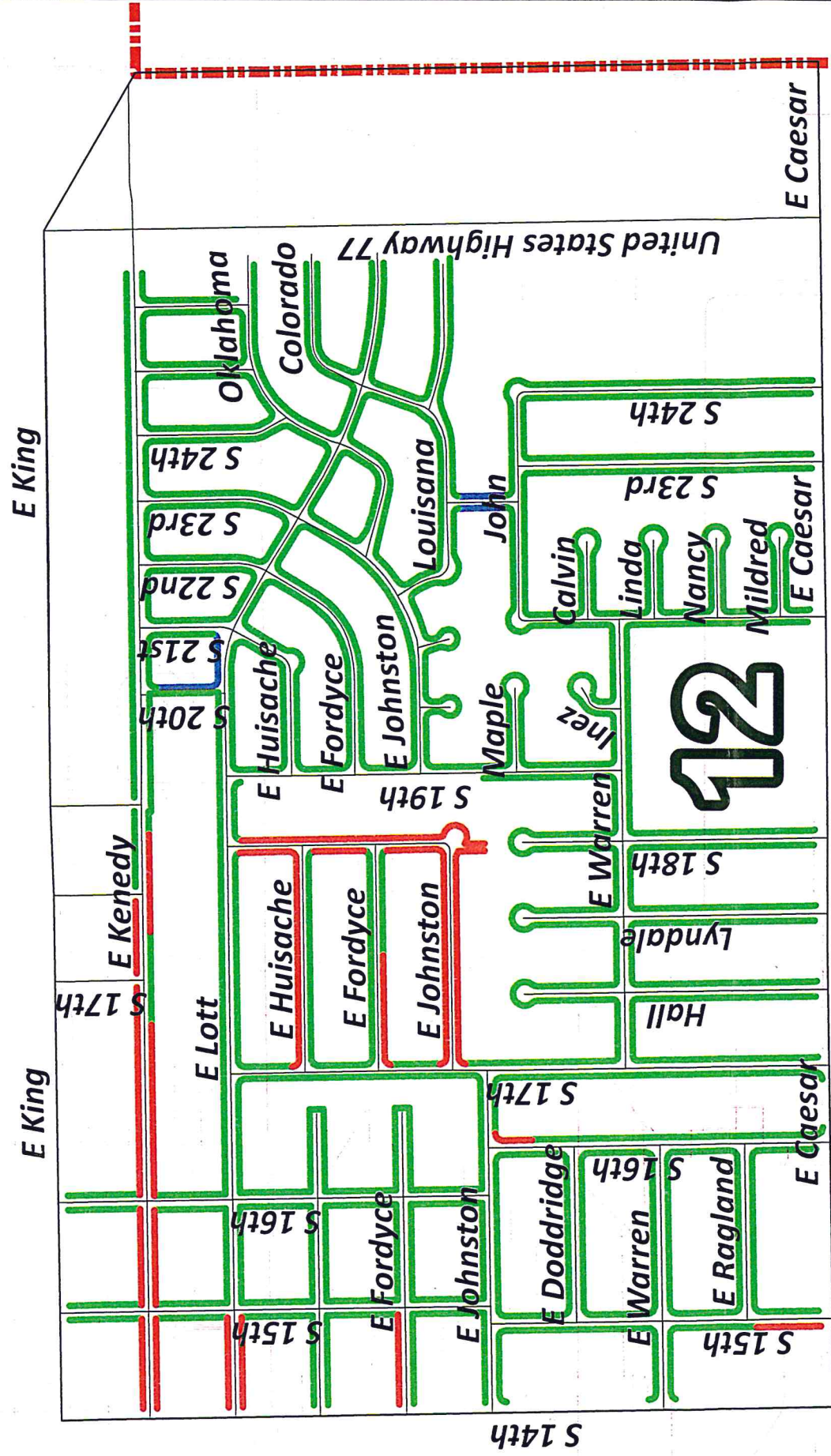


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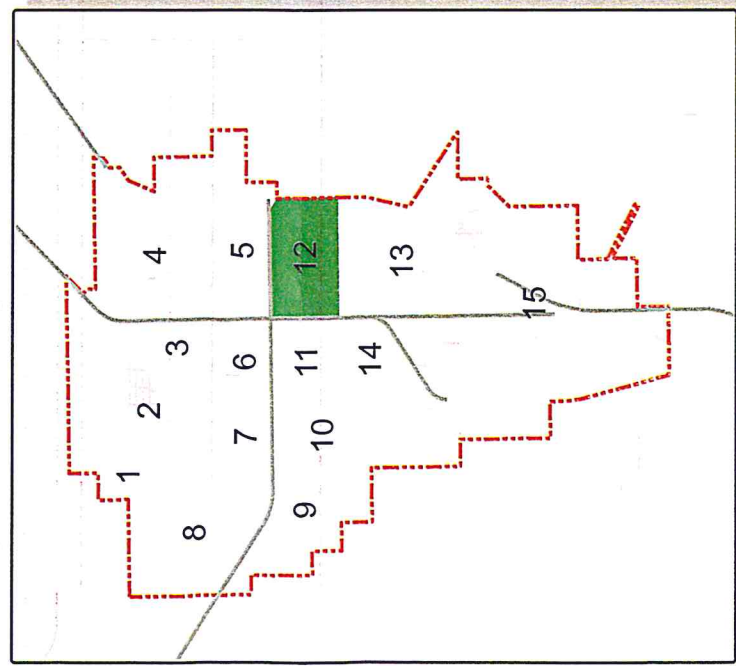
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# Zone 12

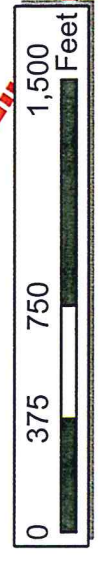


Blocks = 133  
Curb and Gutter = 106,446 linear ft. ~ 20.16 miles




**Legend**

- Curb\_Gutter Inventory Zone 12 Poor
- Curb\_Gutter Inventory Zone 12 Fair
- Curb\_Gutter Inventory Zone 12 Good
- Zone\_12



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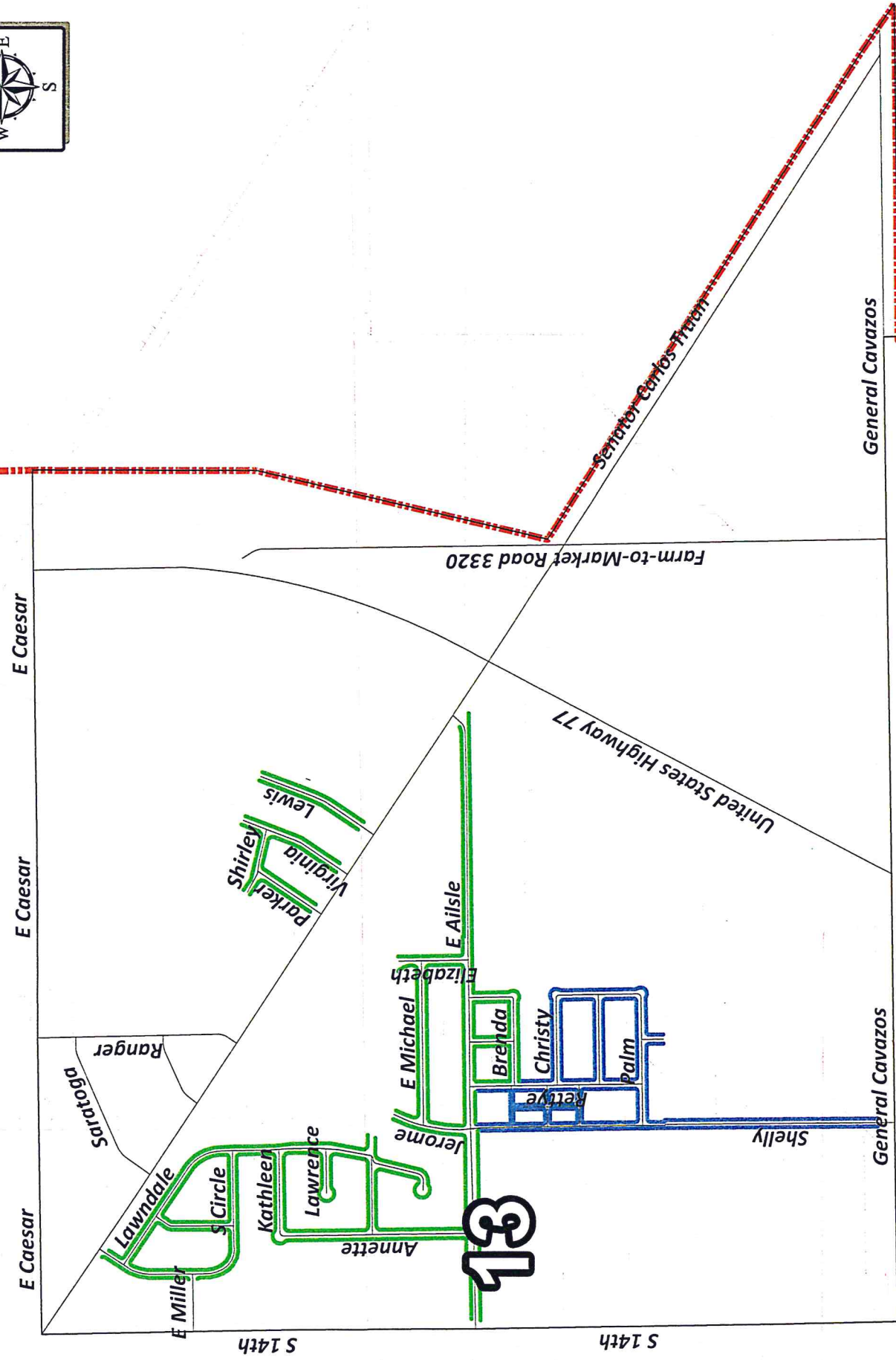
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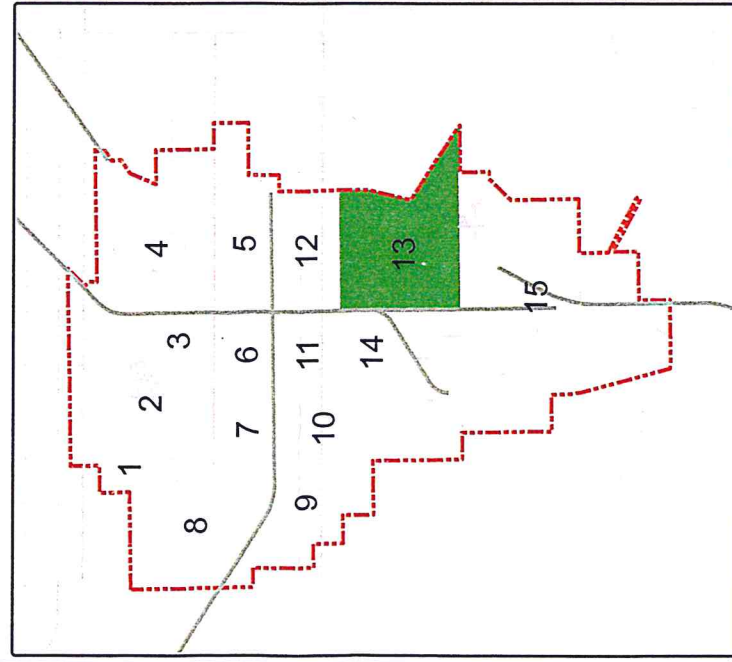
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# Zone 13



Blocks = 62  
 Curb and Gutter = 45,830 linear ft. ~ 8.68 miles



**Legend**

- Curb\_Gutter Inventory Zone 13 Fair
- Curb\_Gutter Inventory Zone 13 Good
- Zone\_13



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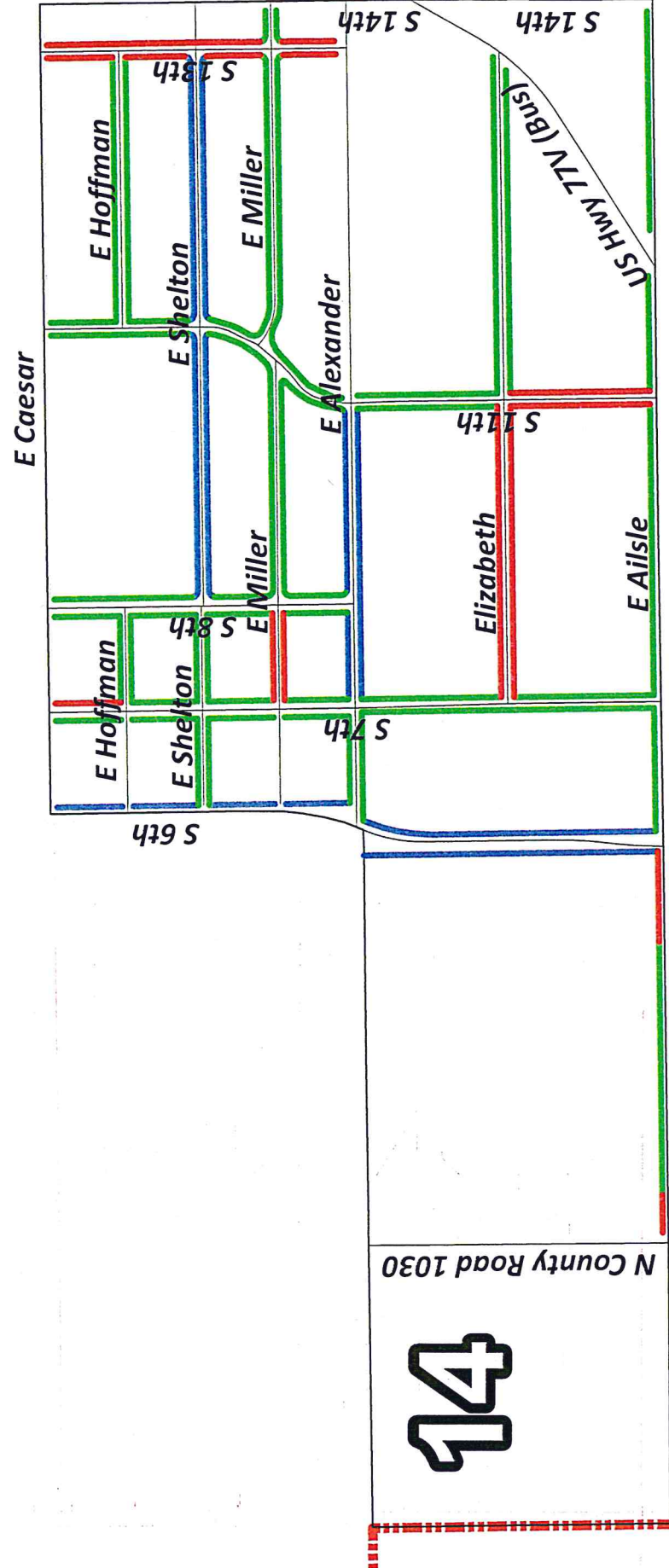
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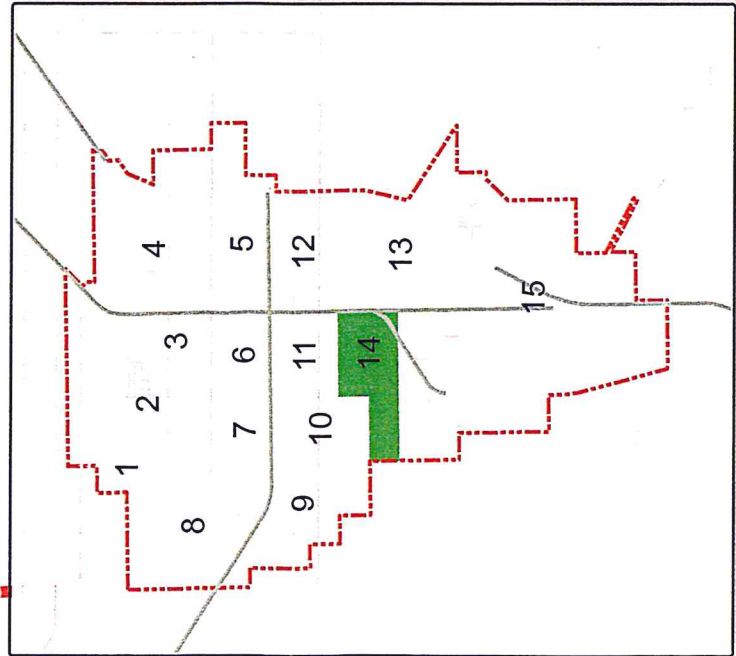
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# Zone 14



**14**  
N County Road 1030

Blocks = 60  
Curb and Gutter = 48,365 linear ft. ~ 9.16 miles




**Legend**

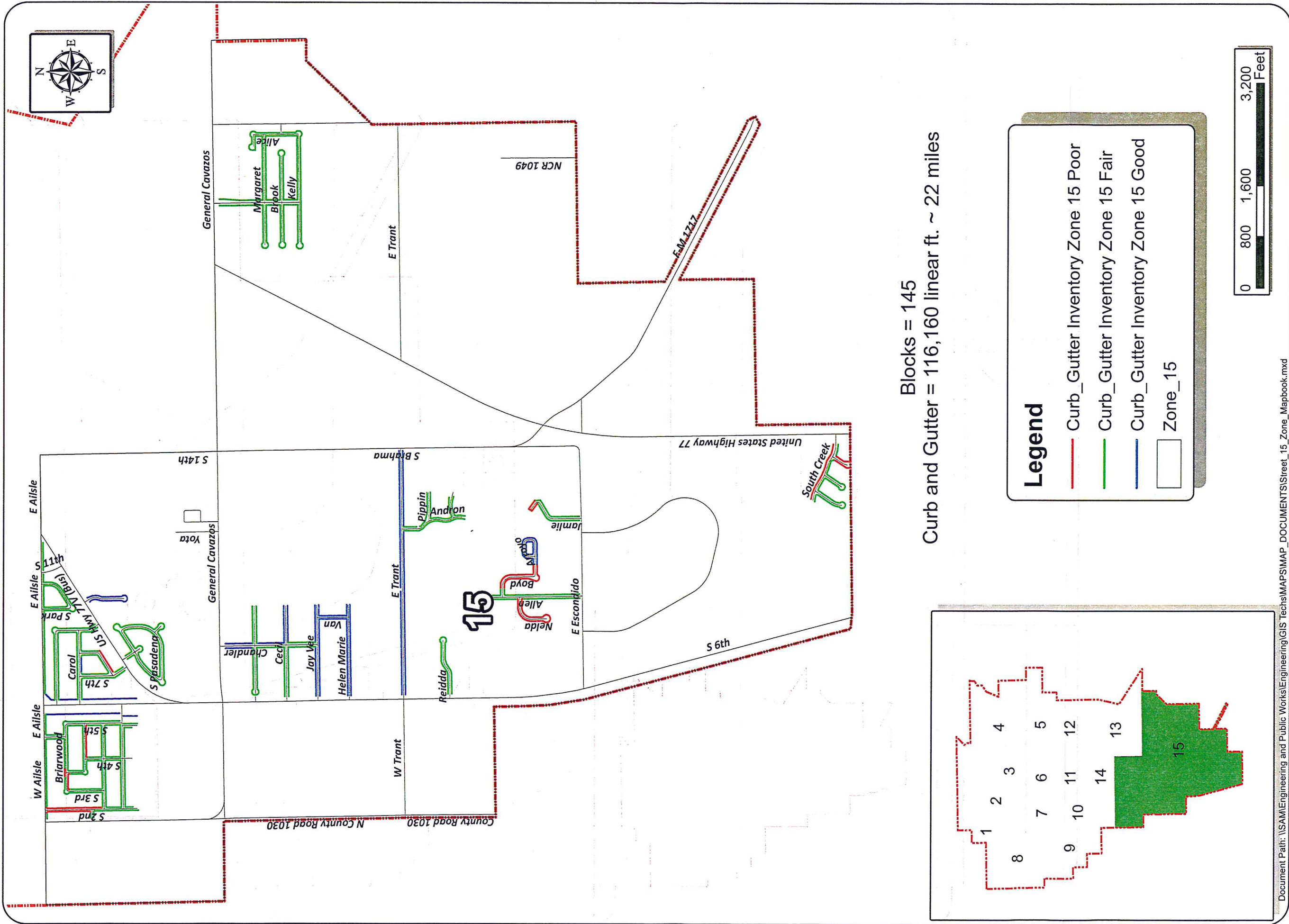
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- Curb\_Gutter Inventory Sone 14 Good
- Zone\_14



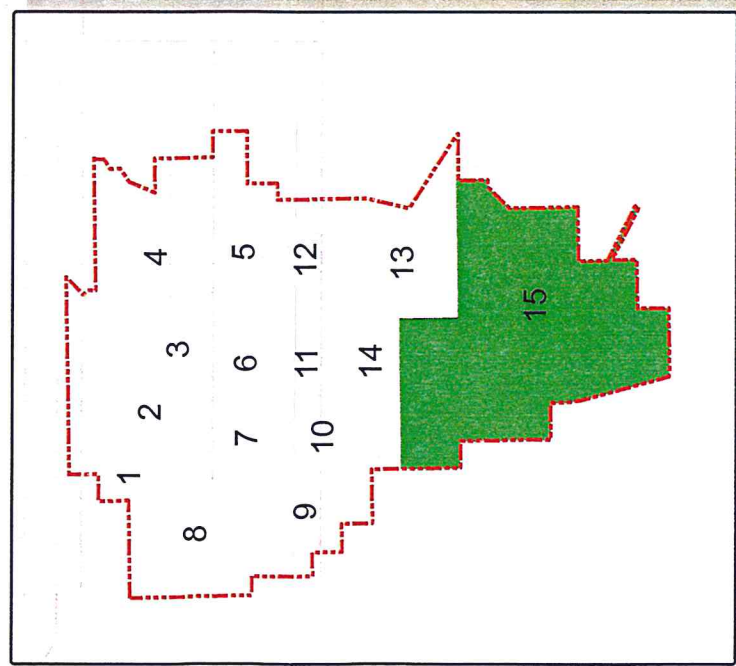
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# Zone 15



Blocks = 145  
Curb and Gutter = 116,160 linear ft. ~ 22 miles




**Legend**

- Curb\_Gutter Inventory Zone 15 Poor
- Curb\_Gutter Inventory Zone 15 Fair
- Curb\_Gutter Inventory Zone 15 Good
- Zone\_15



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