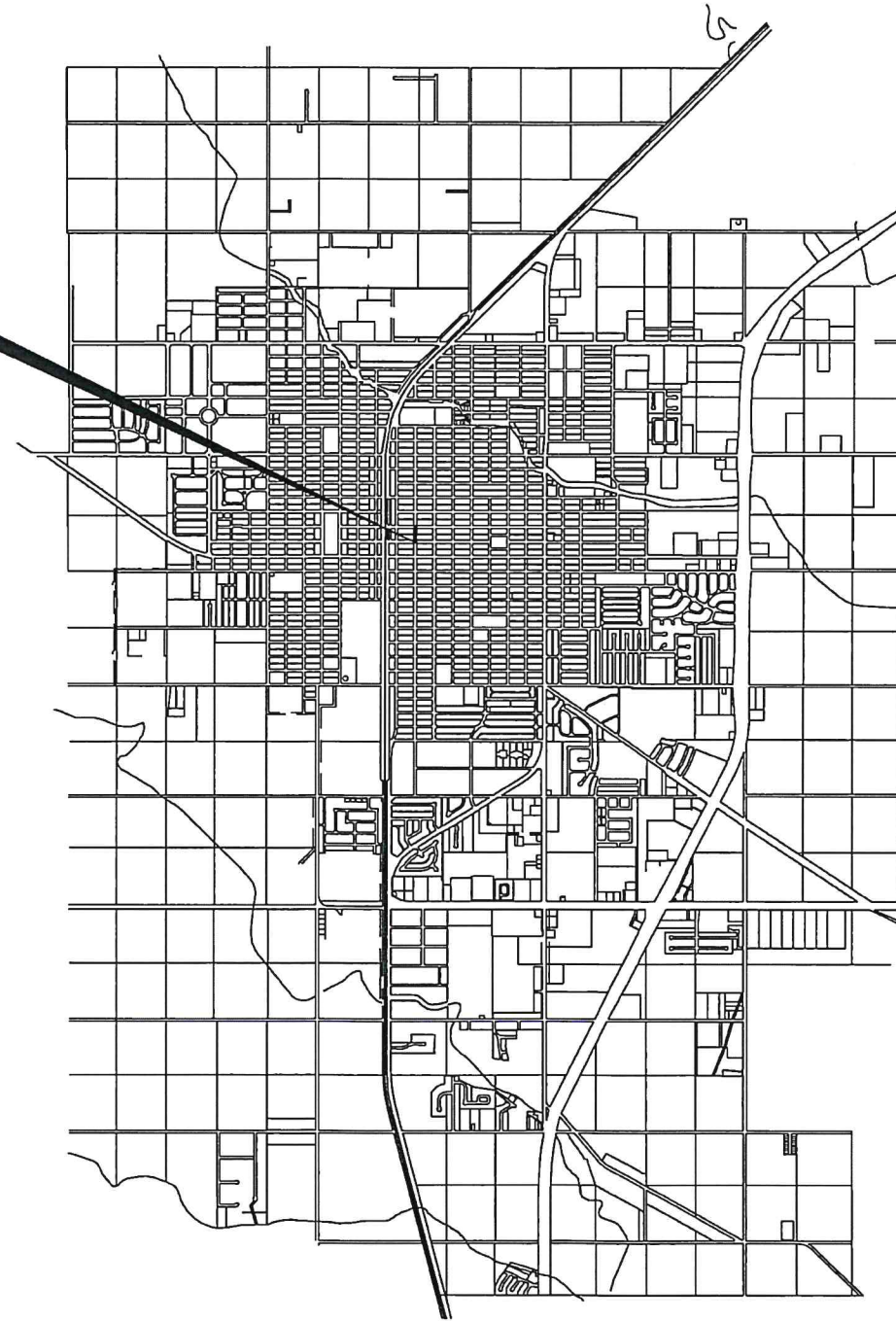


# CITY OF KINGSVILLE

## 2021 DOWNTOWN IMPROVEMENTS - 7TH STREET

**PROJECT LOCATION**  
 7TH ST. FROM  
 KLEBERG AVE. TO YOAKUM AVE.



LOCATION MAP  
NOT TO SCALE

JULY 2020

### SHEET INDEX

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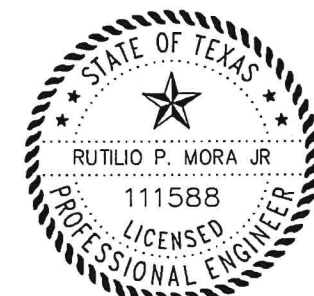
MAYOR  
SAM FUGATE

CITY MANAGER  
MARK MCLAUGHLIN

CITY COMMISSIONERS  
HECTOR M. HINOJOSA  
NORMA NELDA ALVAREZ  
ANN MARIE TORRES  
EDNA LOPEZ

| REVISION: | DATE:      | DESCRIPTION:                  |
|-----------|------------|-------------------------------|
| 1         | 08/14/2020 | CHANGES IN SHEETS AND DETAILS |

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*Rutilio P. Mora Jr* 12/15/2020  
RUTILIO P. MORA JR, P.E. NO. 111588

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Drawn by: V. MARQUEZ  
 Date: 02/01/2020  
 Checked by: R. MORA  
 Job:

**2021 DOWNTOWN IMPROVEMENTS -  
7TH STREET**  
**COVER SHEET**

SHEET  
1

**GENERAL CONSTRUCTION NOTES:**

- ALL IMPROVEMENTS TO BE IN ACCORDANCE WITH CITY OF KINGSVILLE CODES.
- CONTRACTOR TO VERIFY LOCATION AND ELEVATION OF EXISTING FACILITIES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO PLAN AND PERFORM HIS WORK IN A MANNER THAT WILL PERMIT SAFE PUBLIC TRAFFIC MOVEMENT ON ALL STREETS.
- TEMPORARY TRAFFIC CONTROL DURING CONSTRUCTION TO BE IN ACCORDANCE WITH SPECIFICATIONS.
- CONTRACTOR SHALL PROVIDE PROTECTIVE DEVICES SUCH AS SIGNS, LIGHTS, AND SIGNALS FOR THE SAFETY OF THE PUBLIC AND WORKERS, AS REQUIRED, AND AS DIRECTED BY CITY INSPECTOR.
- CONTRACTOR TO BE RESPONSIBLE FOR PROTECTION AND/OR SAFETY OF THE WORK SITE, WORKERS, SUBCONTRACTORS, MATERIALS AND/OR EQUIPMENT.
- CONSTRUCTION STAKING SHALL BE PROVIDED BY THE CONTRACTOR AND AT CONTRACTOR'S EXPENSE. BASE LINES ARE STAKED AS SHOWN ON PLANS. ALL DIMENSIONS ARE TO BACK OF CURBS UNLESS SHOWN OTHERWISE.
- MATERIAL TESTING SHALL BE PROVIDED BY THE CITY OF KINGSVILLE. RE-TEST DUE TO FAILURES TO BE AT CONTRACTOR'S EXPENSE.
- CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL BY THE CITY ENGINEERING OFFICE PRIOR TO CONSTRUCTION. CONTRACTOR MAY CLOSE STREETS TO THRU TRAFFIC IN INCREMENTS AS LONG AS ACCESS IS MAINTAINED TO ALL RESIDENCES, BUSINESSES, & ADJOINING STREETS. TRAFFIC CONTROL PLAN WILL BE IN ACCORDANCE WITH TXDOT'S TCP (2-2) - 18 ON SHEET 20 AND THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED AND AT CONTRACTOR'S EXPENSE.
- ANY DAMAGE TO EXISTING PAVEMENT, DRAINAGE OR EXISTING STRUCTURES SHALL BE REPAIRED TO PRE-CONSTRUCTION CONDITION AT CONTRACTOR'S EXPENSE.
- THESE PLANS, PREPARED BY THE CITY OF KINGSVILLE ENGINEERING DEPARTMENT DO NOT EXTEND TO OR INCLUDE DESIGNS OR SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF CITY OF KINGSVILLE'S REGISTERED PROFESSIONAL ENGINEER(S) HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED IN THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS, INCLUDING THE PLANS AND SPECIFICATIONS REQUIRED BY THE HOUSE BILLS 662 AND 665 ENACTED BY THE TEXAS LEGISLATURE IN THE 70TH LEGISLATURE - REGULAR SESSION."
- CONTRACTOR SHALL GIVE NOTICE TO ALL AUTHORIZED INSPECTORS, SUPERINTENDENTS OR PERSONS IN CHARGE OF PRIVATE AND PUBLIC UTILITIES AFFECTED BY HIS OPERATIONS PRIOR TO COMMENCEMENT OF WORK. NOTIFY TEXAS ONE CALL FOR UTILITY LOCATIONS PRIOR TO ANY & ALL EXCAVATIONS. COORDINATION OF ALL RELOCATION OF UTILITY POLES, ETC. TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR TO COORDINATE WITH THE CITY OF KINGSVILLE ON WORK SCHEDULES, TESTING, GENERAL INSPECTION, AND EXISTING FACILITIES AND/OR UTILITIES.
- CONTRACTOR TO EXERCISE CAUTION WHEN WORKING NEAR EXISTING FACILITIES AND/OR UTILITIES. ALL DAMAGE TO BE REPAIRED AT CONTRACTOR'S EXPENSE. ALL COSTS FOR INTERRUPTION OF GAS, ELECTRICAL, COMMUNICATIONS AND/OR WATER SERVICE DUE TO CONTRACTOR'S WORK SHALL BE BORNE BY THE CONTRACTOR.
- INFORMATION ON EXISTING UTILITIES IS FROM BEST AVAILABLE INFORMATION OF RECORD AND SPOT FIELD LOCATIONS. CONTRACTOR IS RESPONSIBLE FOR FIELD LOCATION OF THESE UNDERGROUND UTILITIES AS REQUIRED AT NO SEPARATE PAY. CITY OF KINGSVILLE PERSONNEL WILL BE AVAILABLE FOR ASSISTANCE AND OPERATION OF VALVES AS REQUIRED. CONTRACTOR TO COORDINATE WITH OTHER UTILITY COMPANIES, INCLUDING AEP ON ELECTRICAL UTILITIES, CENTERPOINT ENERGY ON GAS UTILITIES AND SOUTHWESTERN BELL ON TELEPHONE UTILITIES.
- ALL SPOIL MATERIAL AND DEBRIS SHALL BE DISPOSED OF BY CONTRACTOR. FURNISHING AND TRANSPORTATION OF ALL OFFSITE MATERIAL TO BE AT CONTRACTOR'S EXPENSE.
- UPON COMPLETION OF CONSTRUCTION, CONTRACTOR SHALL RETURN THE SITE TO ORIGINAL CONTOURS UNLESS DIFFERENT FINISHED ELEVATIONS ARE SHOWN ON PLANS. CONTRACTOR TO INSURE NO AREAS OF PONDING ARE PRESENT.
- CONTRACTOR TO INSURE SAME DAY ACCESS TO SCHOOL, ALL RESIDENCES AND BUSINESSES ADJACENT TO CONSTRUCTION.
- DEMOLITION, REMOVAL & DISPOSAL OF ALL EXCESS CONCRETE, CURBS, RUBBLE, ETC. TO BE AT CONTRACTOR'S EXPENSE.
- STREET & SUBGRADE EXCAVATION TO BE THE PROPERTY OF THE CITY AND TO BE STOCK PILED AT THE CITY OF KINGSVILLE LANDFILL AS DIRECTED BY THE CITY REPRESENTATIVE.
- CONCRETE NOTES:
  - ALL CONCRETE WORK TO BE FORMED. UNLESS OTHERWISE APPROVED.
  - ALL CONCRETE TO BE 3000 PSI MINIMUM AT 28 DAYS, UNLESS OTHERWISE SHOWN. STRENGTH TO BE DETERMINED BY CYLINDER BREAK TEST.
  - ALL REINFORCING STEEL TO BE ASTM A-615, GRADE 60. UNLESS OTHERWISE SHOWN.
  - ALL EXPOSED CONCRETE WORK TO BE CHAMFERED.
- CONTRACTOR TO VERIFY THAT ALL CONCRETE SURFACES MEET THE FOLLOWING SLOPE CRITERIA PRIOR TO PLACEMENT OF CONCRETE.
  - RAMP SLOPE 12:1 (8.33%) (MAX.)
  - RUNNING SLOPE 20:1 (5%) (MAX.)
  - CROSS SLOPE 50:1 (2%) (MAX.)

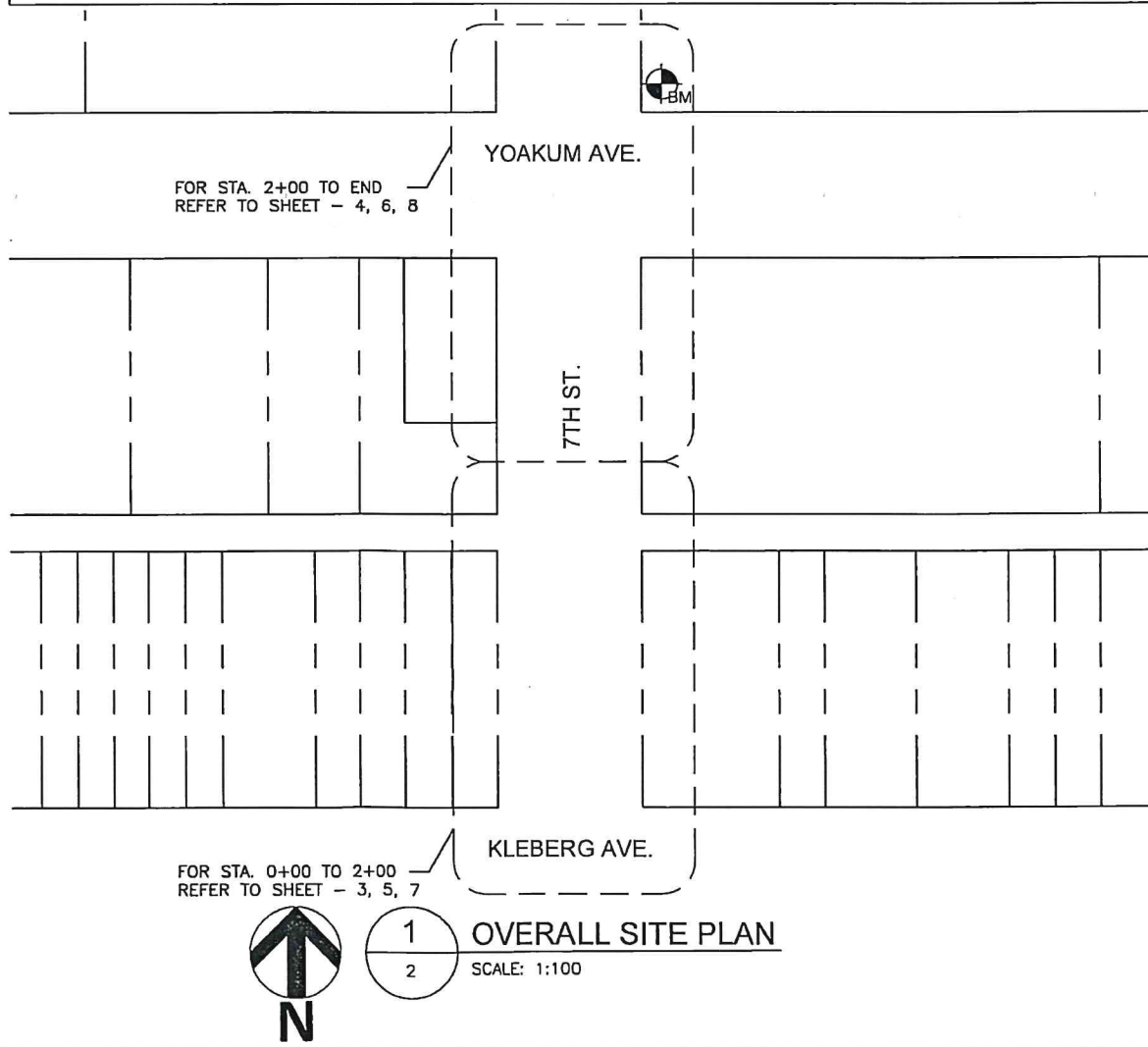
(CROSS SLOPE IS SLOPE PERPENDICULAR TO THE DIRECTION OF TRAVEL.)
- CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING SILT FENCE IN AREAS OF DISTURBED SOIL TO PREVENT EROSION FROM ENTERING DRIVEWAYS, INLETS, AND STORMWATER SYSTEM.

**PROTECTION OF HISTORIC FEATURES DURING CONSTRUCTION:**

PROTECTION NOTES FOR THE REMOVAL OF EXISTING PAVEMENT, CURB OR SIDEWALK AND CONSTRUCTION OF NEW PAVEMENT, CURB OR SIDEWALK ADJACENT TO HISTORIC BUILDINGS, CANOPIES, MATERIALS, FENCES, AND RETAINING WALLS WHERE PROPOSED WORK IS IN PROXIMITY TO HISTORIC BUILDINGS OR OTHER STRUCTURES (WALLS, CANOPIES, RETAINING WALLS, FENCES), AND PLANTING BEDS, AND VEGETATION/GROUNDCOVER, FOLLOW THE PROCEDURES LISTED BELOW FOR DEMOLITION, PROTECTION, AND CONSTRUCTION AT THESE ADDRESSES:  
IN THE CITY OF KINGSVILLE, AT THE 200 BLK. OF 7TH ST.:

- TO MINIMIZE POTENTIAL DAMAGE TO HISTORIC STRUCTURES AND MATERIALS, CONTRACTOR TO SAW CUT EXISTING SIDEWALK 8 TO 12 INCHES AWAY FROM THE HISTORIC STRUCTURE, CANOPY SUPPORTS, FENCE, OR RETAINING WALL.
- CONTRACTOR TO CONSTRUCT NEW SIDEWALK NEXT TO THE SAW CUT EDGE WITH INSTALLATION OF EXPANSION JOINT IN BETWEEN. IF EXISTING SIDEWALK IS TO BE REMOVED ENTIRELY, THE REMAINING 8 TO 12 INCHES NEXT TO THE HISTORIC STRUCTURE, CANOPY SUPPORTS, MATERIAL, FENCE, OR RETAINING WALL WILL BE REMOVED BY HAND. EXPANSION JOINT TO BE PLACED BETWEEN HISTORIC STRUCTURE, CANOPY SUPPORT, MATERIAL, FENCE, OR RETAINING WALL AND NEW SIDEWALK.
- CONTRACTOR IS RESPONSIBLE FOR PREVENTING DAMAGE TO HISTORIC STRUCTURE, CANOPY SUPPORTS AND THEIR AWNING, MATERIALS, FENCES, RETAINING WALLS, INCLUDING GARDEN ELEMENTS (PLANTING BEDS, PLANTINGS) DURING THE ENTIRE CONSTRUCTION PROJECT, ESPECIALLY DURING REMOVAL OF EXISTING PAVEMENT, CURB, OR SIDEWALK. DURING THE SAW CUT AND HAND REMOVAL PROCESS, CONTRACTOR WILL EXERCISE UTMOST CAUTION AND WILL PHYSICALLY PROTECT HISTORIC STRUCTURE FOUNDATION, CANOPY SUPPORTS, MATERIALS, ELEVATIONS, ENTRYWAYS WITH DECORATIVE FLOORING, FENCES, RETAINING WALLS, AND LANDSCAPE ELEMENTS.
- CONTRACTOR TO REPAIR OR REPLACE IN KIND, AT HIS OWN EXPENSE, ANY HISTORIC MATERIALS DAMAGED IN THE COURSE OF EXECUTING THE WORK. CONTRACTOR IS RESPONSIBLE FOR LOCATING REPLACEMENT SOURCE FOR HISTORIC MATERIALS DAMAGED IN THE COURSE OF THE WORK.

| EXISTING |                   | LEGEND |                          | PROPOSED |                 |
|----------|-------------------|--------|--------------------------|----------|-----------------|
|          | LIGHT POLE        |        | DOOR SWING               |          | DECORATIVE LAMP |
|          | STREET LAMP       |        | TOP OF CURB              |          | SLOPE           |
|          | WATER VALVE       |        | GUTTER                   |          | SIGN            |
|          | FIRE HYDRANT      |        | FINISH FLOOR             |          | TOP OF CURB     |
|          | POWER POLE        |        | TOP OF WALK              |          | GUTTER          |
|          | GUY WIRE          |        | TOP OF STEP              |          | TOP OF STEP     |
|          | WATER METER       |        | TOP OF PAVEMENT          |          | TOP OF WALK     |
|          | BRICK PAVERS      |        | SLOPE                    |          | BRICK PAVERS    |
|          | SIGN              |        | GRASS AREA               |          | SAWCUT          |
|          | TREE              |        | ELECTRIC BOX             |          | BUILT UP RAMP   |
|          | SIDEWALK DRAIN    |        | SIDEWALK                 |          | STATIONING      |
|          | VALLEY GUTTER     |        | MANHOLE                  |          |                 |
|          | FIBER OPTICS      |        | GRATE INLET              |          |                 |
|          | OVERHEAD ELECTRIC |        | STORM SEWER JUNCTION BOX |          |                 |
|          | FENCE             |        | STORM SEWER              |          |                 |
|          | WASTEWATER        |        |                          |          |                 |
|          | WATER             |        |                          |          |                 |
|          | GAS               |        |                          |          |                 |
|          | BUILDING EDGE     |        |                          |          |                 |



**CALL BEFORE YOU DIG!**

Know what's below. Call before you dig.

811

THE LONE STAR NOTIFICATION COMPANY  
AT 1-800-669-8344

BENCHMARK ELEVATION: 61.57'

NORTHEAST CORNER OF 7TH STREET AND YOAKUM AVENUE AT THE EXISTING PARKING LOT LIGHT. THERE IS A 60D NAIL ON THE SOUTH SIDE OF THE LIGHT POLE BASE IN THE CENTER ON THE ASPHALT.

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*Rutilio P. Mora Jr.* 7/20/2020  
RUTILIO P. MORA JR, P.E. NO. 111588

CITY OF KINGSVILLE  
ENGINEERING DEPARTMENT

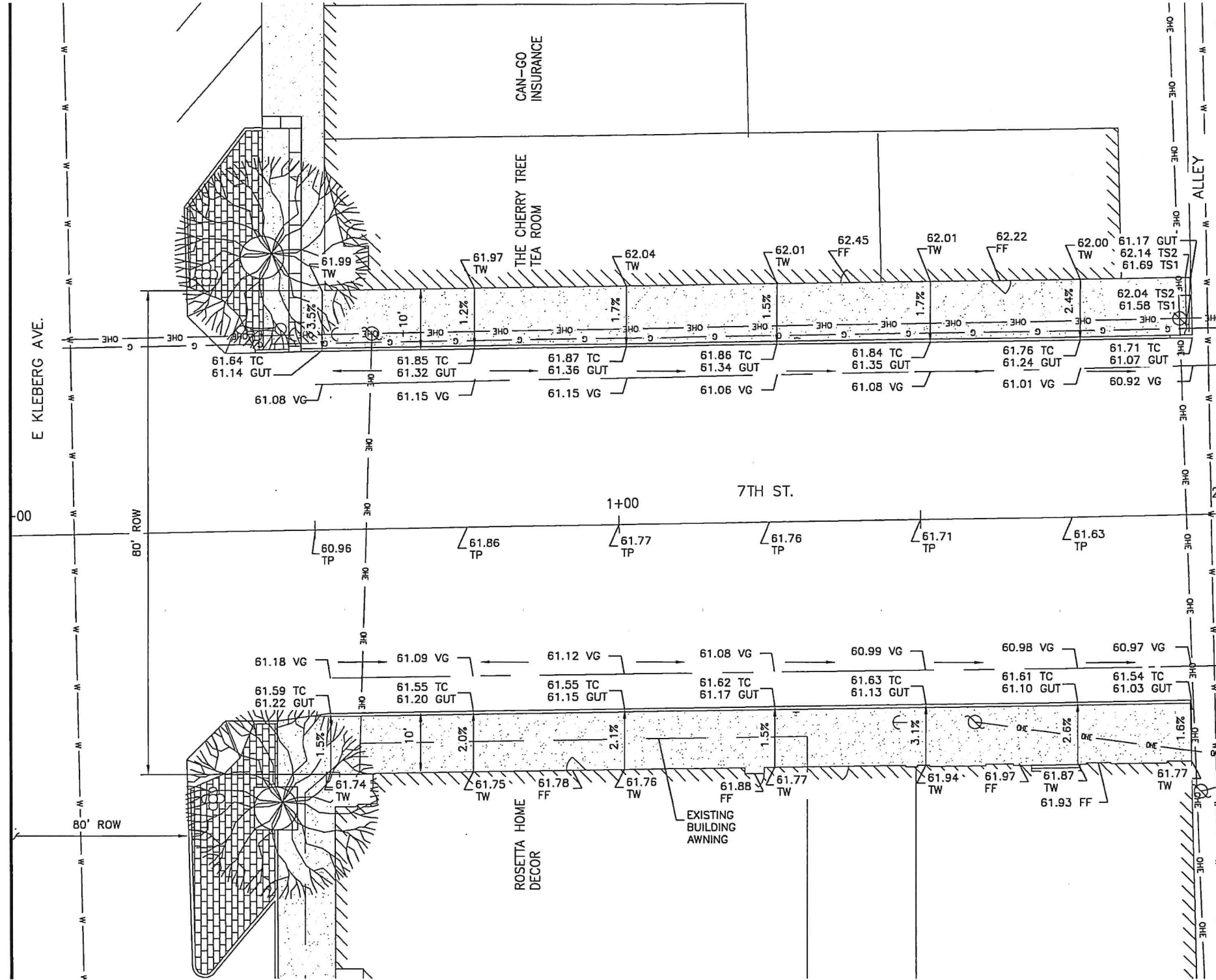
400 West King  
Kingsville, Texas 78363  
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Fax 361.595.8035



Drawn by: V. MARQUEZ  
Date: 02/01/2020  
Checked by: R. MORA  
Job:

**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**GENERAL NOTES & OVERALL SITE PLAN**

MATCHLINE STA. 0 + 0 0

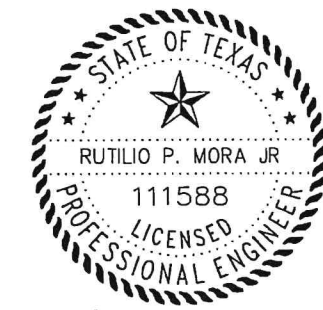


MATCHLINE STA. 2 + 0 0



A  
3

STA. 0+00 TO STA. 2+00 (EXISTING PLAN)  
SCALE: 1:20



*Rutilio P. Mora Jr.* 7/20/2020  
RUTILIO P. MORA JR., P.E. NO. 111588

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**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
STA. 0+00 TO STA. 2+00 (EXISTING PLAN)**

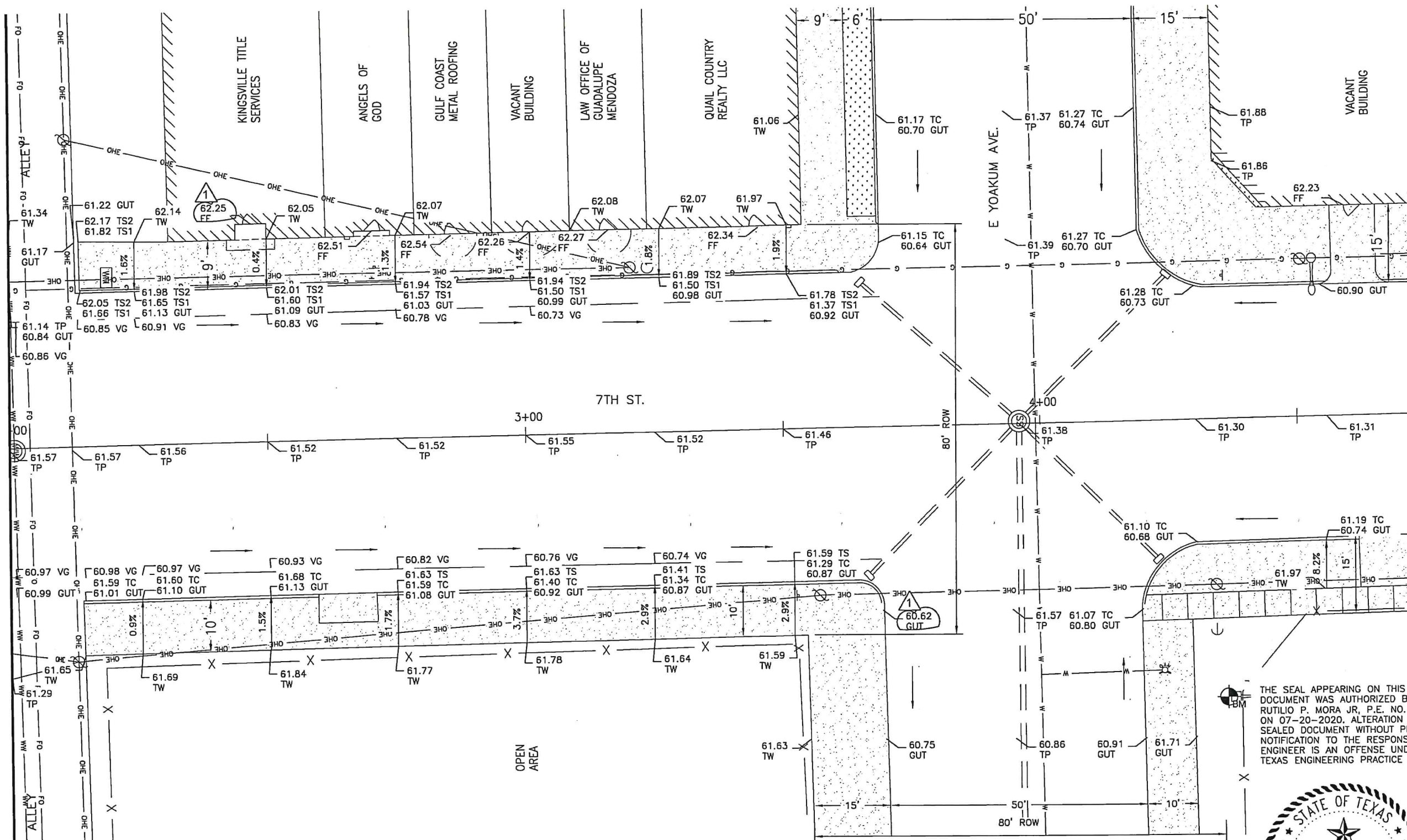
CITY OF KINGSVILLE  
ENGINEERING DEPARTMENT  
400 West King  
Kingsville, Texas 78363  
Office 361.595.8007  
Fax 361.595.8035



Drawn by: V. MARQUEZ  
Date: 02/01/2020  
Checked by: R. MORA  
Job:  
Scale: N.T.S.

MATCHLINE STA. 2 + 0 0

MATCHLINE END

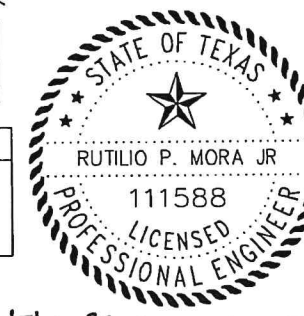


A  
4

STA. 2+00 TO END (EXISTING PLAN)

SCALE: 1:20

| REVISION: | DATE:      | DESCRIPTION:                                     |
|-----------|------------|--------------------------------------------------|
| 1         | 08/14/2020 | ADDITIONAL ELEVATIONS TO FINISH FLOOR AND GUTTER |



*Rutilio P. Mora Jr*, 8/14/2020

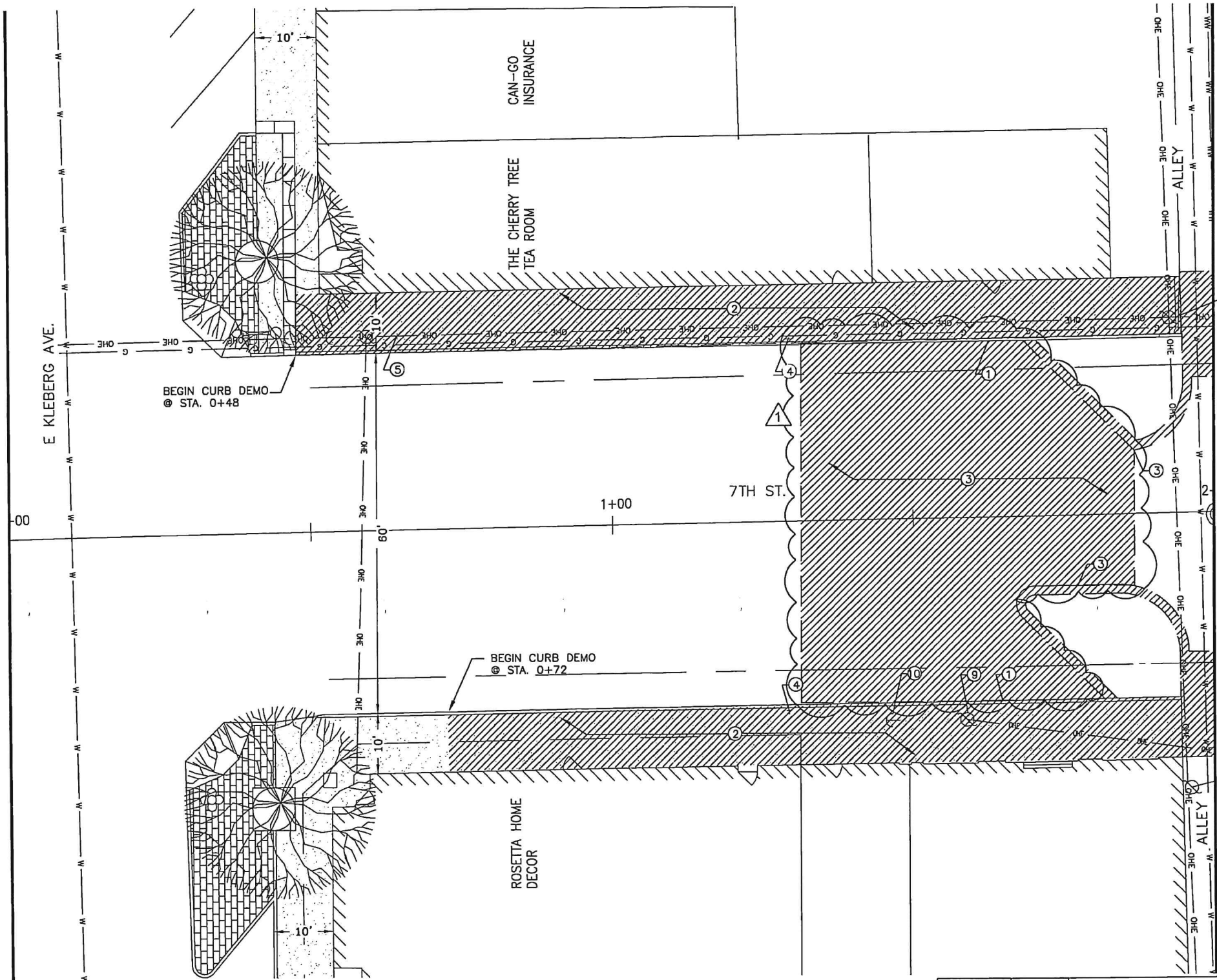
RUTILIO P. MORA JR, P.E. NO. 111588

**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
STA. 2+00 TO END (EXISTING PLAN)**

Drawn by: V. MARQUEZ  
Date: 02/01/2020  
Checked by: R. MORA  
Job:  
Scale: N.T.S.

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MATCHLINE STA. 0 + 0 0



MATCHLINE STA. 2 + 0 0

**DEMOLITION NOTES**

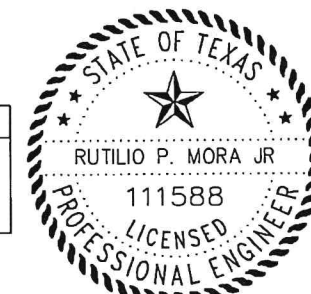
- ① REMOVE EXISTING CURB
- ② REMOVE EXISTING SIDEWALK
- ③ REMOVE EXISTING PAVEMENT
- ④ REMOVE SIGN & PROVIDE TO CITY
- ⑤ RELOCATE STOP SIGN ON NEW POST
- ⑥ REMOVE GRATE INLETS
- ⑦ DEMO STORM SEWER PIPING
- ⑧ DEMO JUNCTION BOX
- ⑨ EXISTING POLE TO REMAIN
- ⑩ EXISTING GUY WIRE TO REMAIN

- AREAS TO DEMOLISH
- AREAS TO SAWCUT
- AREAS TO DEMOLISH (ALT. BID 1)
- AREAS TO SAWCUT (ALT. BID 1)



**A** STA. 0+00 TO STA. 2+00 (DEMOLITION PLAN)  
5 SCALE: 1:20

| REVISION: | DATE:      | DESCRIPTION:             |
|-----------|------------|--------------------------|
|           | 08/14/2020 | ADDITIONAL PAVEMENT DEMO |



*Rutilio P. Mora Jr.* 8/14/2020  
RUTILIO P. MORA JR., P.E. NO. 111588

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**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET**

STA. 0+00 TO STA. 2+00 (DEMOLITION PLAN)

SHEET  
5

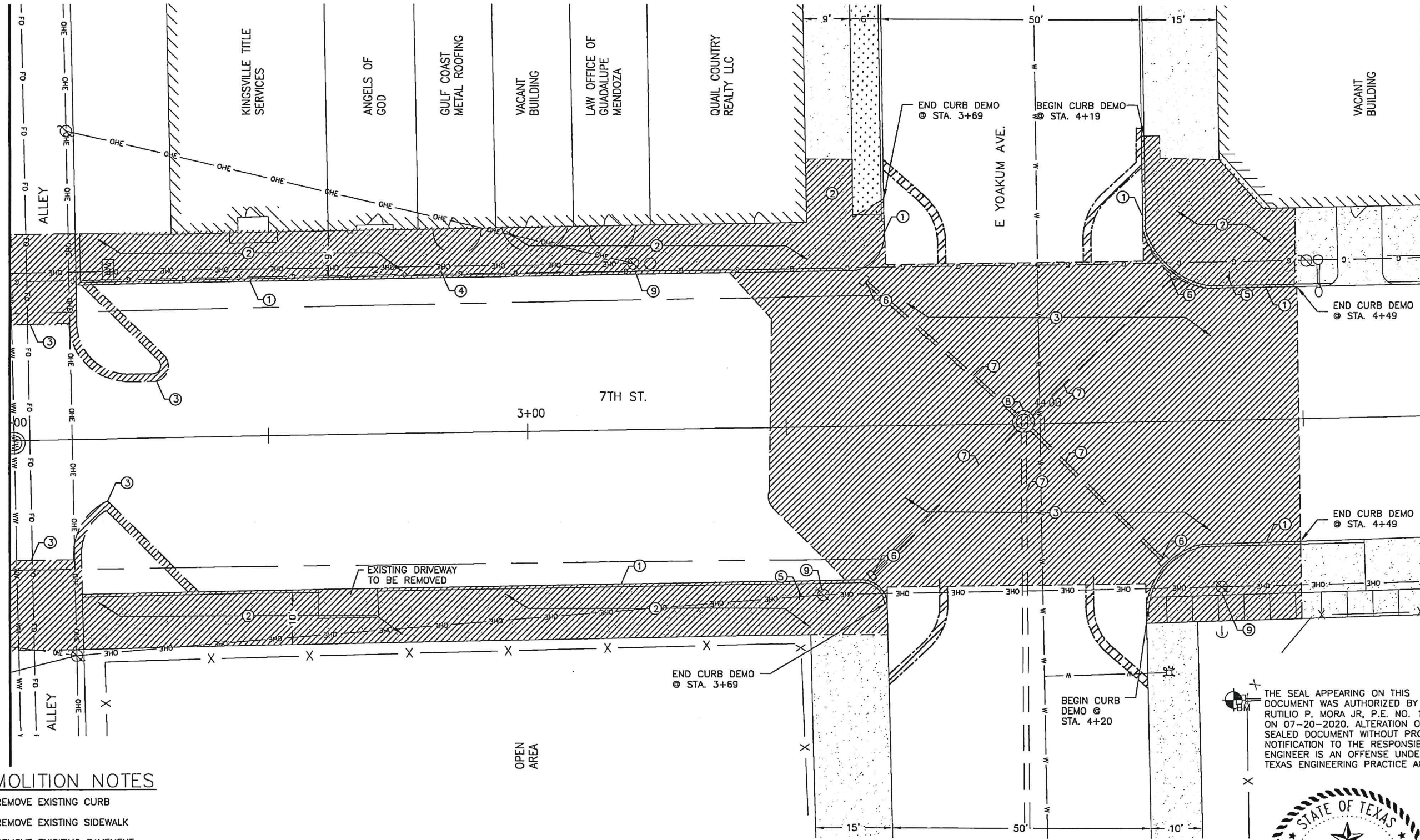
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Date: 02/03/2020  
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Job:  
Scaler: N.T.S.



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MATCHLINE STA. 2 + 0 0

MATCHLINE END



**DEMOLITION NOTES**

- ① REMOVE EXISTING CURB
- ② REMOVE EXISTING SIDEWALK
- ③ REMOVE EXISTING PAVEMENT
- ④ REMOVE SIGN & PROVIDE TO CITY
- ⑤ RELOCATE STOP SIGN ON NEW POST
- ⑥ REMOVE GRATE INLETS
- ⑦ DEMO STORM SEWER PIPING
- ⑧ DEMO JUNCTION BOX
- ⑨ EXISTING POLE TO REMAIN
- ⑩ EXISTING GUY WIRE TO REMAIN

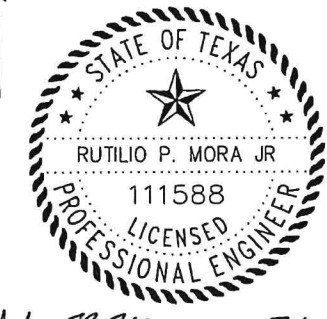
- AREAS TO DEMOLISH
- AREAS TO SAWCUT
- AREAS TO DEMOLISH (ALT. BID 1)
- AREAS TO SAWCUT (ALT. BID 1)



A  
6  
SCALE: 1:20

**STA. 2+00 TO END (DEMOLITION PLAN)**

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*Rutilio P. Mora Jr* 7/20/2020  
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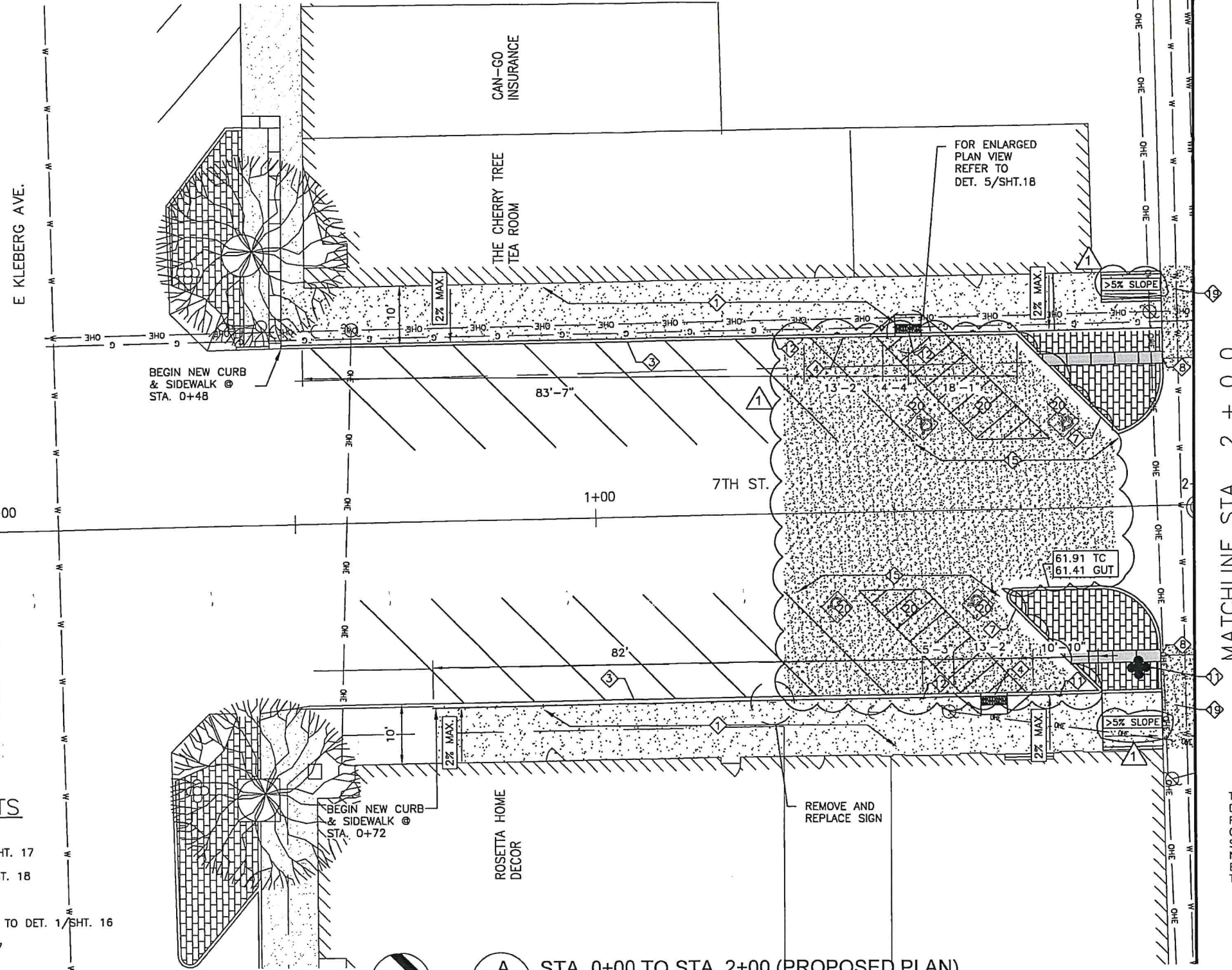


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Checked by: R. MORA  
Job:  
Scale: N.T.S.

**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
STA. 2+00 TO END (DEMOLITION PLAN)**

SHEET  
6

MATCHLINE STA. 0 + 0 0



**PROPOSED IMPROVEMENTS**

- ① NEW SIDEWALK, REFER TO DET. 1/SHT. 15
- ② NEW CONCRETE STEPS, REFER TO DET. 6/SHT. 17
- ③ NEW CONCRETE CURB, REFER TO DET. 4/SHT. 18
- ④ NEW ADA RAMP, REFER TO DET. 1/SHT. 16
- ⑤ NEW ADA LANDING (TURNING SPACE), REFER TO DET. 1/SHT. 16
- ⑥ NEW CROSSWALK, REFER TO DET. 3/SHT. 17
- ⑦ NEW BRICK PAVERS (MATCH WITH EXISTING AT KLEBERG AVE), REFER TO DET. 5/SHT. 17
- ⑧ NEW SIDEWALK DRAIN, REFER TO DET. 1/SHT. 17
- ⑨ NEW CURB INLET, REFER TO DET. 4/SHT. 10
- ⑩ NEW BUILT UP RAMP, REFER TO DET. 2/SHT. 18
- ⑪ NEW DECORATIVE LAMP WITH BASE, REFER TO SHT. 14
- ⑫ NEW SIGN, REFER TO DET. 4/SHT. 17
- ⑬ RELOCATED STOP SIGN
- ⑭ STOP BAR, REFER TO DET. 3/SHT. 17

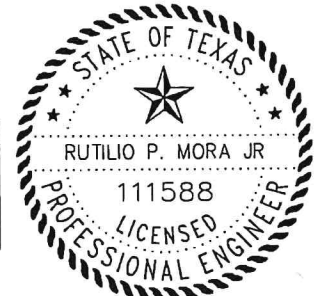
- ⑮ NEW ADA PARKING MARKING AND SYMBOLS, REFER TO DET. 1/SHT. 18
- ⑯ NEW JUNCTION BOX, REFER TO DET. 4/SHT. 9
- ⑰ CITY SHALL ADJUST WATERLINE
- ⑱ GAS COMPANY SHALL ADJUST EXISTING LINE IF NEEDED
- ⑲ NEW CONCRETE PAVEMENT
- ⑳ ADA PARKING AND ACCESS ISLE SHALL HAVE 2% MAX SLOPE IN ANY DIRECTION

A  
7

STA. 0+00 TO STA. 2+00 (PROPOSED PLAN)

SCALE: 1:20

| REVISION: | DATE:      | DESCRIPTION:                                           |
|-----------|------------|--------------------------------------------------------|
| ①         | 08/14/2020 | ADDITIONAL ADA SLOPES, NEW PAVEMENT, ADA PARKING NOTES |



*Rutilio P. Mora Jr.* 8/14/2020  
RUTILIO P. MORA JR., P.E. NO. 111588

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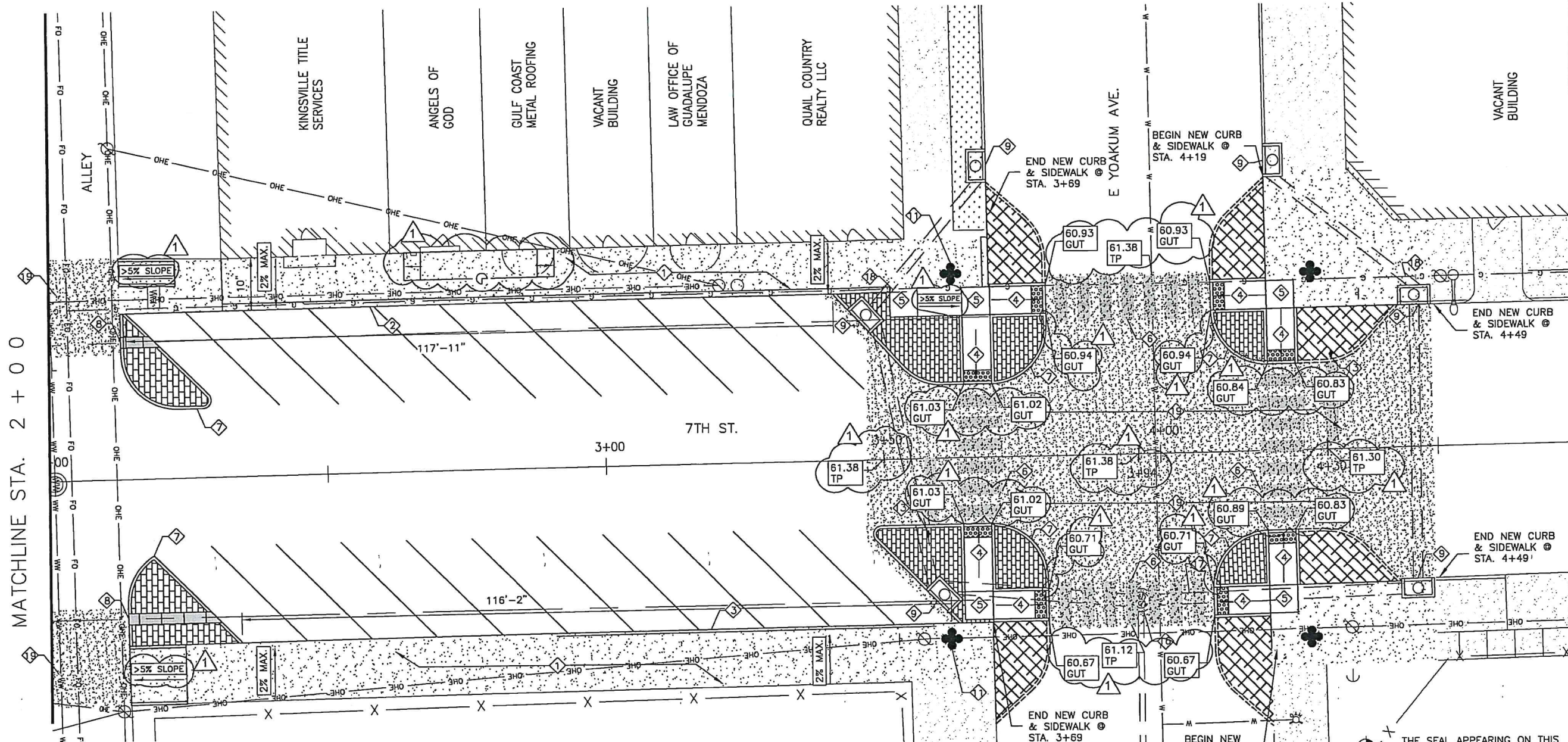
**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET**

STA. 0+00 TO STA. 2+00 (PROPOSED PLAN)

Drawn by: V. MARQUEZ  
Date: 02/01/2020  
Checked by: R. MORA  
Job:  
Scale: N.T.S.

CITY OF KINGSVILLE  
ENGINEERING DEPARTMENT  
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Kingsville, Texas 78363  
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Fax 361.595.8035

NOTE:  
WATER LINE AND GAS LINE  
ARE APPROX. 3' DEEP



**PROPOSED IMPROVEMENTS**

- ① NEW SIDEWALK, REFER TO DET. 1/SHT. 15
- ② NEW CONCRETE STEPS, REFER TO DET. 6/SHT. 17
- ③ NEW CONCRETE CURB, REFER TO DET. 4/SHT. 18
- ④ NEW ADA RAMP, REFER TO DET. 1/SHT. 16
- ⑤ NEW ADA LANDING (TURNING SPACE), REFER TO DET. 1/SHT. 16
- ⑥ NEW CROSSWALK, REFER TO DET. 3/SHT. 17
- ⑦ NEW BRICK PAVERS (MATCH WITH EXISTING AT KLEBERG AVE), REFER TO DET. 5/SHT. 17
- ⑧ NEW SIDEWALK DRAIN, REFER TO DET. 1/SHT. 17
- ⑨ NEW CURB INLET, REFER TO DET. 4/SHT. 10
- ⑩ NEW BUILT UP RAMP, REFER TO DET. 2/SHT. 18
- ⑪ NEW DECORATIVE LAMP WITH BASE, REFER TO SHT. 14
- ⑫ NEW SIGN, REFER TO DET. 4/SHT. 17
- ⑬ RELOCATED STOP SIGN
- ⑭ STOP BAR, REFER TO DET. 3/SHT. 17
- ⑮ NEW ADA PARKING MARKING AND SYMBOLS, REFER TO DET. 1/SHT. 18
- ⑯ NEW JUNCTION BOX, REFER TO DET. 4/SHT. 9
- ⑰ CITY SHALL ADJUST WATERLINE
- ⑱ GAS COMPANY SHALL ADJUST EXISTING LINE IF NEEDED
- ⑲ NEW CONCRETE PAVEMENT



**A** STA. 2+00 TO END (PROPOSED PLAN)  
8 SCALE: 1:20

**ALTERNATE BID ITEMS**

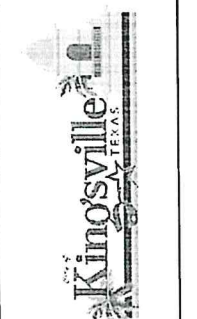
- NEW BRICK PAVERS (ALT. BID 1)
- NEW DECORATIVE LAMP (ALT. BID 2)
- NEW CURB (ALT. BID 1)

| REVISION: | DATE:      | DESCRIPTION:                                        |
|-----------|------------|-----------------------------------------------------|
| ①         | 08/14/2020 | ADDITIONAL SLOPES AND ELEVATIONS, NEW SIDEWALK RAMP |



*Rutilio P. Mora Jr.* 8/14/2020  
RUTILIO P. MORA JR., P.E. NO. 111588

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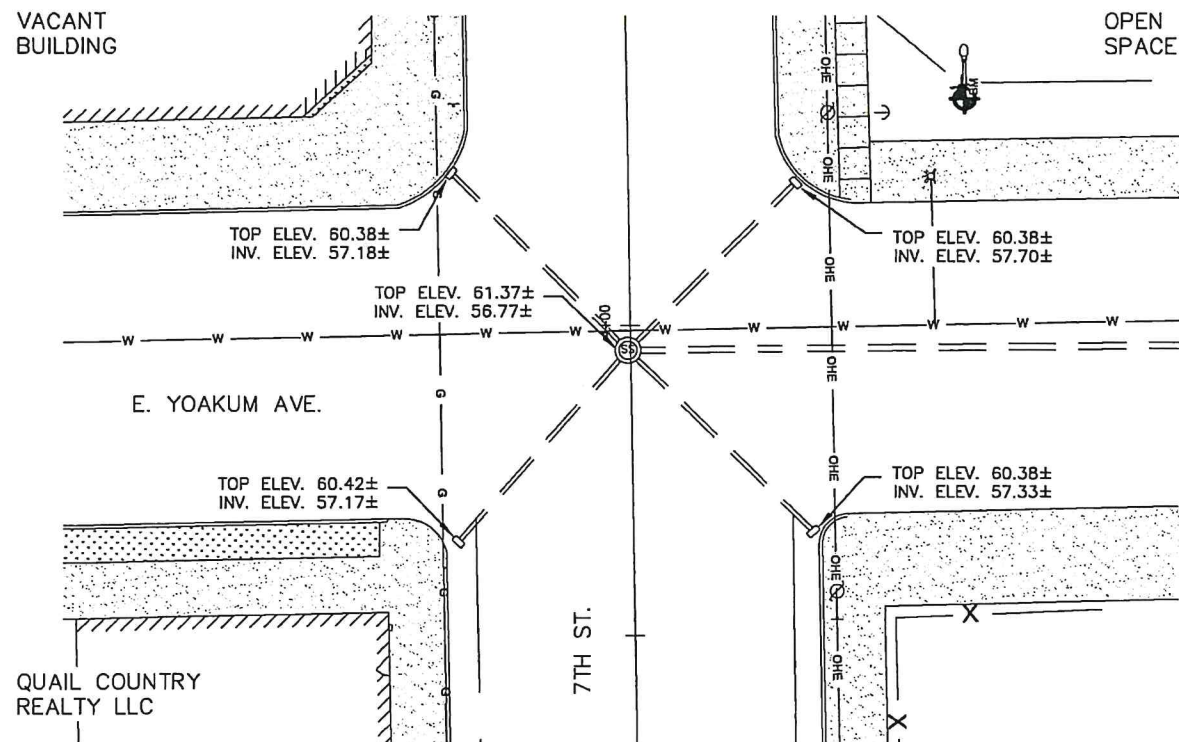


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Checked by: R. MORA  
Job:  
Scale: N.T.S.

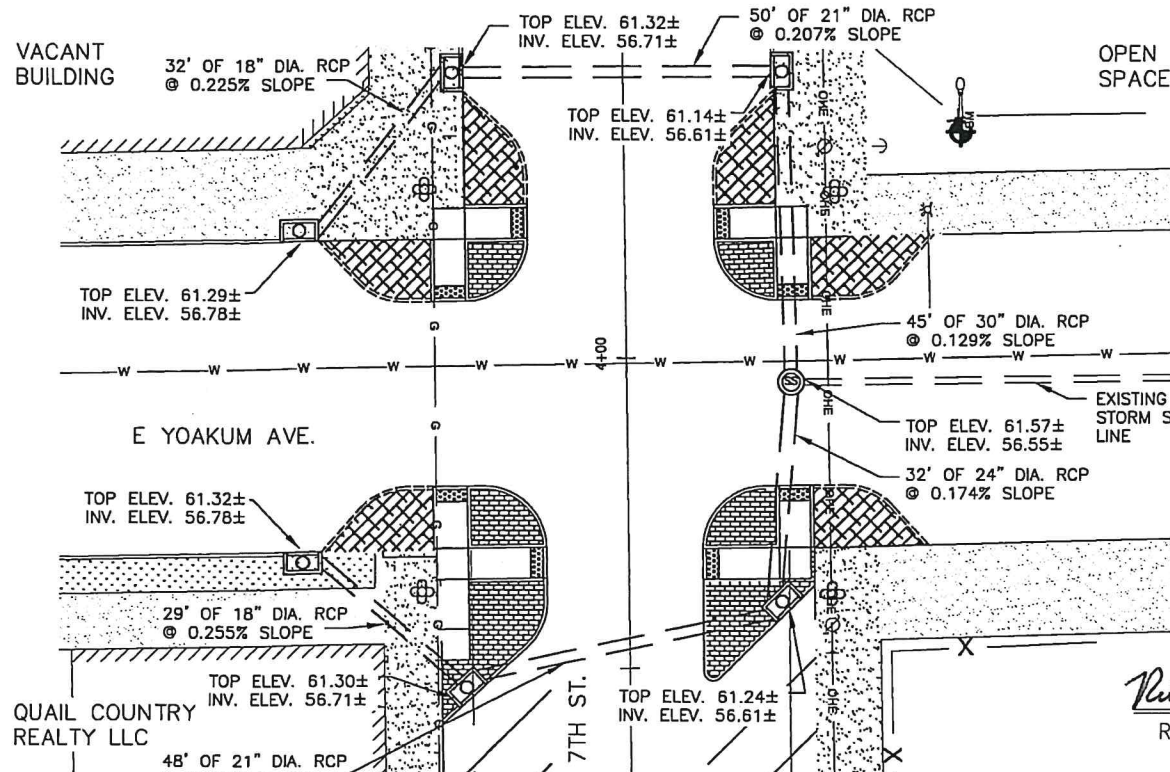
**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**STA. 2+00 TO END (PROPOSED PLAN)**

**SHEET**  
8



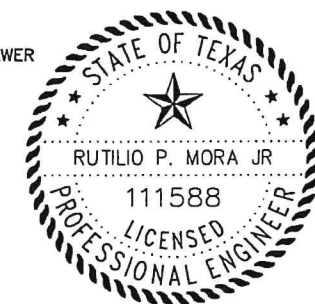


**1** EXISTING STORMWATER DRAINAGE  
SCALE: 1:20

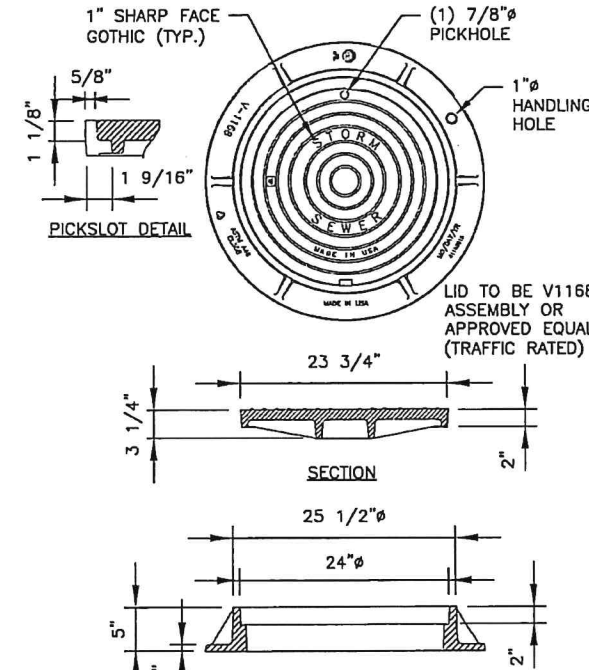
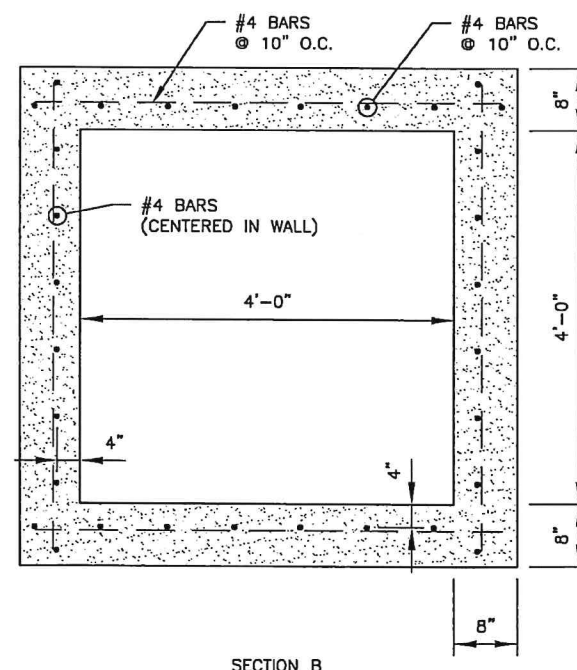
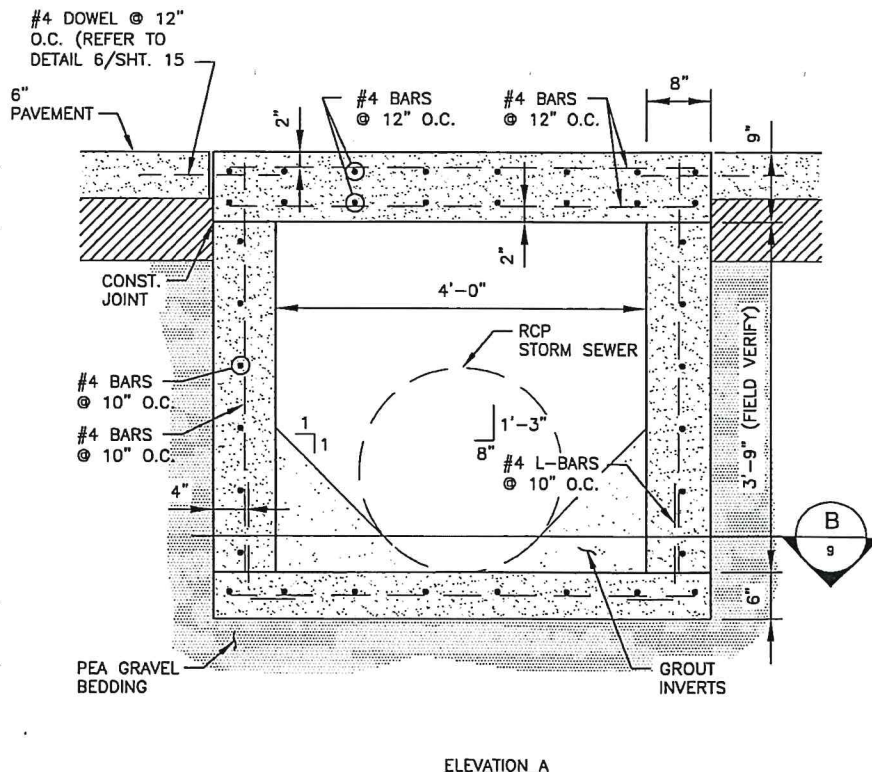
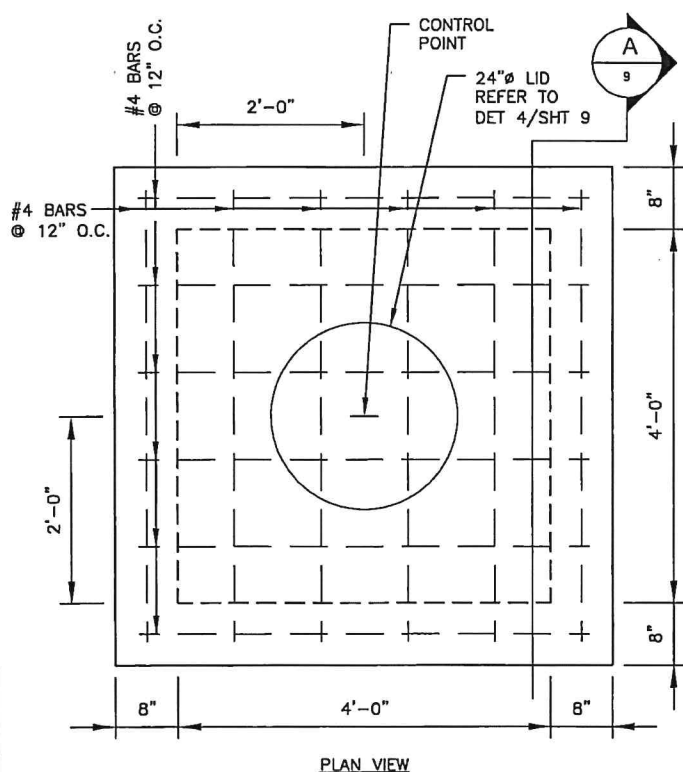


**2** PROPOSED STORMWATER DRAINAGE  
SCALE: 1:20

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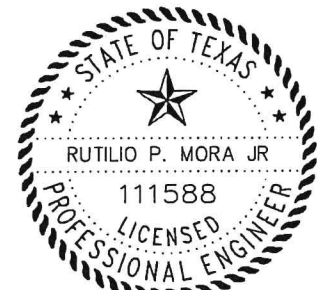
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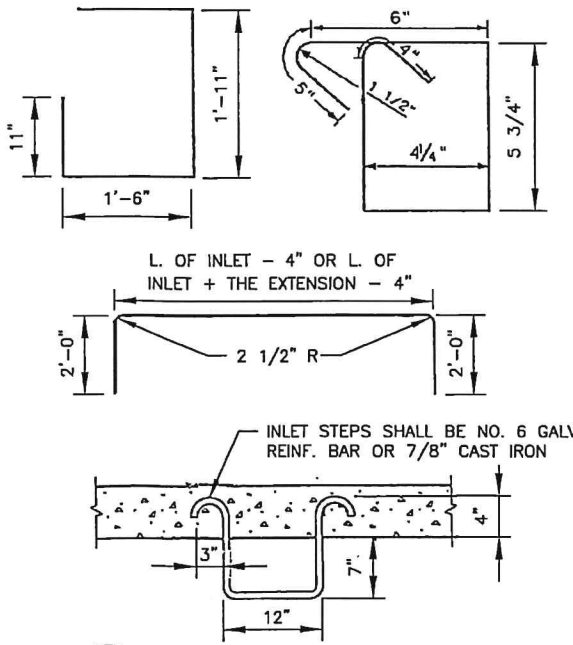
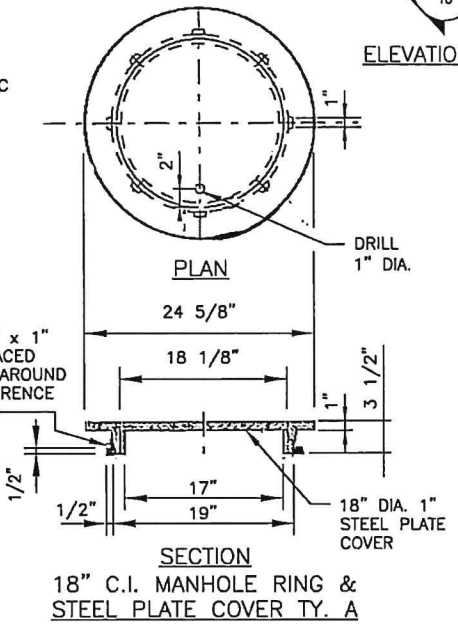
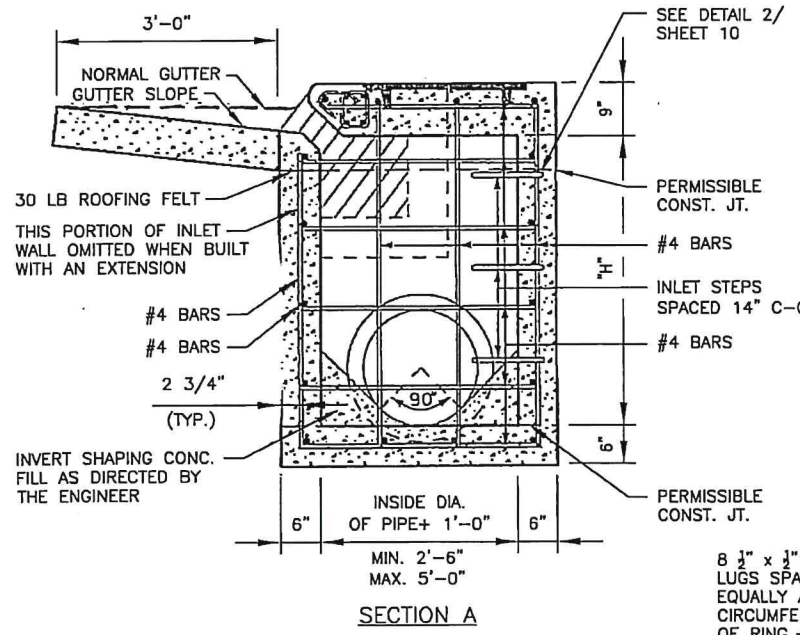
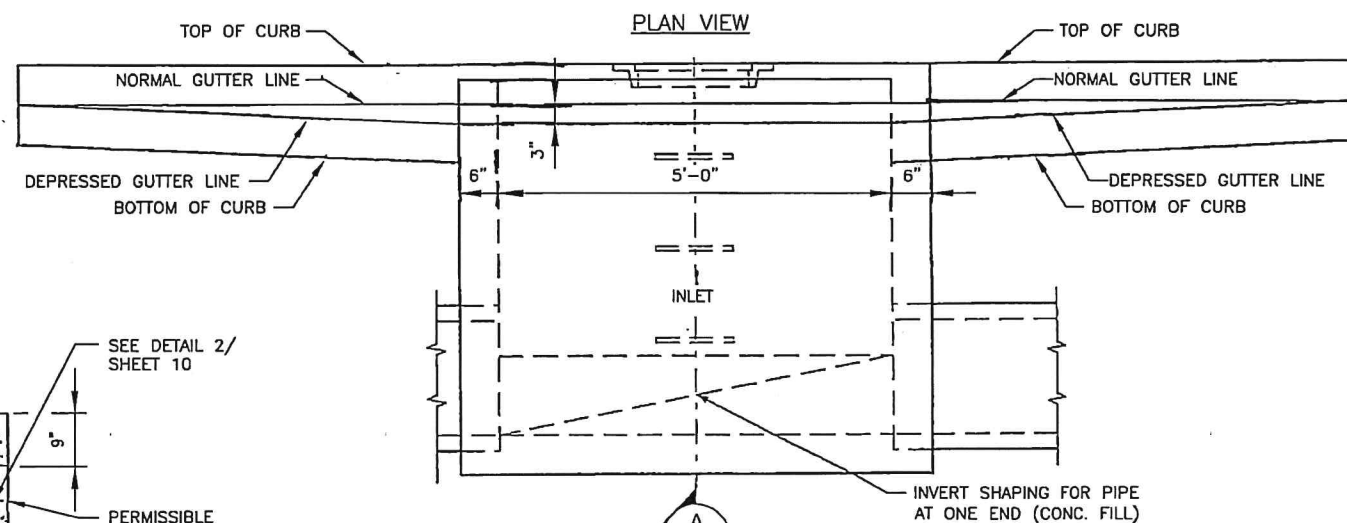
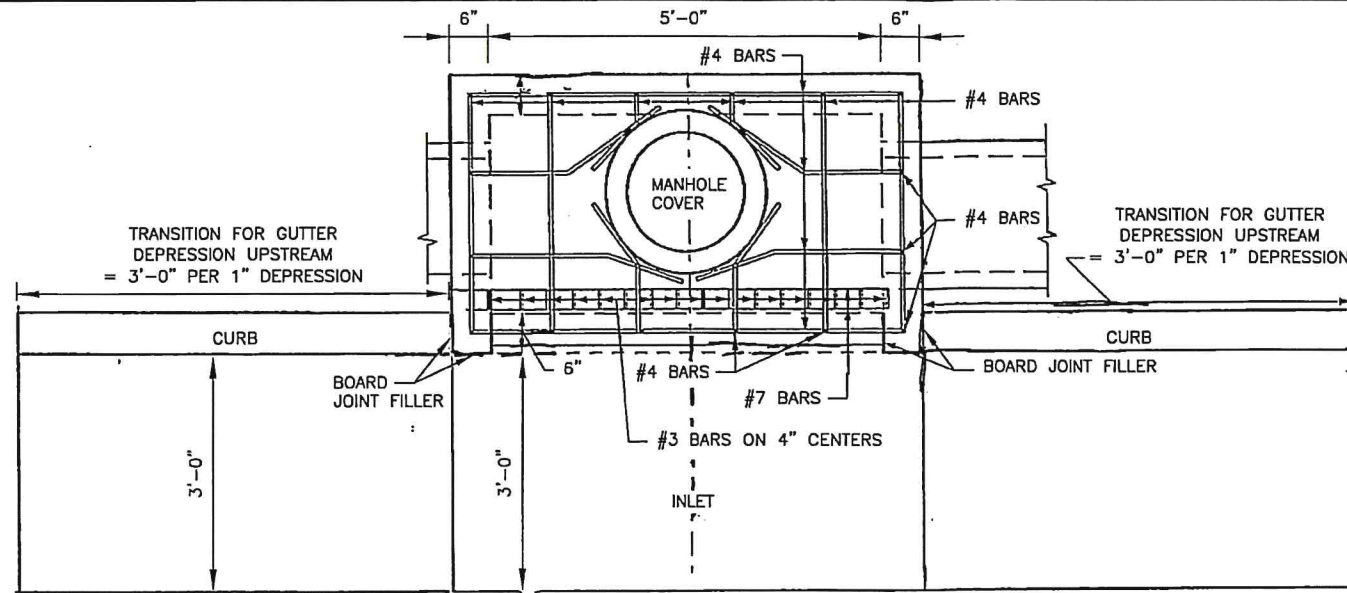
**3** PROPOSED JUNCTION BOX  
SCALE: N.T.S.

**4** JUNCTION BOX LID DETAIL  
SCALE: N.T.S.

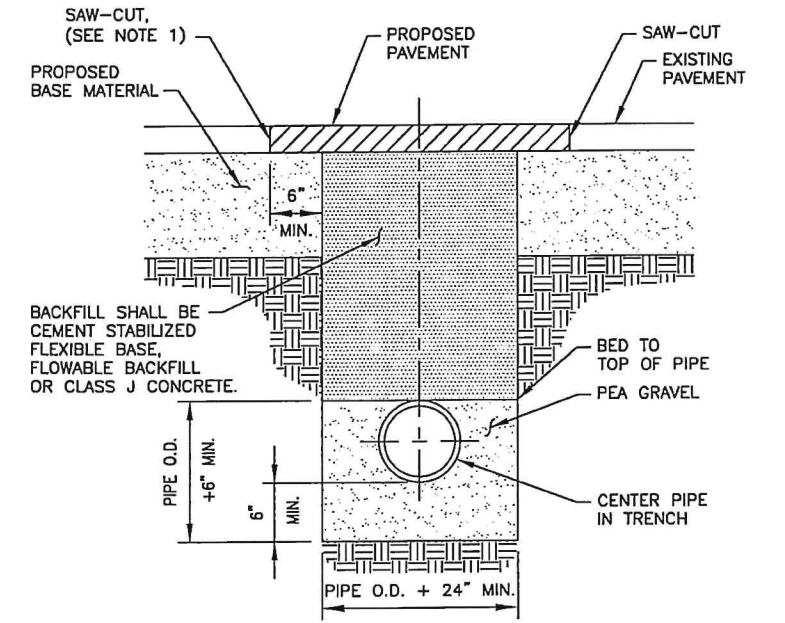
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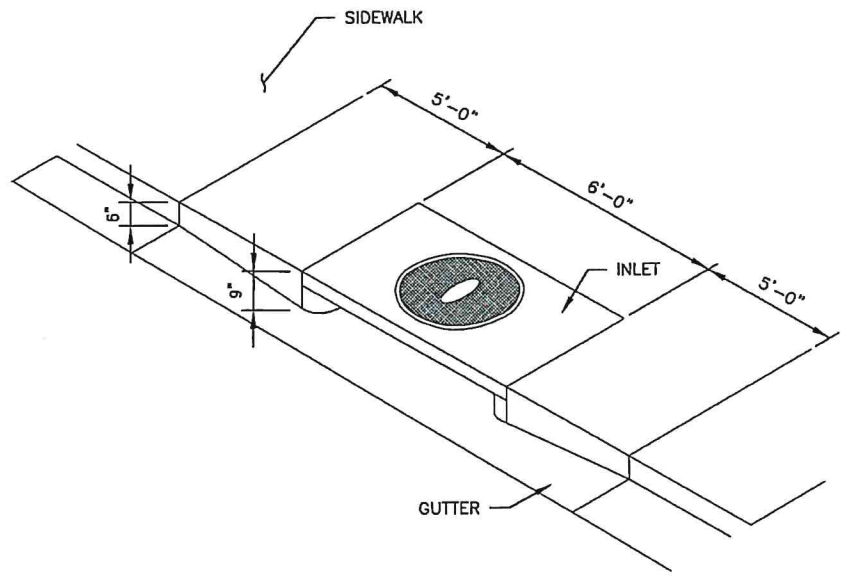
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RUTILIO P. MORA JR, P.E. NO. 111588



- NOTES:**
1. THE CONTRACTOR SHALL SAW-CUT, REMOVE AND REPLACE EXISTING PAVEMENT A MINIMUM OF 6" BEYOND EITHER THE EDGE OF THE STORM SEWER TRENCH OR THE POINT WHERE EXISTING PAVEMENT IS DAMAGED DUE TO TRENCHING OPERATIONS, WHICHEVER IS GREATER.
  2. INSTALLATION OF BACKFILL, SAW-CUTTING AND REMOVAL OF EXISTING PAVEMENT AND SURFACE PATCH, SHALL NOT BE PAID FOR SEPARATELY. COSTS FOR THESE ITEMS SHALL BE INCLUDED IN UNIT PRICE BIDS FOR STORM SEWER PIPE.
  3. THE CONTRACTOR SHALL PROVIDE STEEL PLATES TO SPAN THE TRENCH AS NECESSARY OR TO ALLOW BACKFILL TO CURE. SUCH PLATES SHALL BE SUITABLE FOR VEHICLE PASSAGE OVER THE TRENCH AND SHALL BE SATISFACTORILY ANCHORED IN PLACE. COSTS FOR THIS ITEM SHALL BE INCLUDED IN UNIT PRICE BIDS FOR STORM SEWER PIPE.
  4. ALL TRENCHING AND TRENCH SAFETY SHALL COMPLY WITH APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS.



**3 UTILITY BEDDING DETAIL**  
SCALE: NOT TO SCALE



**4 CURB INLET & SIDEWALK DETAIL**  
SCALE: N.T.S.

**1 CURB INLET DETAILS**  
SCALE: NOT TO SCALE

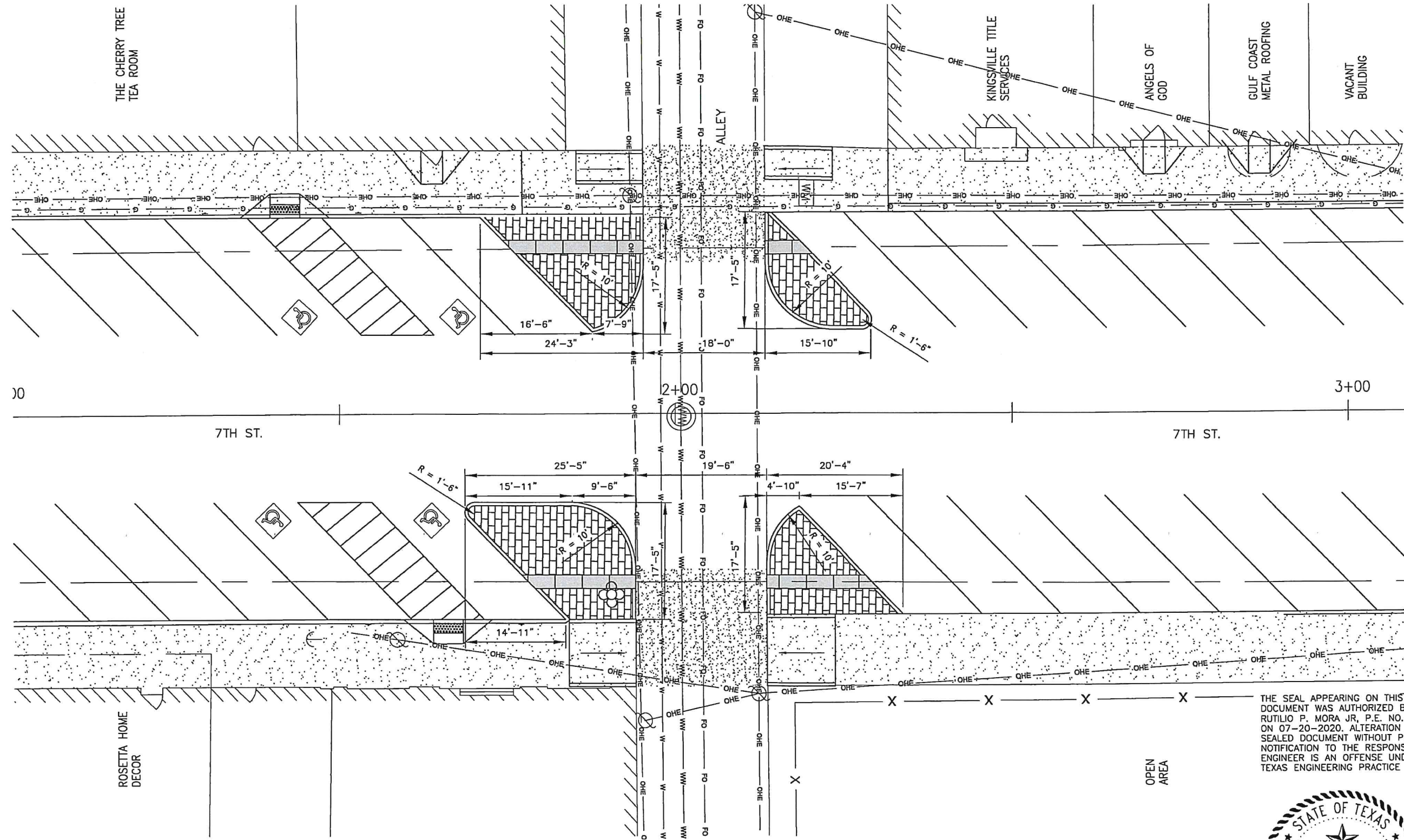
**2 DETAIL OF INLET STEPS**  
SCALE: NOT TO SCALE

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Date: 02/01/2020  
Checked by: R. MORA  
Job:

**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**STORMWATER DRAINAGE & DETAILS II**



**A** PROPOSED 7TH ST. ALLEY (BULB OUTS)  
 11 SCALE: N.T.S.



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 RUTILIO P. MORA JR., P.E. NO. 111588

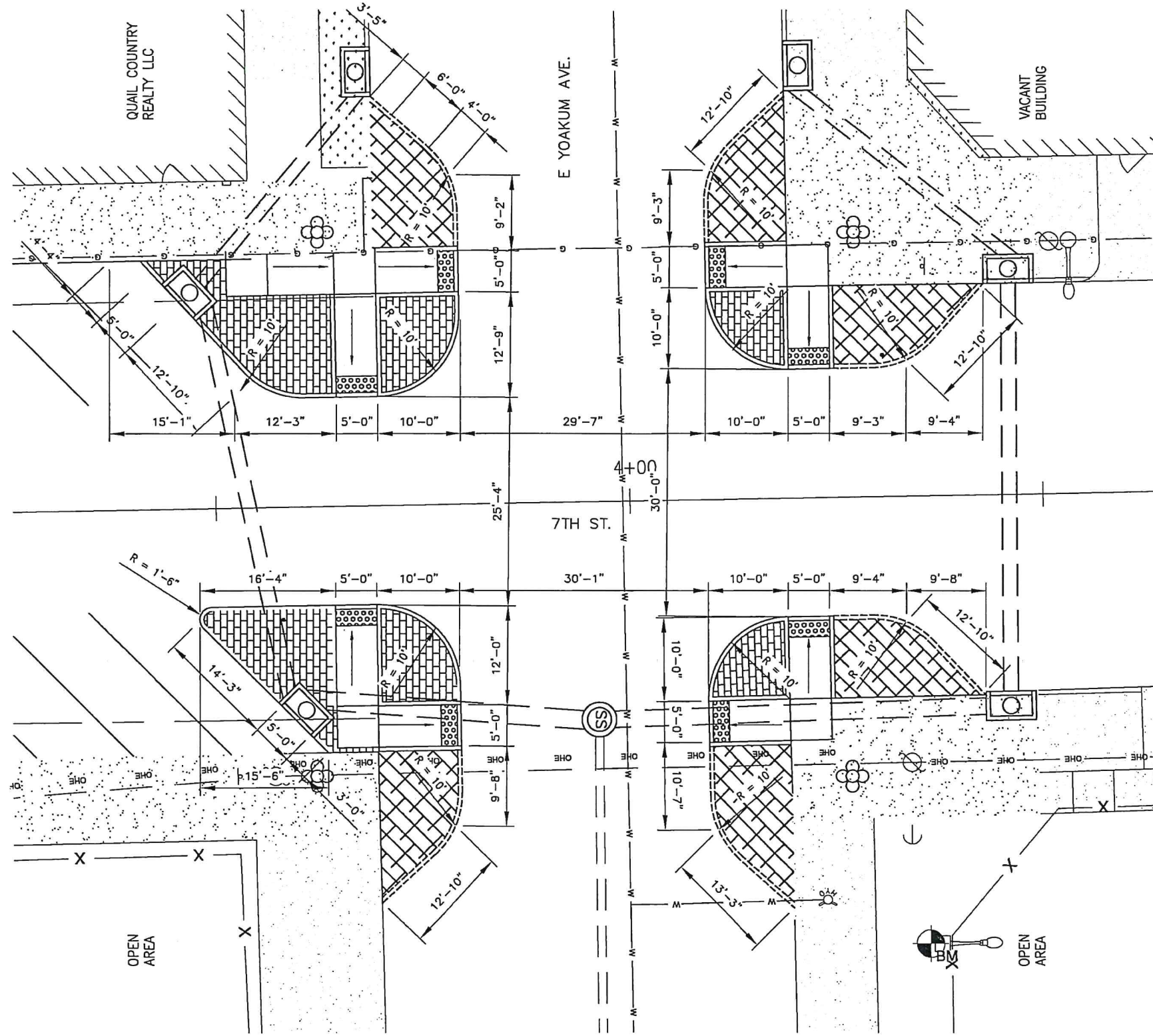
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**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**PROPOSED 7TH ST. ALLEY (BULB OUTS)**

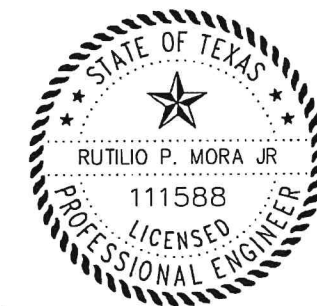


A  
12

PROPOSED 7TH ST. & YOAKUM AVE. (BULB OUTS)

SCALE: N.T.S.

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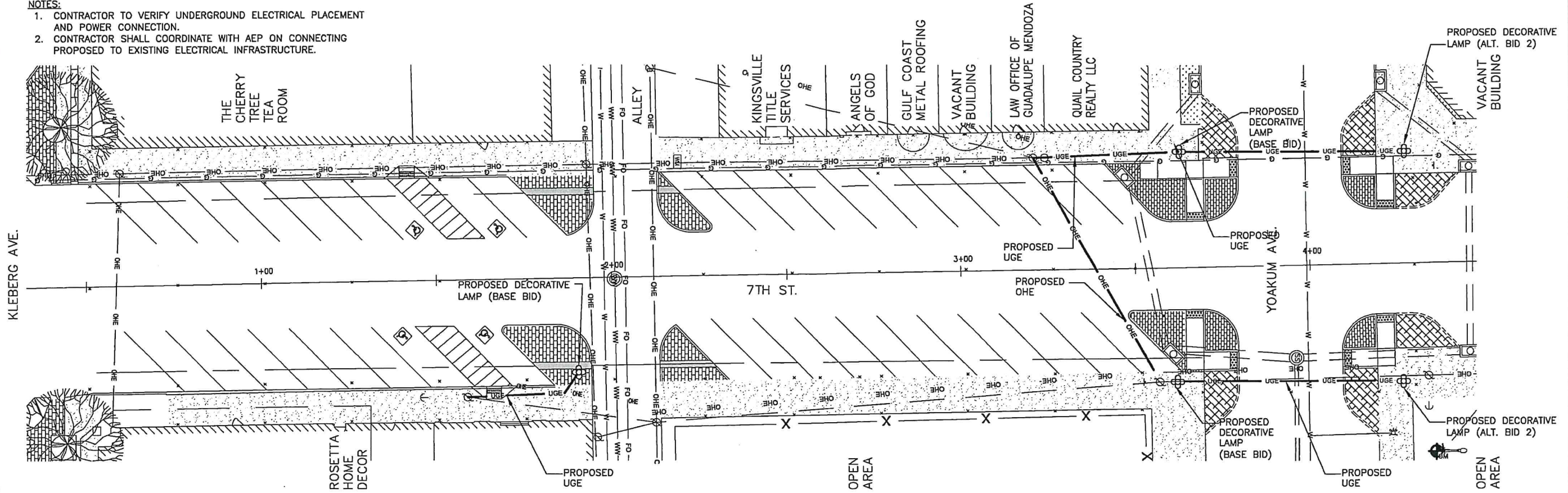


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Job:

2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
7TH ST. BETWEEN KLEBERG AVE. & YOAKUM AVE.  
PROPOSED 7TH ST. & YOAKUM AVE. (BULB OUTS)

**NOTES:**

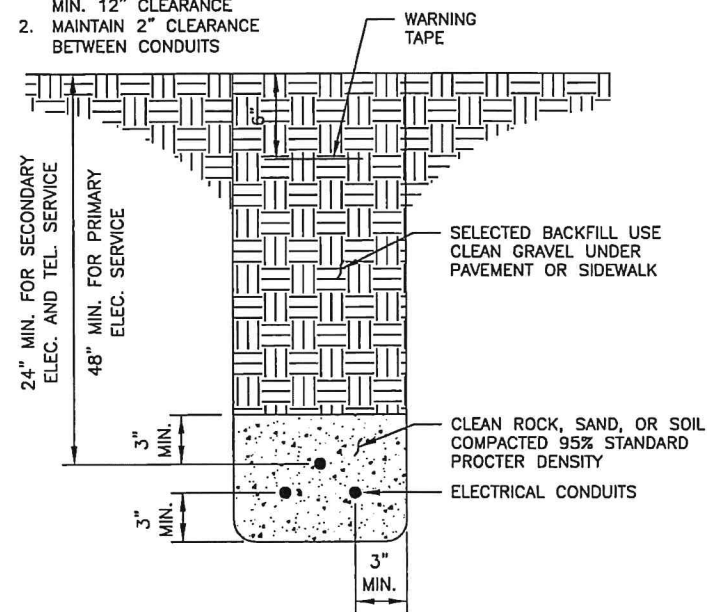
1. CONTRACTOR TO VERIFY UNDERGROUND ELECTRICAL PLACEMENT AND POWER CONNECTION.
2. CONTRACTOR SHALL COORDINATE WITH AEP ON CONNECTING PROPOSED TO EXISTING ELECTRICAL INFRASTRUCTURE.



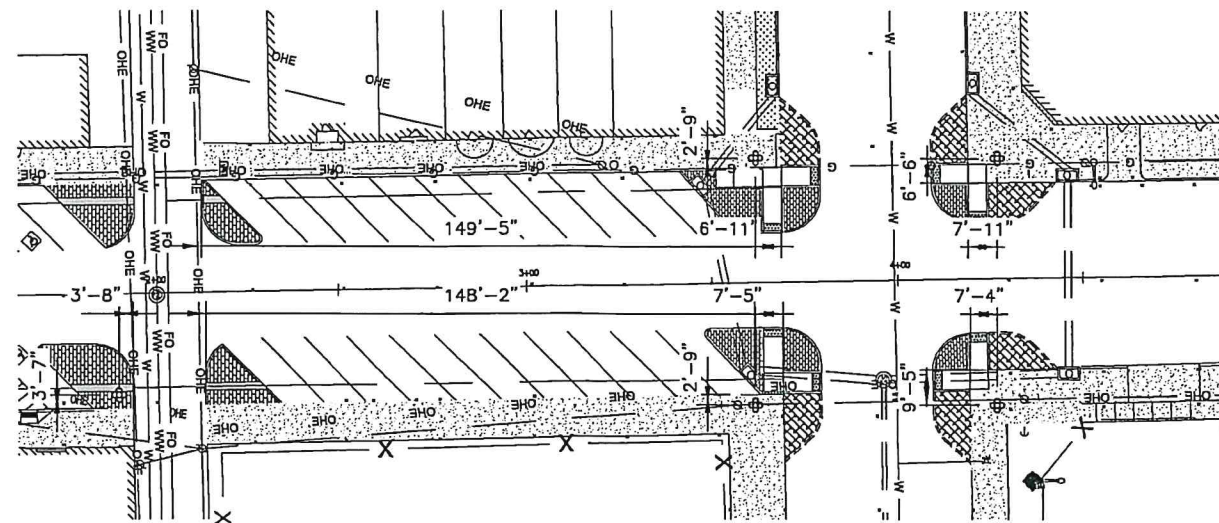
**1 ELECTRICAL SITE PLAN**  
SCALE: 1:30

**NOTE:**

1. WHERE CONDUITS ARE NEAR OTHER UTILITIES, MAINTAIN MIN. 12" CLEARANCE
2. MAINTAIN 2" CLEARANCE BETWEEN CONDUITS



**2 UNDERGROUND CONDUIT (TYP.)**  
SCALE: N.T.S.



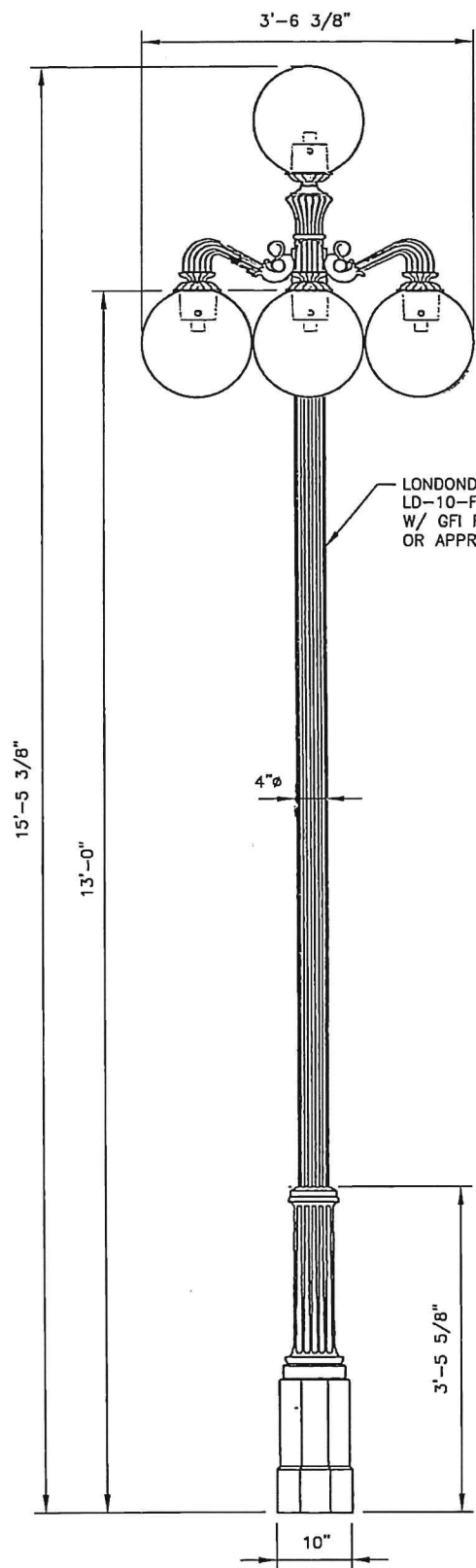
**3 ELECTRICAL SITE DIMENSION**  
SCALE: 1:50

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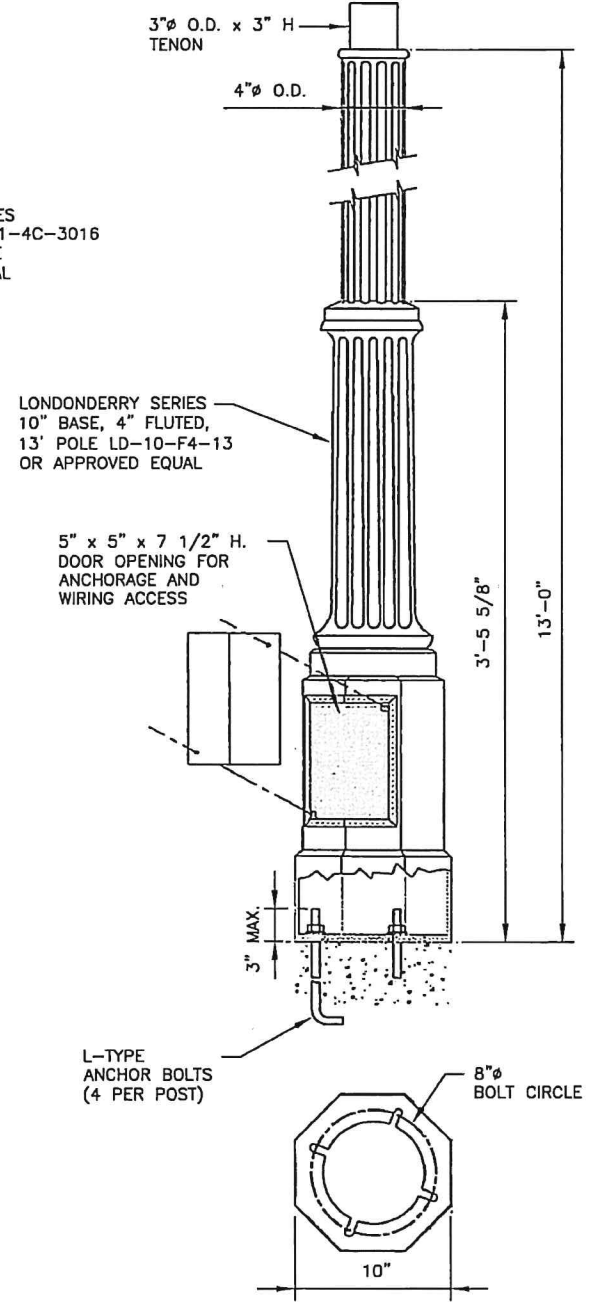
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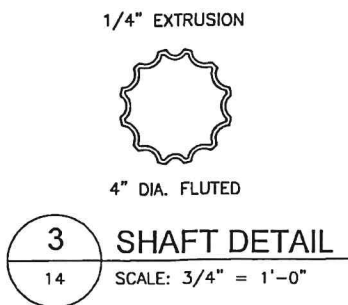


**1** DECORATIVE LAMP DETAIL  
14 SCALE: 1/2" = 1'-0"

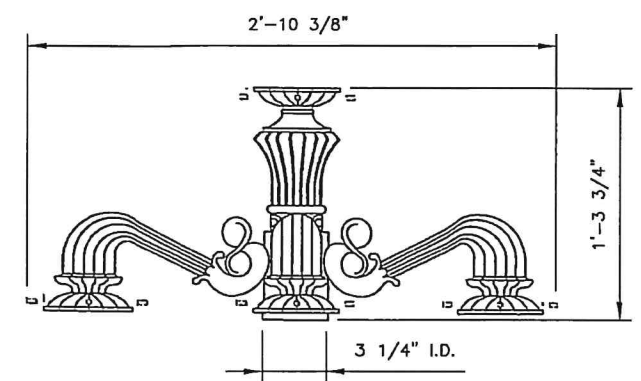
- ELECTRICAL NOTES:**
1. ALL WIRING SHALL BE CONTAINED IN CONDUIT OF PROPER SIZE.
  2. ALL WIRING SHALL CONFORM TO LOCAL, STATE AND FEDERAL CODES.
  3. ELECTRICAL PANEL TO BE LABELED CORRECTLY WITH LEGIBLE PRINT.
  4. ALL CONDUCTORS TO BE COPPER #12 AWG MINIMUM SIZE, OR AS REQUIRED BY LOAD AND OVER CURRENT PROTECTION.



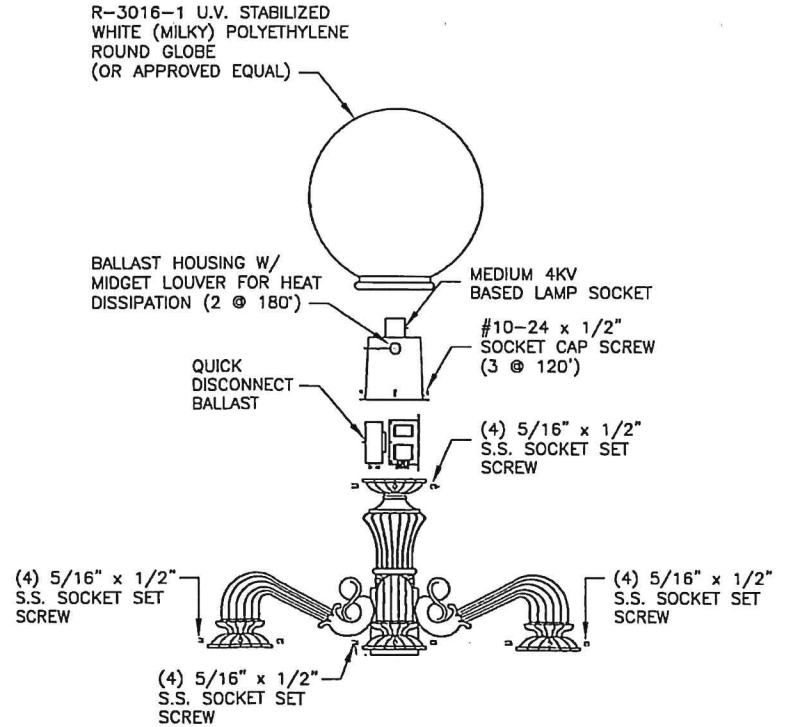
**2** DECORATIVE LAMP BASE DETAIL  
14 SCALE: 1" = 1'-0"



**3** SHAFT DETAIL  
14 SCALE: 3/4" = 1'-0"

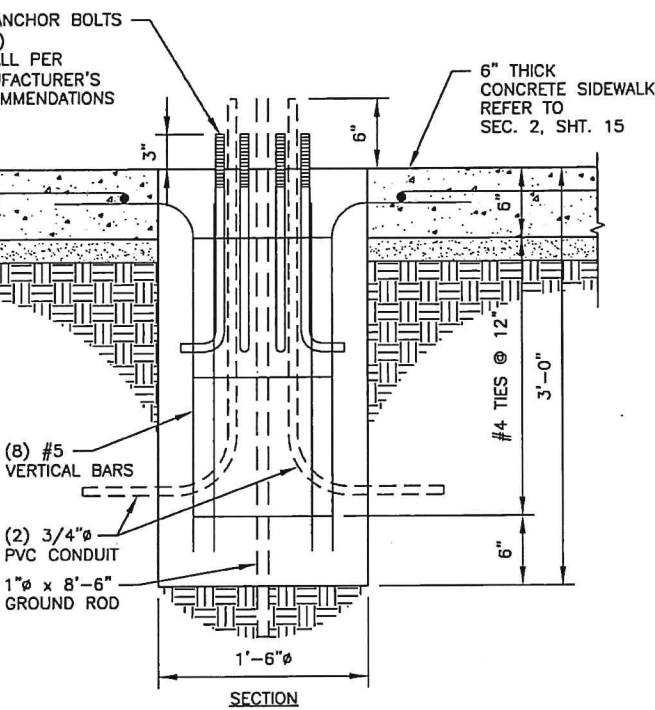
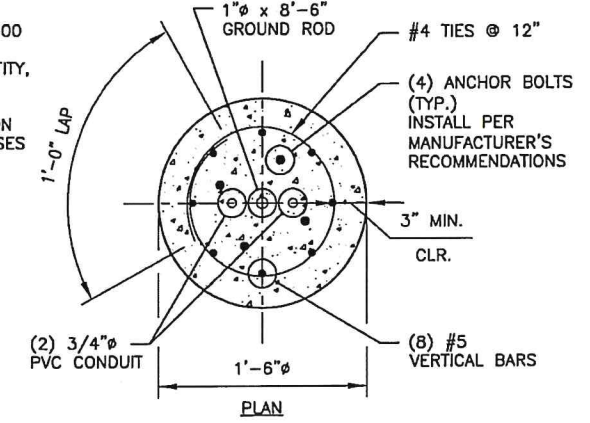


**4** DECORATIVE LAMP BASE DETAIL  
14 SCALE: 1" = 1'-0"



**5** DECORATIVE LAMP BASE DETAIL  
14 SCALE: 1/2" = 1'-0"

- NOTES:**
1. CONCRETE TO BE A MIN. 3000 PSI STRENGTH - 28 DAY.
  2. VERIFY ANCHOR BOLT QUANTITY, SIZE AND PROJECTION WITH POLE MFR.
  3. CONFIRM POLE LOCATIONS ON SITE PRIOR TO POURING BASES & RUNNING CONDUITS.



**6** DECORATIVE LAMP BASE DETAIL  
14 SCALE: 1/2" = 1'-0"

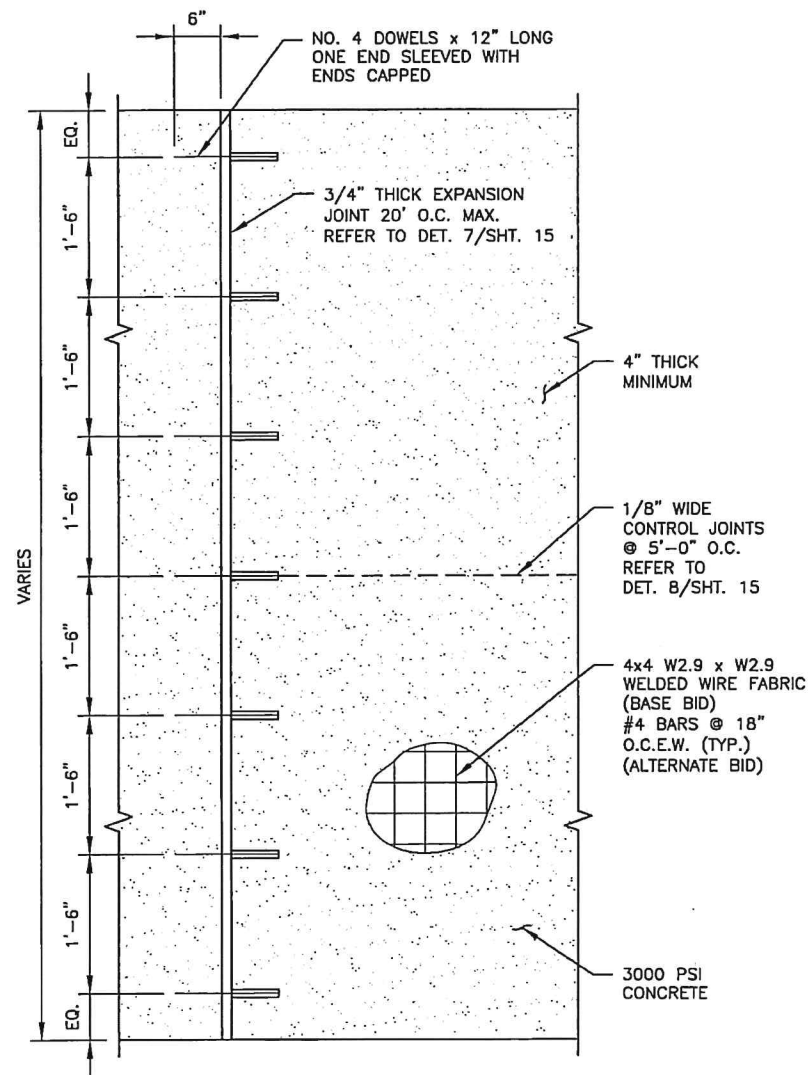
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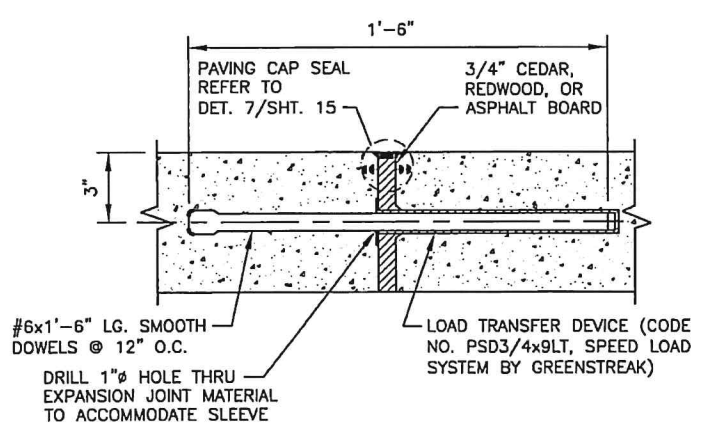
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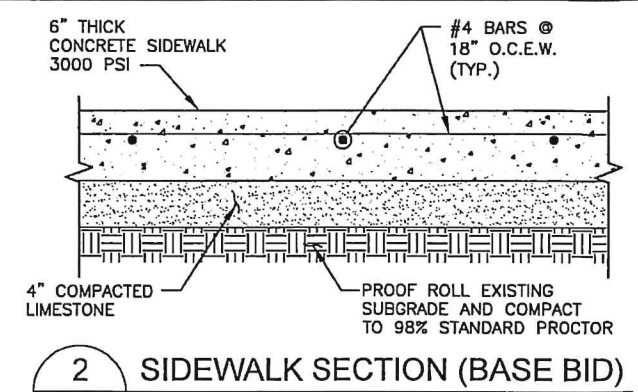
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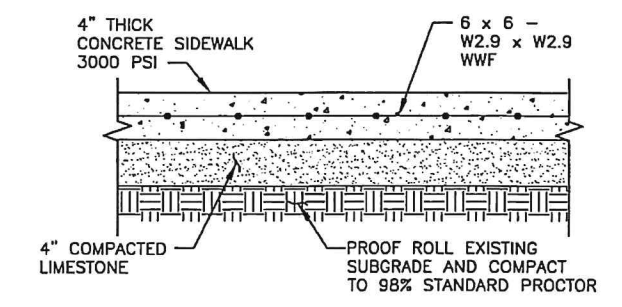
**1** SIDEWALK DETAIL  
15 SCALE: 1/2" = 1'-0"



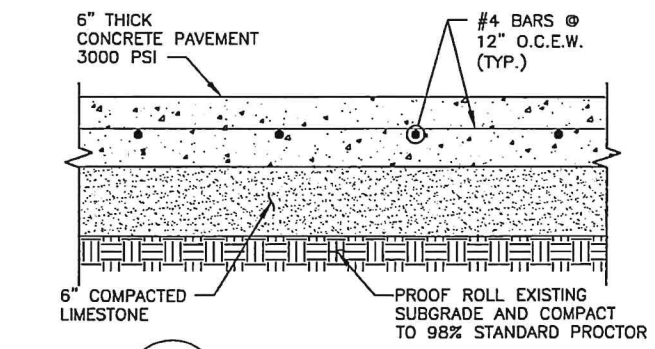
**6** TYP. CONCRETE PAVEMENT EXPANSION JOINT DETAIL  
15 SCALE: 3/4" = 1'-0"



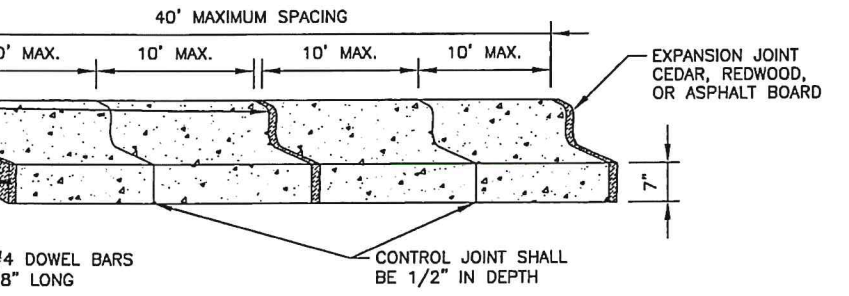
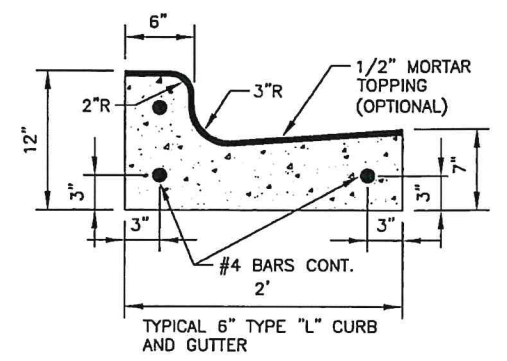
**2** SIDEWALK SECTION (BASE BID)  
15 SCALE: 3/4" = 1'-0"



**2A** SIDEWALK SECTION (ALTERNATE BID #3)  
15 SCALE: 3/4" = 1'-0"

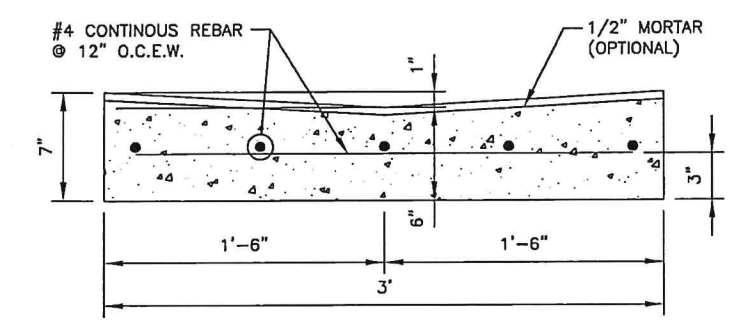


**3** PAVEMENT SECTION  
15 SCALE: 3/4" = 1'-0"

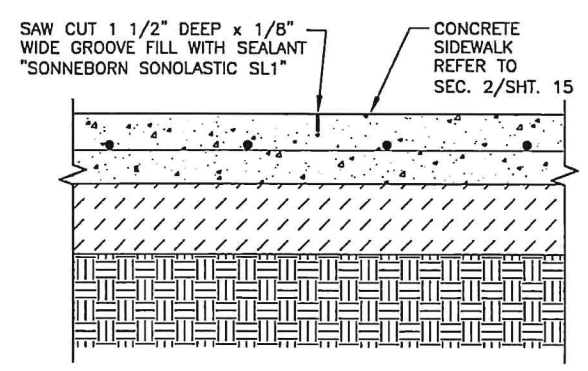


NOTE:  
1. BACKFILL BACK OF CURB.  
2. REPAIR ALL HONEYCOMBING AND CRACKS IN MORTAR OR CONCRETE PRIOR TO BACKFILL AND PAVING.

**4** CURB & GUTTER DETAILS  
15 SCALE: 3/4" = 1'-0"

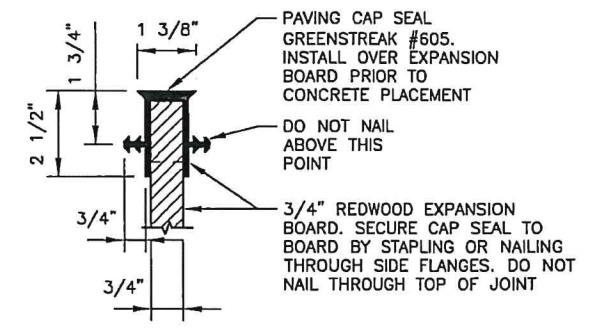


**5** VALLEY GUTTER DETAIL  
15 SCALE: NOT TO SCALE



SAW CUT NOTE:  
USE CONVENTIONAL SAW TO CUT JOINTS WITHIN 4 TO 12 HOURS AFTER FINISHING AND AS SOON AS THE CONCRETE HAS HARDENED SUFFICIENTLY TO PREVENT AGGREGATES FROM BEING DISLODGED BY THE SAW.

**8** TYP. CONCRETE PAVEMENT CONTROL JOINT DETAIL  
15 SCALE: 3/4" = 1'-0"



**7** CAP SEAL DETAIL  
15 SCALE: NOT TO SCALE



Drawn by: V. MARQUEZ  
Date: 02/01/2020  
Checked by: R. MORA  
Job:

**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**MISCELLANEOUS DETAILS I**

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**CURB RAMPS**

1. INSTALL A CURB RAMP OR BLENDED TRANSITION AT EACH PEDESTRIAN STREET CROSSING.
2. ALL SLOPES SHOWN ARE MAXIMUM ALLOWABLE. CROSS SLOPES OF 1.5% AND LESSER RUNNING SHOULD BE USED. ADJUST CURB RAMP LENGTH OR GRADE OF APPROACH SIDEWALKS AS DIRECTED.
3. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK AND CURB RAMP SURFACES IS 2%.
4. THE MINIMUM SIDEWALK WIDTH IS 5'. WHERE THE SIDEWALK IS ADJACENT TO THE BACK OF CURB, A 6' SIDEWALK WIDTH IS DESIRABLE. WHERE A 5' SIDEWALK CANNOT BE PROVIDED DUE TO SITE CONSTRAINTS, SIDEWALK WIDTH MAY BE REDUCED TO 4' FOR SHORT DISTANCES. 5' X 5' PASSING AREAS AT INTERVALS NOT TO EXCEED 200' ARE REQUIRED.
5. TURNING SPACES SHALL BE 5' X 5' MINIMUM. CROSS SLOPE SHALL BE MAXIMUM 2%.
6. CLEAR SPACE AT THE BOTTOM OF CURB RAMPS SHALL BE A MINIMUM OF 4' X 4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICULAR TRAVEL PATH.
7. PROVIDE FLARED SIDES WHERE THE PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. FLARED SIDES SHALL BE SLOPED AT 10% MAXIMUM, MEASURED PARALLEL TO THE CURB. RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP, EITHER BECAUSE THE ADJACENT SURFACE IS PLANTED, SUBSTANTIALLY OBSTRUCTED, OR OTHERWISE PROTECTED.
8. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE LATEST DRAFT OF THE PROPOSED GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT OF WAY (PROWAG) AS PUBLISHED BY THE U.S. ARCHITECTURAL AND TRANSPORTATION BARRIERS COMPLIANCE BOARD (ACCESS BOARD).
9. TO SERVE AS A PEDESTRIAN REFUGE AREA, THE MEDIAN SHOULD BE A MINIMUM OF 6' WIDE, MEASURED FROM BACK OF CURBS. MEDIANS SHOULD BE DESIGNED TO PROVIDE ACCESSIBLE PASSAGE OVER OR THROUGH THEM.
10. SMALL CHANNELIZATION ISLANDS, WHICH DO NOT PROVIDE A MINIMUM 5' X 5' LANDING AT THE TOP OF CURB RAMPS, SHALL BE CUT THROUGH LEVEL WITH THE SURFACE OF THE STREET.
11. CROSSWALK DIMENSIONS, CROSSWALK MARKINGS AND STOP BAR LOCATIONS SHALL BE AS SHOWN ELSEWHERE IN THE PLANS. AT INTERSECTIONS WHERE CROSSWALK MARKINGS ARE NOT REQUIRED, CURB RAMPS SHALL ALIGN WITH THEORETICAL CROSSWALKS UNLESS OTHERWISE DIRECTED.
12. PROVIDE CURB RAMPS TO CONNECT THE PEDESTRIAN ACCESS ROUTE AT EACH PEDESTRIAN STREET CROSSING. HANDRAILS ARE NOT REQUIRED ON CURB RAMPS.
13. CURB RAMPS AND LANDINGS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM 531 "SIDEWALKS".
14. PLACE CONCRETE AT A MINIMUM DEPTH OF 5" FOR RAMPS, FLARES AND LANDINGS, UNLESS OTHERWISE DIRECTED.
15. FURNISH AND INSTALL NO. 3 REINFORCING STEEL BARS AT 18" O.C. BOTH WAYS, UNLESS OTHERWISE DIRECTED.
16. PROVIDE A SMOOTH TRANSITION WHERE THE CURB RAMPS CONNECT TO THE STREET.
17. EXISTING FEATURES THAT COMPLY WITH APPLICABLE STANDARDS MAY REMAIN IN PLACE UNLESS OTHERWISE SHOWN ON THE PLANS.

**DETECTABLE WARNING MATERIAL**

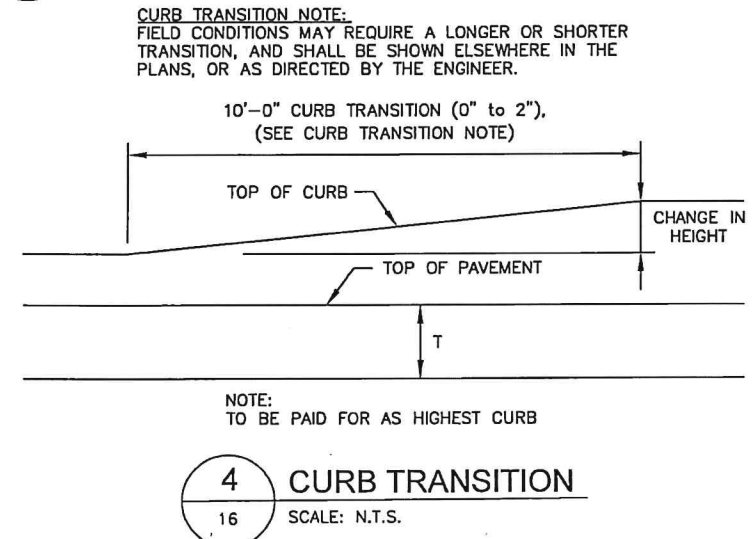
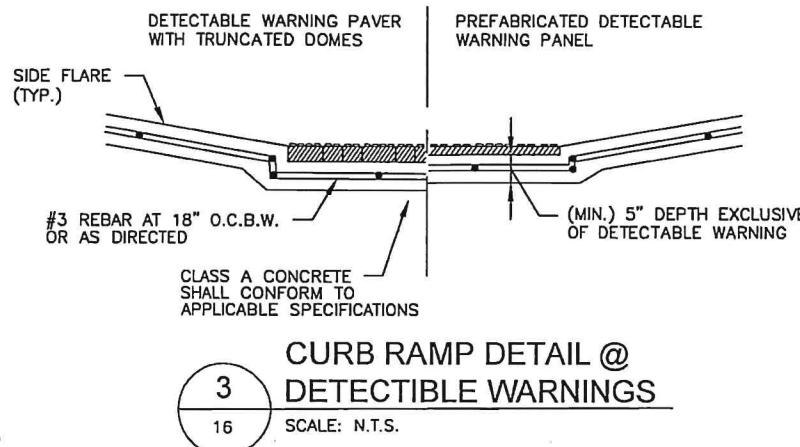
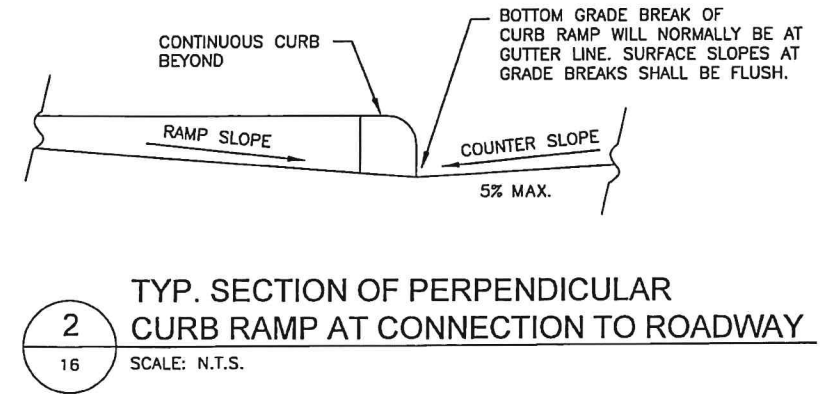
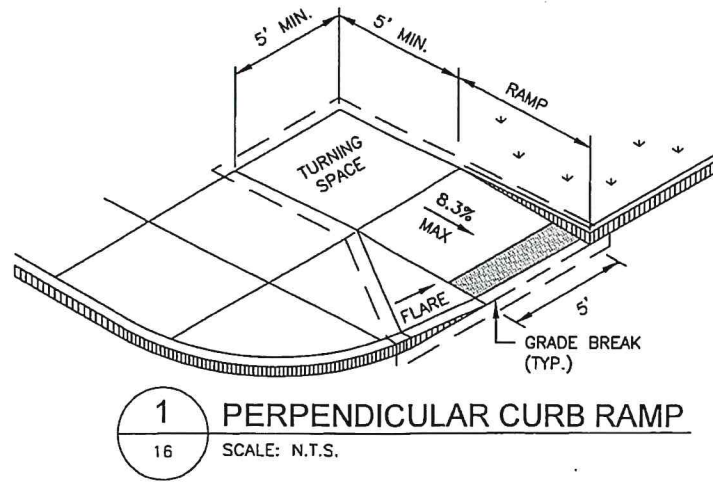
1. CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES COMPLYING WITH PROWAG. THE SURFACE MUST CONTRAST VISUALLY WITH ADJOINING SURFACES, INCLUDING SIDE FLARES. FURNISH AND INSTALL AN APPROVED CAST-IN-PLACE DARK BROWN OR DARK RED DETECTABLE WARNING SURFACE MATERIAL ADJACENT TO UNCOLORED CONCRETE, UNLESS SPECIFIED ELSEWHERE IN THE PLANS.
2. DETECTABLE WARNING MATERIALS MUST MEET TXDOT DEPARTMENTAL MATERIALS SPECIFICATION DMS 4350 AND BE LISTED ON THE MATERIAL PRODUCER LIST. INSTALL PRODUCTS IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
3. DETECTABLE WARNING SURFACES MUST BE FIRM, STABLE AND SLIP RESISTANT.
4. DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 24 INCHES IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL, AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR LANDING WHERE THE PEDESTRIAN ACCESS ROUTE ENTERS THE STREET.
5. DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS AT THE BACK OF CURB AND NEITHER END OF THAT EDGE IS GREATER THAN 5 FEET FROM THE BACK OF CURB. DETECTABLE WARNING SURFACES MAY BE CURVED ALONG THE CORNER RADIUS.
6. SHADED AREAS ON SHEET 1 OF 4 INDICATE THE APPROXIMATE LOCATION FOR THE DETECTABLE WARNING SURFACE FOR EACH CURB RAMP TYPE.

**DETECTABLE WARNING PAVERS (IF USED)**

1. FURNISH DETECTABLE WARNING PAVER UNITS MEETING ALL REQUIREMENTS OF ASTM C-936, C-33. LAY IN A TWO BY TWO UNIT BASKET WEAVE PATTERN OR AS DIRECTED.
2. LAY FULL-SIZE UNITS FIRST FOLLOWED BY CLOSURE UNITS CONSISTING OF AT LEAST 25 PERCENT (25%) OF A FULL UNIT. CUT DETECTABLE WARNING PAVER UNITS USING A POWER SAW.

**SIDEWALKS**

1. PROVIDE CLEAR GROUND SPACE AT OPERABLE PARTS, INCLUDING PEDESTRIAN PUSH BUTTONS. OPERABLE PARTS SHALL BE PLACED WITHIN UNOBSTRUCTED REACH RANGE SPECIFIED IN PROWAG SECTION R406.
2. PLACE TRAFFIC SIGNAL OR ILLUMINATION POLES, GROUND BOXES, CONTROLLER BOXES, SIGNS, DRAINAGE FACILITIES AND OTHER ITEMS SO AS NOT TO OBSTRUCT THE PEDESTRIAN ACCESS ROUTE OR CLEAR GROUND SPACE.
3. STREET GRADES AND CROSS SLOPES SHALL BE AS SHOWN ELSEWHERE IN THE PLANS.
4. CHANGES IN LEVEL GREATER THAN 1/4 INCH ARE NOT PERMITTED.
5. THE LEAST POSSIBLE GRADE SHOULD BE USED TO MAXIMIZE ACCESSIBILITY. THE RUNNING SLOPE OF SIDEWALKS AND CROSSWALKS WITHIN THE PUBLIC RIGHT OF WAY MAY FOLLOW THE GRADE OF THE PARALLEL ROADWAY. WHERE A CONTINUOUS GRADE GREATER THAN FIVE PERCENT (5%) MUST BE PROVIDED, HANDRAILS MAY BE DESIRABLE TO IMPROVE ACCESSIBILITY. HANDRAILS MAY ALSO BE NEEDED TO PROTECT PEDESTRIANS FROM POTENTIALLY HAZARDOUS CONDITIONS. IF PROVIDED, HANDRAILS SHALL COMPLY WITH PROWAG R409.
6. HANDRAIL EXTENSIONS SHALL NOT PROTRUDE INTO THE USABLE LANDING AREA OR INTO INTERSECTING PEDESTRIAN ROUTES.
7. DRIVEWAYS AND TURNOUTS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM "INTERSECTIONS, DRIVEWAYS AND TURNOUTS". SIDEWALKS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM, "SIDEWALKS".
8. SIDEWALK DETAILS ARE SHOWN ELSEWHERE IN THE PLANS.



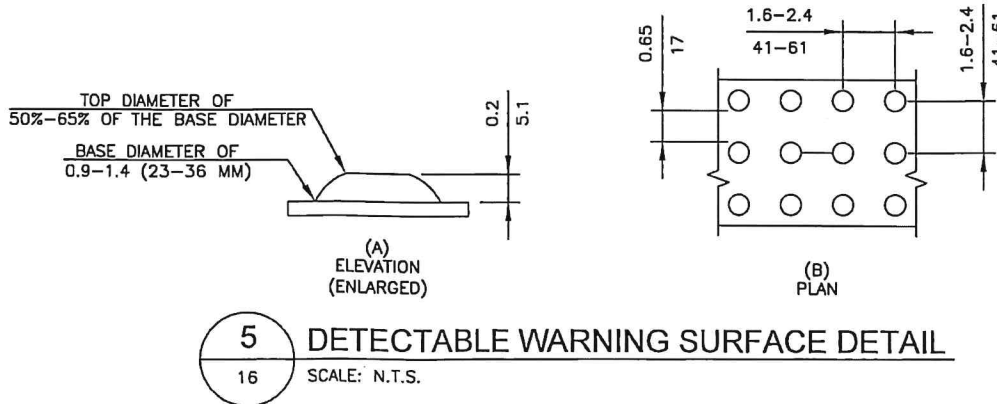
**705.1 GENERAL**  
DETECTABLE WARNINGS SHALL CONSIST OF A SURFACE OF TRUNCATED DOMES AND SHALL COMPLY WITH 705.

**705.1.1 DOME SIZE**  
TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL HAVE A BASE DIAMETER OF 0.9 INCH (23 MM) MINIMUM AND 1.4 INCHES (36 MM) MAXIMUM, A TOP DIAMETER OF 50 PERCENT OF THE BASE DIAMETER MINIMUM TO 65 PERCENT OF THE BASE DIAMETER MAXIMUM, AND A HEIGHT OF 0.2 INCH (5.1 MM).

**705.1.2 DOME SPACING**  
TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL HAVE A CENTER-TO-CENTER SPACING OF 1.6 INCHES (41 MM) MINIMUM AND 2.4 INCHES (61 MM) MAXIMUM, AND A BASE-TO-BASE SPACING OF 0.65 INCH (17 MM) MINIMUM, MEASURED BETWEEN THE MOST ADJACENT DOMES ON A SQUARE GRID.

**705.1.3 CONTRAST**  
DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT.

**705.2 PLATFORM EDGES**  
DETECTABLE WARNING SURFACES AT PLATFORM BOARDING EDGES SHALL BE 24 INCHES (610 MM) WIDE AND SHALL EXTEND THE FULL LENGTH OF THE PUBLIC USE AREAS OF THE PLATFORM.



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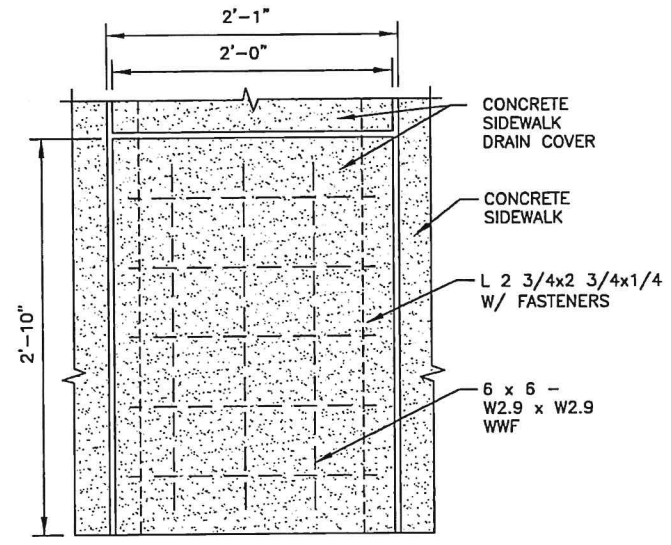
**2020 DOWNTOWN IMPROVEMENTS  
7TH STREET**

MISCELLANEOUS DETAILS II

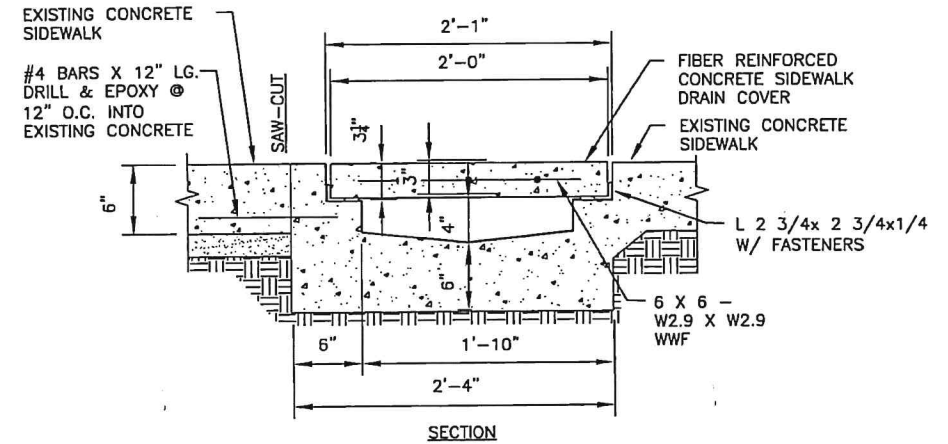
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Date: 02/01/2020  
Checked by: R. MORA  
Job:

SHEET 16



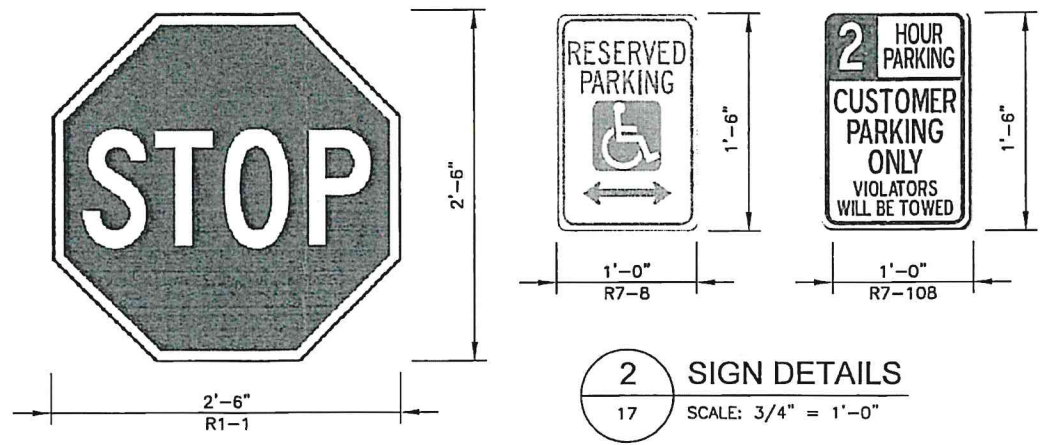


PLAN

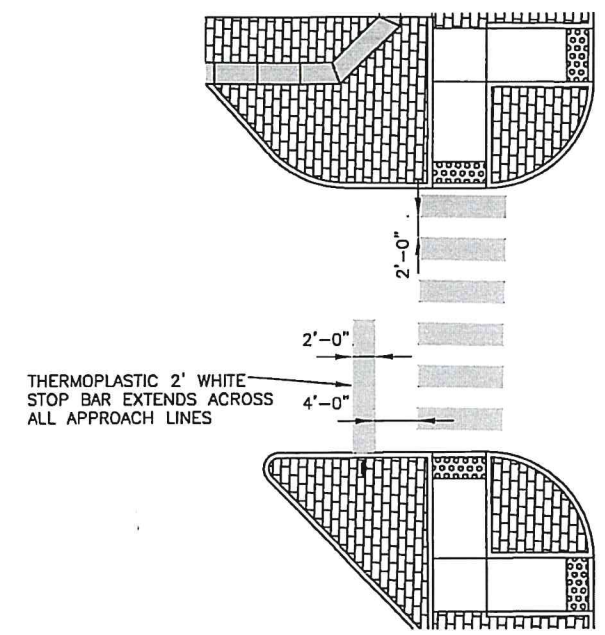


SECTION

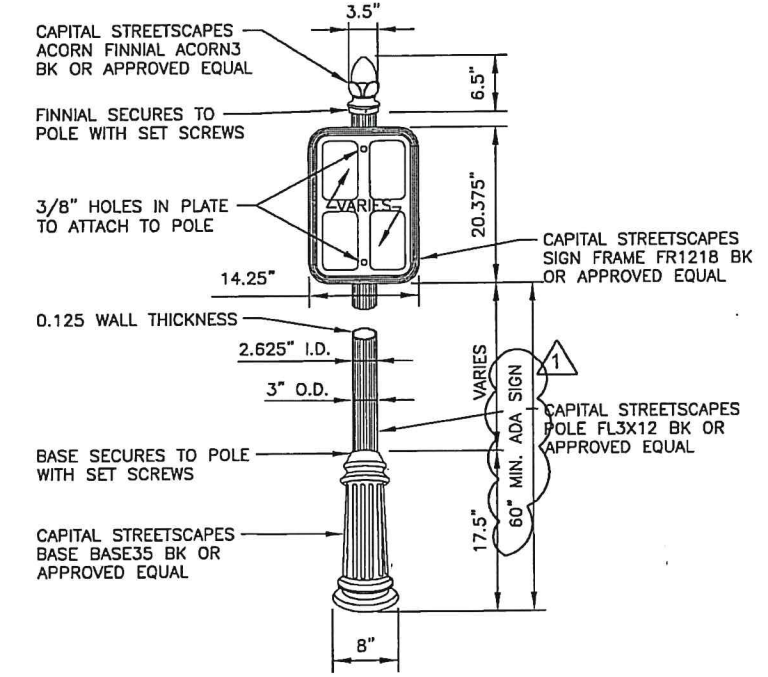
1 SIDEWALK DRAIN DETAIL  
17 SCALE: 3/4" = 1'-0"



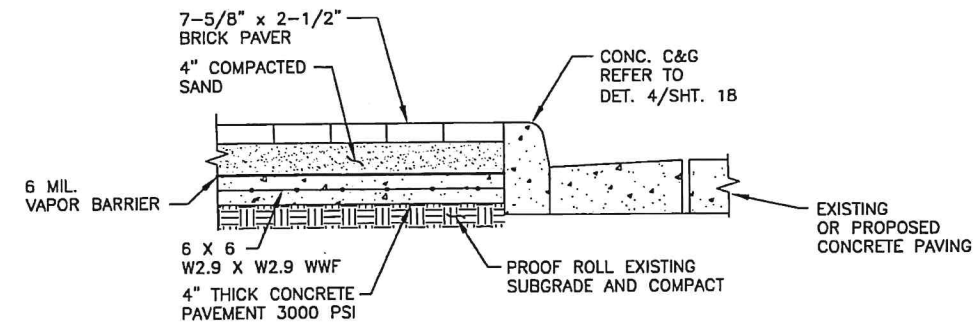
2 SIGN DETAILS  
17 SCALE: 3/4" = 1'-0"



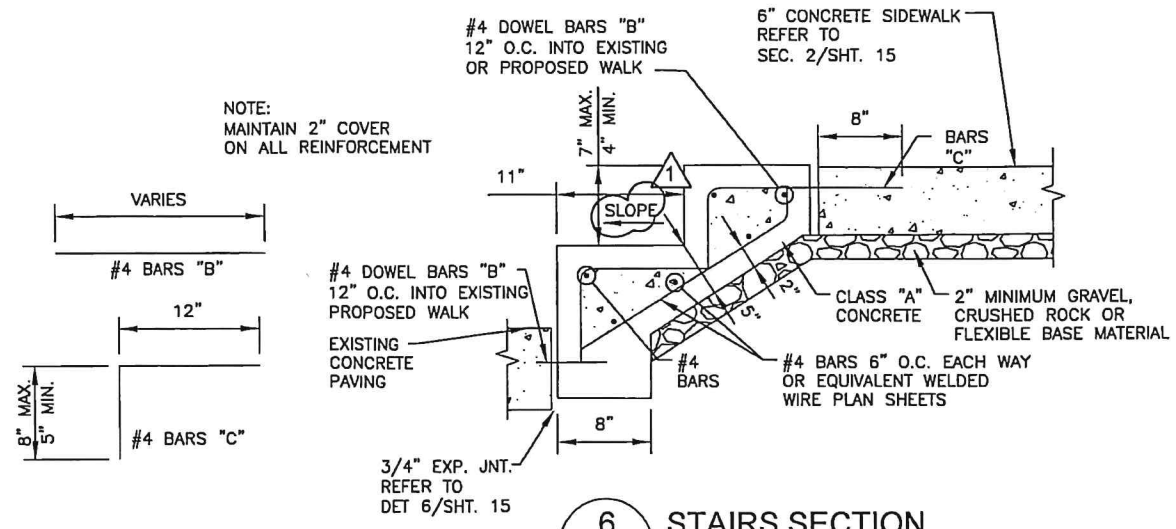
3 CROSSWALK DETAIL  
17 SCALE: N.T.S.



4 DECORATIVE SIGN POST DETAIL  
17 SCALE: 1/2" = 1'-0"



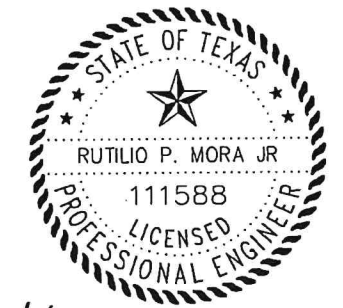
5 BRICK PAVER DETAIL  
17 SCALE: 1/2" = 1'-0"



6 STAIRS SECTION  
17 SCALE: 3/4" = 1'-0"

| REVISION: | DATE:      | DESCRIPTION:                               |
|-----------|------------|--------------------------------------------|
| 1         | 08/14/2020 | ADDITIONAL SLOPES, ELEVATIONS, AND DETAILS |

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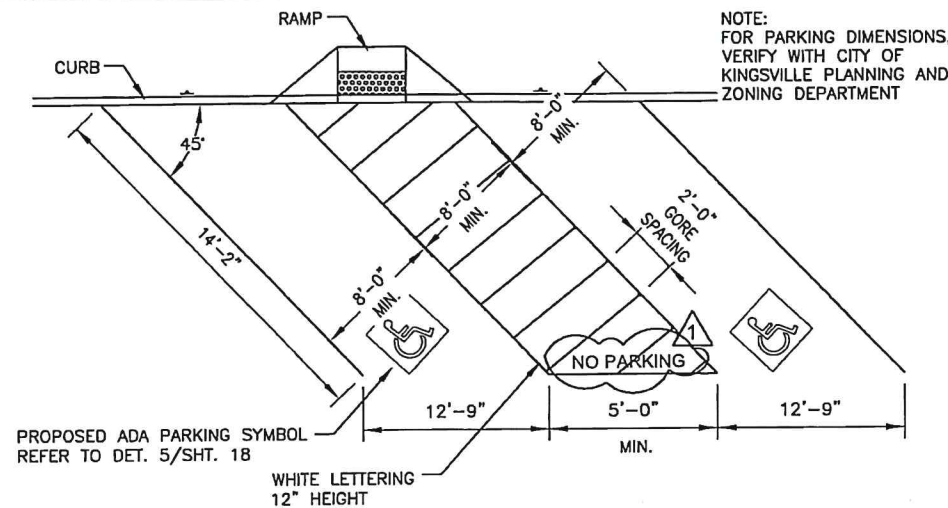
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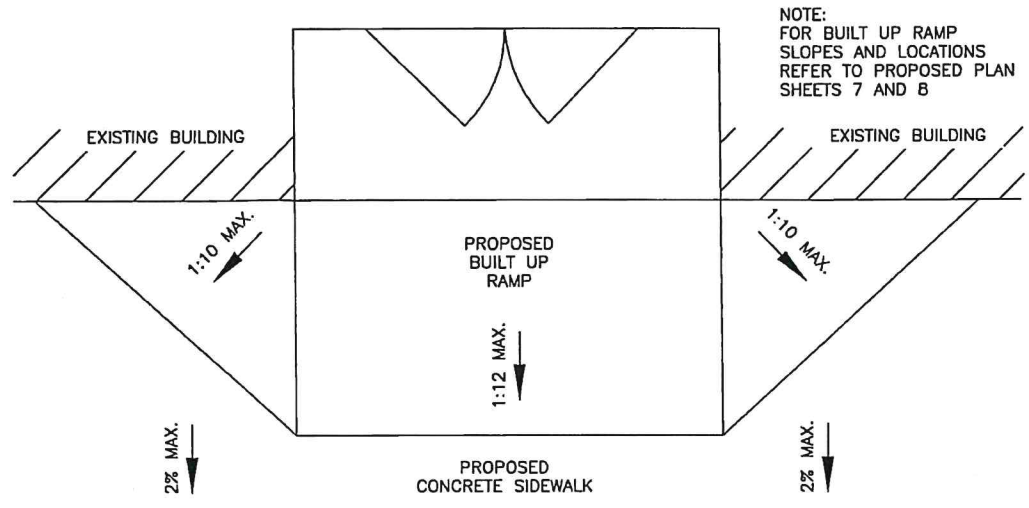


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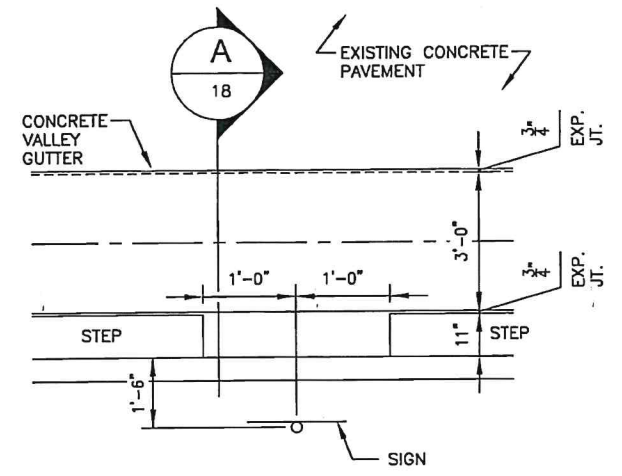
2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
MISCELLANEOUS DETAILS III



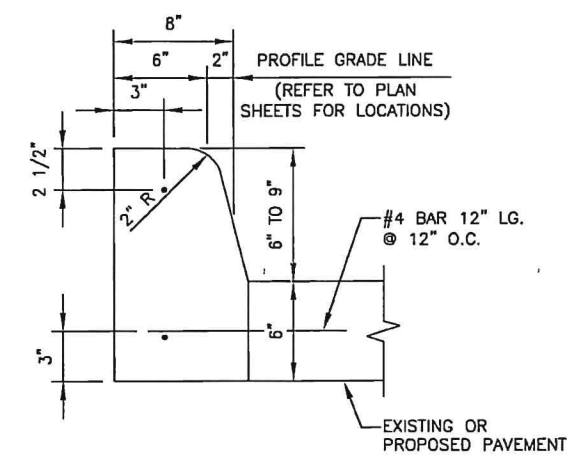
**1 ADA PARKING MARKINGS AND SYMBOLS**  
 18 SCALE: 1" = 1'-0"



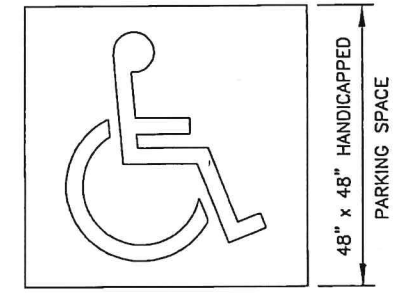
**2 BUILT UP RAMP DETAIL**  
 18 SCALE: 1/4" = 1'-0"



**3 SIGN PLACEMENT DETAIL**  
 18 SCALE: 1/4" = 1'-0"



**4 TYPE II MONOLITHIC CURB DETAIL**  
 18 SCALE: 1" = 1'-0"



**5 ADA PARKING SYMBOL DETAIL**  
 18 SCALE: N.T.S.

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|-----------|------------|--------------------------------------------|
| 1         | 08/14/2020 | ADDITIONAL SLOPES, ELEVATIONS, AND DETAILS |

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**2020 DOWNTOWN IMPROVEMENTS  
 7TH STREET  
 MISCELLANEOUS DETAILS IV**

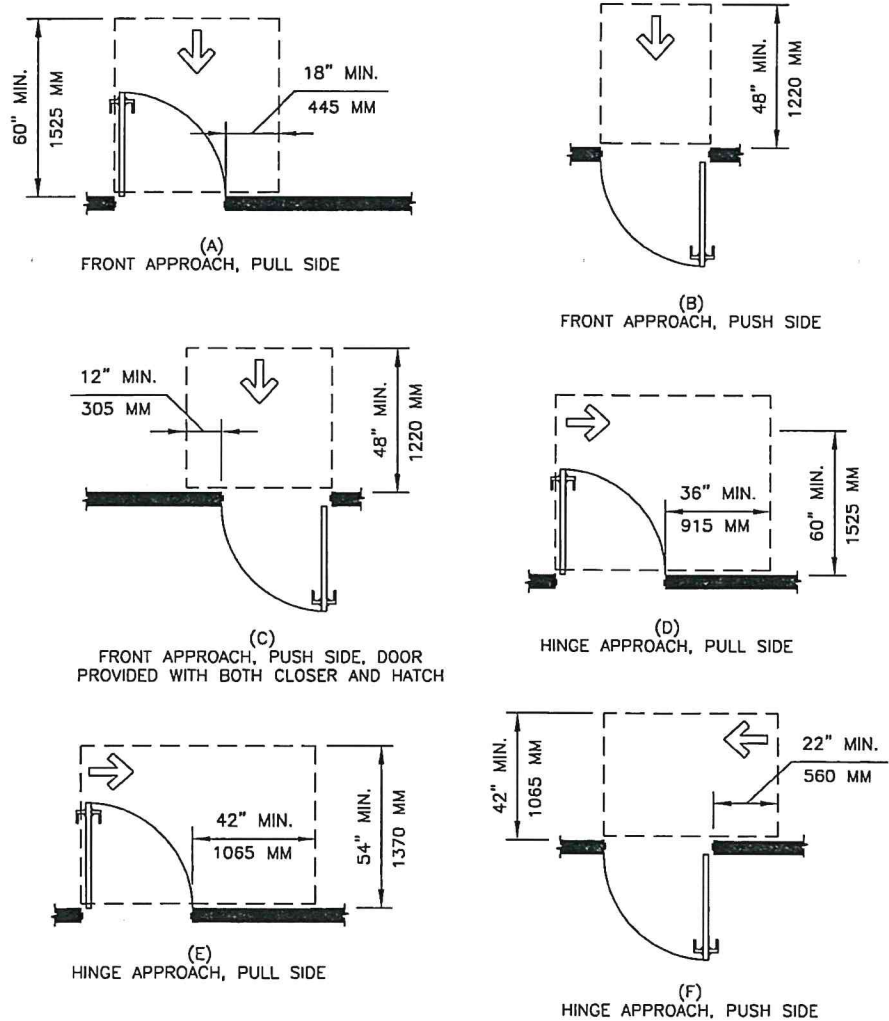
**404 DOORS, DOORWAYS AND GATES**

404.2.4 MANEUVERING CLEARANCES. MINIMUM MANEUVERING CLEARANCES AT DOORS AND GATES SHALL COMPLY WITH 404.2.4.1 MANEUVERING CLEARANCES SHALL EXTEND THE FULL WIDTH OF THE DOORWAY AND THE REQUIRED LATCH SIDE OR HINGE SIDE CLEARANCE.

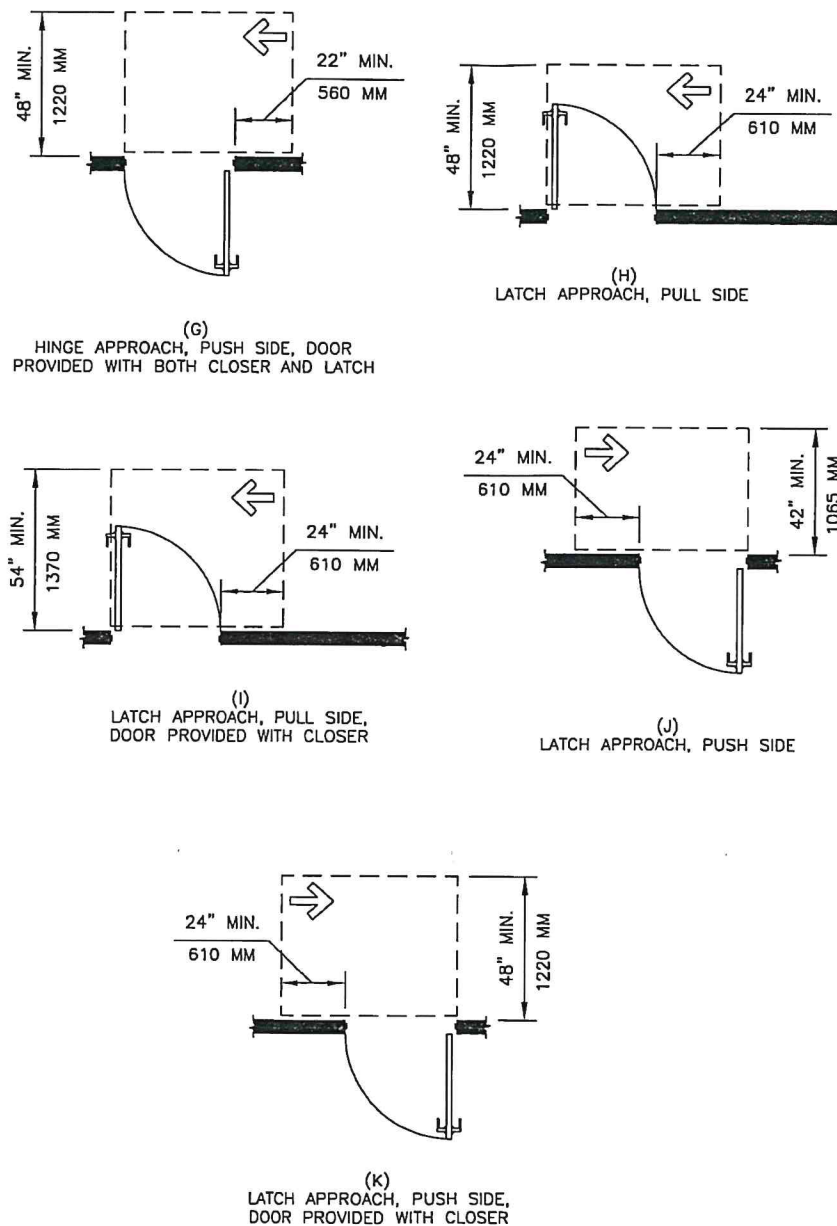
404.2.4.1 SWINGING DOORS AND GATES. SWINGING DOORS AND GATES SHALL HAVE MANEUVERING CLEARANCES COMPLYING WITH TABLE 404.2.4.1

**TABLE 404.2.4.1 MANEUVERING CLEARANCES AT MANUAL SWINGING DOORS AND GATES - TYPE OF USE AND MINIMUM MANEUVERING CLEARANCE**

| APPROACH DIRECTION | DOOR OR GATE SIDE | PERPENDICULAR TO DOORWAY                                                      | PARALLEL TO DOORWAY (BEYOND LATCH SIDE UNLESS NOTED)                       |
|--------------------|-------------------|-------------------------------------------------------------------------------|----------------------------------------------------------------------------|
| FROM FRONT         | PULL              | 60 INCHES (1525 MM)                                                           | 18 INCHES (455 MM)                                                         |
| FROM FRONT         | PUSH              | 48 INCHES (1220 MM)                                                           | 0 INCHES (0 MM) - ADD 12 INCHES (305 MM) IF CLOSER AND LATCH ARE PROVIDED. |
| FROM HINGE SIDE    | PULL              | 60 INCHES (1525 MM)                                                           | 36 INCHES (915 MM)                                                         |
| FROM HINGE SIDE    | PULL              | 54 INCHES (1370 MM)                                                           | 42 INCHES (1065 MM)                                                        |
| FROM HINGE SIDE    | PUSH              | 42 INCHES (1065 MM) - ADD 6 INCHES (150 MM) IF CLOSER AND LATCH ARE PROVIDED. | 22 INCHES (560 MM) - BEYOND HINGE SIDE                                     |
| FROM LATCH SIDE    | PULL              | 48 INCHES (1220 MM) - ADD 6 INCHES (150 MM) IF CLOSER IS PROVIDED.            | 24 INCHES (610 MM)                                                         |
| FROM LATCH SIDE    | PUSH              | 42 INCHES (1065 MM) - ADD 6 INCHES (150 MM) IF CLOSER IS PROVIDED.            | 24 INCHES (610 MM)                                                         |



**FIGURE 404.2.4.1 MANEUVERING CLEARANCES AT MANUAL SWINGING DOORS AND GATES**

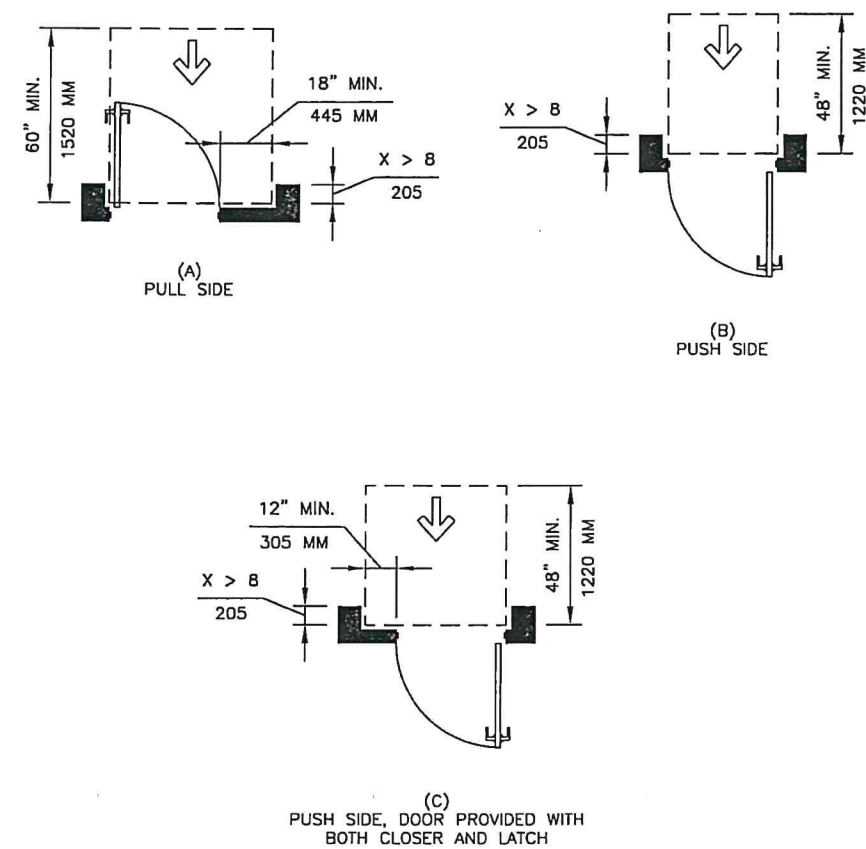


**FIGURE 404.2.4.1 MANEUVERING CLEARANCES AT MANUAL SWINGING DOORS AND GATES (CONTINUED)**

404.2.4.2 DOORWAYS WITHOUT DOORS OR GATES, SLIDING DOORS AND FOLDING DOORS. DOORWAYS LESS THAN 36 INCHES (915 MM) WIDE WITHOUT DOORS OR GATES, SLIDING DOORS, OR FOLDING DOORS SHALL HAVE MANEUVERING CLEARANCES COMPLYING WITH TABLE 404.2.4.2.

**FIGURE 404.2.4.3 RECESSED DOORS AND GATES**

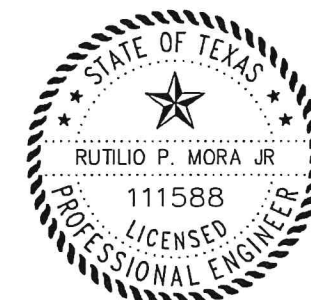
MANEUVERING CLEARANCES FOR FORWARD APPROACH SHALL BE PROVIDED WHEN ANY OBSTRUCTION WITHIN 18 INCHES (455 MM) OF THE LATCH SIDE OF A DOORWAY PROJECTS MORE THAN 8 INCHES (205 MM) BEYOND THE FACE OF THE DOOR, MEASURED PERPENDICULAR TO THE FACE OF THE DOOR OR GATE.



**FIGURE 404.2.4.3 MANEUVERING CLEARANCES AT RECESSED DOORS AND GATES**

404.2.4.4 FLOOR OR GROUND SURFACE. FLOOR OR GROUND SURFACE WITHIN REQUIRED MANEUVERING CLEARANCES SHALL COMPLY WITH 302. CHANGES IN LEVEL ARE NOT PERMITTED.

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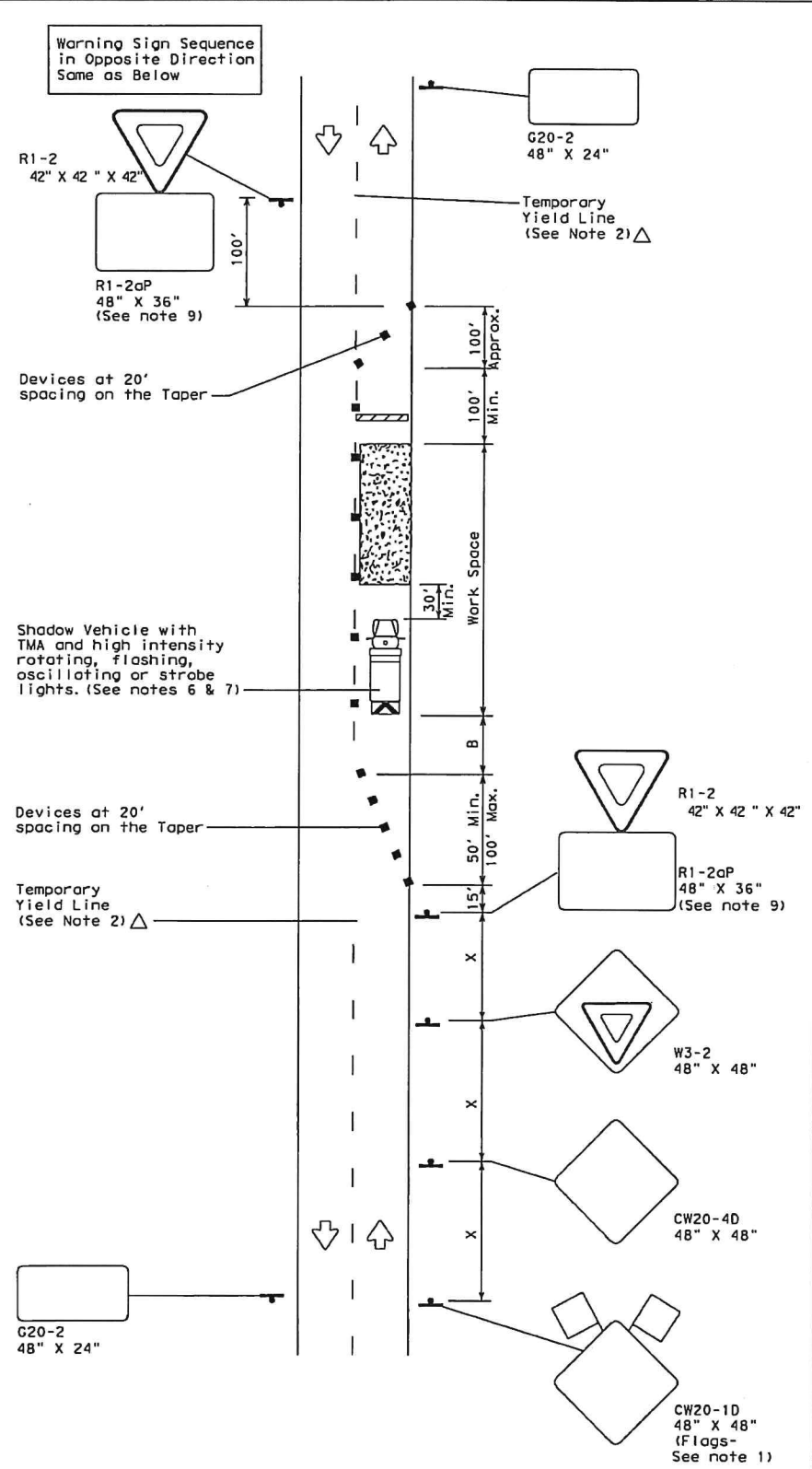


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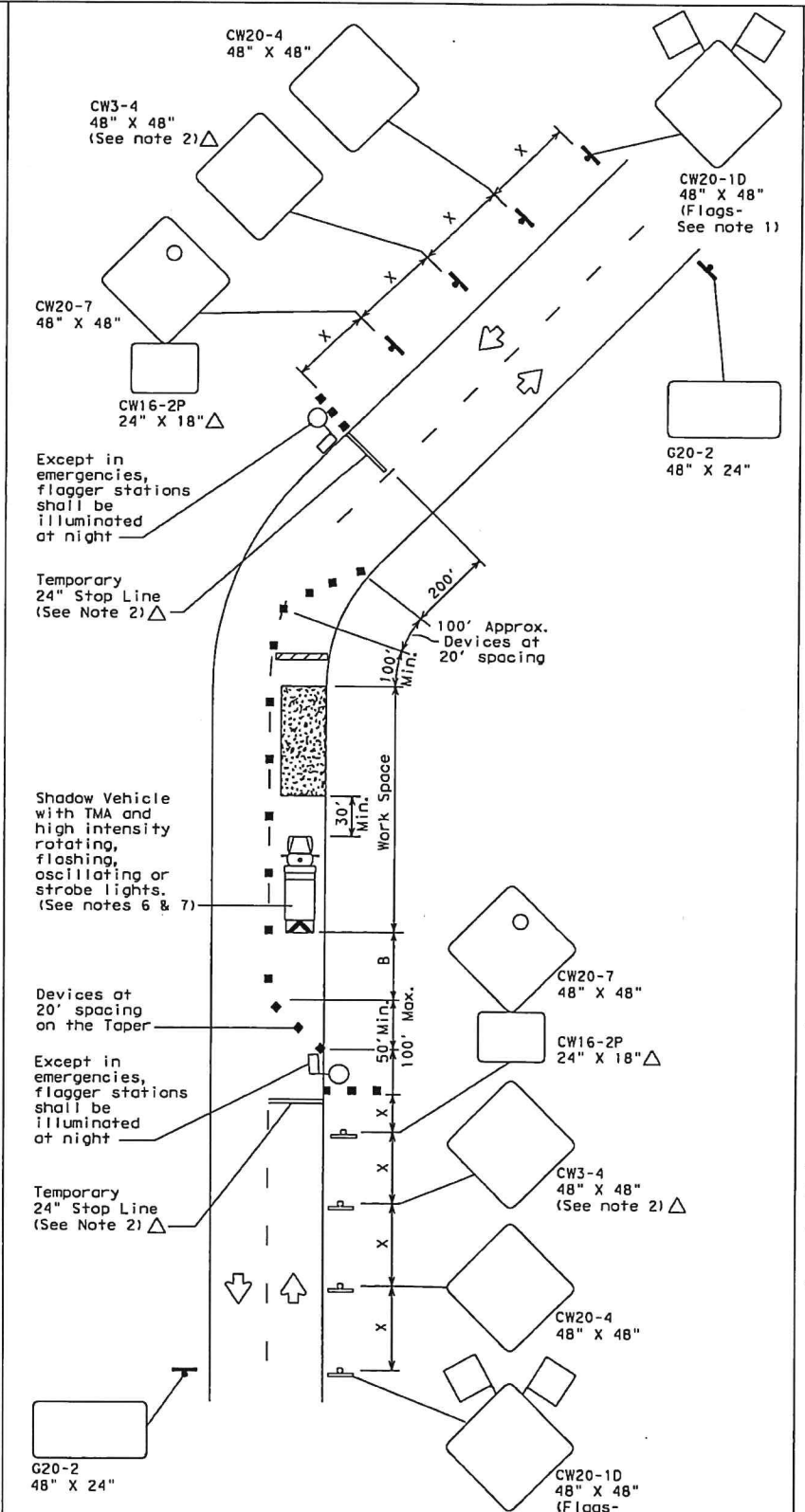
**2020 DOWNTOWN IMPROVEMENTS**  
**7TH STREET**  
**ACCESSIBLE ROUTES**  
**DOORS, DOORWAYS AND GATES**

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DATE: FILE:



TCP (2-2a)  
2-LANE ROADWAY WITHOUT PAVED SHOULDERS  
ONE LANE TWO-WAY  
CONTROL WITH YIELD SIGNS  
(Less than 2000 ADT - See Note 9)



TCP (2-2b)  
2-LANE ROADWAY WITHOUT PAVED SHOULDERS  
ONE LANE TWO-WAY  
CONTROL WITH FLAGGERS

LEGEND

|  |                                      |  |                                         |
|--|--------------------------------------|--|-----------------------------------------|
|  | Type 3 Barricade                     |  | Channelizing Devices                    |
|  | Heavy Work Vehicle                   |  | Truck Mounted Attenuator (TMA)          |
|  | Trailer Mounted Flashing Arrow Board |  | Portable Changeable Message Sign (PCMS) |
|  | Sign                                 |  | Traffic Flow                            |
|  | Flag                                 |  | Flagger                                 |

| Posted Speed<br>* | Formula                  | Minimum Desirable Taper Lengths<br>** |            |            | Suggested Maximum Spacing of Channelizing Devices |              | Minimum Sign Spacing<br>"x" | Suggested Longitudinal Buffer Space<br>"b" | Stopping Sight Distance |
|-------------------|--------------------------|---------------------------------------|------------|------------|---------------------------------------------------|--------------|-----------------------------|--------------------------------------------|-------------------------|
|                   |                          | 10' Offset                            | 11' Offset | 12' Offset | On a Taper                                        | On a Tangent |                             |                                            |                         |
| 30                | L = WS <sup>2</sup> / 60 | 150'                                  | 165'       | 180'       | 30'                                               | 60'          | 120'                        | 90'                                        | 200'                    |
| 35                |                          | 205'                                  | 225'       | 245'       | 35'                                               | 70'          | 160'                        | 120'                                       | 250'                    |
| 40                |                          | 265'                                  | 295'       | 320'       | 40'                                               | 80'          | 240'                        | 155'                                       | 305'                    |
| 45                | L = WS                   | 450'                                  | 495'       | 540'       | 45'                                               | 90'          | 320'                        | 195'                                       | 360'                    |
| 50                |                          | 500'                                  | 550'       | 600'       | 50'                                               | 100'         | 400'                        | 240'                                       | 425'                    |
| 55                |                          | 550'                                  | 605'       | 660'       | 55'                                               | 110'         | 500'                        | 295'                                       | 495'                    |
| 60                | L = WS                   | 600'                                  | 660'       | 720'       | 60'                                               | 120'         | 600'                        | 350'                                       | 570'                    |
| 65                |                          | 650'                                  | 715'       | 780'       | 65'                                               | 130'         | 700'                        | 410'                                       | 645'                    |
| 70                |                          | 700'                                  | 770'       | 840'       | 70'                                               | 140'         | 800'                        | 475'                                       | 730'                    |
| 75                | L = WS                   | 750'                                  | 825'       | 900'       | 75'                                               | 150'         | 900'                        | 540'                                       | 820'                    |
| 80                |                          | 800'                                  | 880'       | 960'       | 80'                                               | 160'         | 1000'                       | 610'                                       | 900'                    |

\* Conventional Roads Only  
\*\* Taper lengths have been rounded off.  
L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE

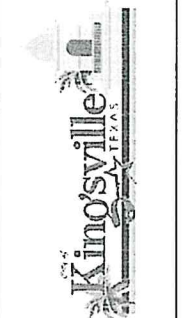
|  | MOBILE | SHORT DURATION | SHORT TERM STATIONARY | INTERMEDIATE TERM STATIONARY | LONG TERM STATIONARY |
|--|--------|----------------|-----------------------|------------------------------|----------------------|
|  |        | ✓              | ✓                     | ✓                            |                      |

GENERAL NOTES

- Flags attached to signs where shown, are REQUIRED.
  - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
  - The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4 "ONE LANE ROAD XXX FT" sign, but proper sign spacing shall be maintained.
  - Flaggers should use two-way radios or other methods of communication to control traffic.
  - Length of work space should be based on the ability of flaggers to communicate.
  - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
  - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.
- TCP (2-2a)
- The R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work space should be no longer than one half city block. In rural areas, roadways with less than 2000 ADT, work space should be no longer than 400 feet.
  - The R1-2aP "YIELD TO ONCOMING TRAFFIC" sign shall be placed on a support at a 7 foot minimum mounting height.
- TCP (2-2b)
- Channelizing devices on the center line may be omitted when a pilot car is leading traffic and approved by the Engineer.
  - If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain stopping sight distance to the flagger and a queue of stopped vehicles. (See table above).
  - Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

TRAFFIC CONTROL PLAN  
ONE-LANE TWO-WAY  
TRAFFIC CONTROL  
TCP (2-2) - 18

|           |               |      |      |     |         |
|-----------|---------------|------|------|-----|---------|
| FILE:     | tcp2-2-18.dgn | DN:  | CK:  | DN: | CK:     |
| © TxDOT   | December 1985 | CONT | SECT | JOB | HIGHWAY |
| REVISIONS |               |      |      |     |         |
| 8-95      | 3-03          |      |      |     |         |
| 1-97      | 2-12          |      |      |     |         |
| 4-98      | 2-18          |      |      |     |         |
| 162       |               |      |      |     |         |



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Date: 02/01/2020  
Checked by: R. MORA  
Job:

2020 DOWNTOWN IMPROVEMENTS  
7TH STREET  
TRAFFIC CONTROL PLAN  
TCP (2-2)-18