

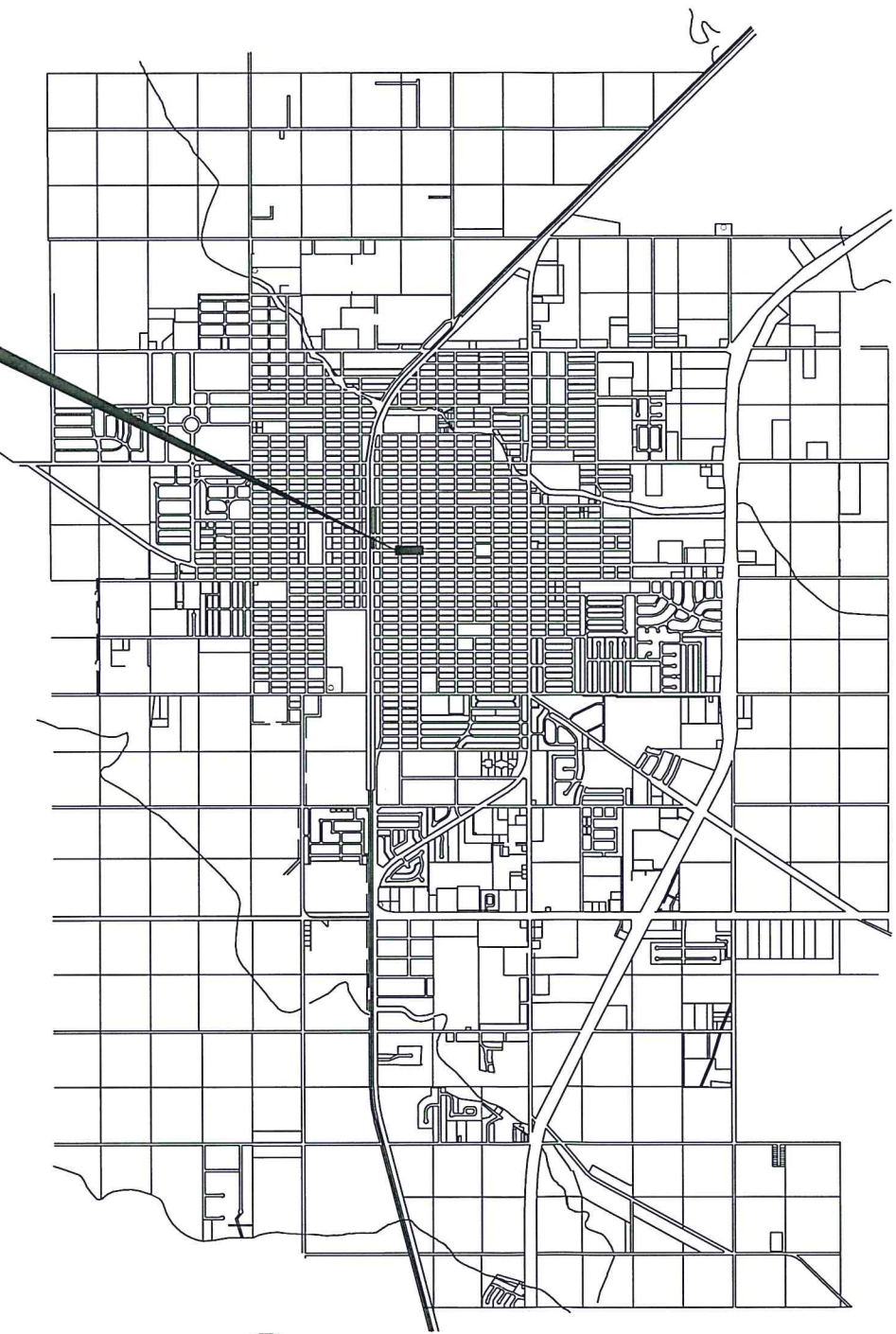
CITY OF KINGSVILLE

2018-2019

DOWNTOWN IMPROVEMENTS

PROJECT LOCATION

KLEBERG AVE. FROM
7TH ST. TO 8TH ST.



MAYOR
SAM FUGATE

INTERIM CITY MANAGER
DEBORAH BALLI

CITY COMMISSIONERS
HECTOR M. HINOJOSA
DIANNE LEUBERT
ARTURO PECOS
EDNA LOPEZ



LOCATION MAP

NOT TO SCALE

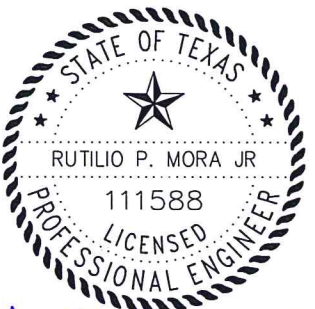
NOVEMBER 2019

SHEET INDEX

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6	STA. -1+00 TO STA. 1+50 (DEMOLITION PLAN)
7	STA. 1+50 TO STA. 3+50 (DEMOLITION PLAN)
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9	STA. -1+00 TO STA. 1+50 (PROPOSED PLAN)
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11	STA. 3+50 TO STA. END (PROPOSED PLAN)
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13	PROPOSED KLEBERG AVENUE & 8TH ST. BULB OUTS
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15	DECORATIVE LAMP DETAILS
16	MISCELLANEOUS DETAILS I
17	MISCELLANEOUS DETAILS II
18	MISCELLANEOUS DETAILS III
19	MISCELLANEOUS DETAILS IV
20	TRAFFIC CONTROL PLAN - TCP (2-2) - 18

THE SEAL APPEARING ON THIS
DOCUMENT WAS AUTHORIZED BY
RUTILIO P. MORA JR, P.E. NO. 111588
ON 11-04-19. ALTERATION OF A
SEALED DOCUMENT WITHOUT PROPER
NOTIFICATION TO THE RESPONSIBLE
ENGINEER IS AN OFFENSE UNDER THE
TEXAS ENGINEERING PRACTICE ACT.



Rutilio P. Mora Jr 11/4/2019

RUTILIO P. MORA JR, P.E. NO. 111588

CITY OF KINGSVILLE
ENGINEERING DEPARTMENT
400 West King
Kingsville, Texas 78363
Office 361.595.8007
Fax 361.595.8035



Drawn by: J. RAMIREZ
Date: 6/04/2019
Checked by: R. MORA
Job:
Scale: N.T.S.

2018-2019 DOWNTOWN IMPROVEMENTS

COVER SHEET

SHEET
1

GENERAL CONSTRUCTION NOTES:

1. ALL IMPROVEMENTS TO BE IN ACCORDANCE WITH CITY OF KINGSVILLE CODES.
2. CONTRACTOR TO VERIFY LOCATION AND ELEVATION OF EXISTING FACILITIES PRIOR TO CONSTRUCTION.
3. CONTRACTOR TO PLAN AND PERFORM HIS WORK IN A MANNER THAT WILL PERMIT SAFE PUBLIC TRAFFIC MOVEMENT ON ALL STREETS.
4. TEMPORARY TRAFFIC CONTROL DURING CONSTRUCTION TO BE IN ACCORDANCE WITH SPECIFICATIONS.
5. CONTRACTOR SHALL PROVIDE PROTECTIVE DEVICES SUCH AS SIGNS, LIGHTS, AND SIGNALS FOR THE SAFETY OF THE PUBLIC AND WORKERS, AS REQUIRED, AND AS DIRECTED BY CITY INSPECTOR.
6. CONTRACTOR TO BE RESPONSIBLE FOR PROTECTION AND/OR SAFETY OF THE WORK SITE, WORKERS, SUBCONTRACTORS, MATERIALS AND/OR EQUIPMENT.
7. CONSTRUCTION STAKING SHALL BE PROVIDED BY THE CONTRACTOR AND AT CONTRACTOR'S EXPENSE. BASE LINES ARE STAKED AS SHOWN ON PLANS. ALL DIMENSIONS ARE TO BACK OF CURBS UNLESS SHOWN OTHERWISE.
8. MATERIAL TESTING SHALL BE PROVIDED BY THE CITY OF KINGSVILLE. RE-TEST DUE TO FAILURES TO BE AT CONTRACTOR'S EXPENSE.
9. CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL BY THE CITY ENGINEERING OFFICE PRIOR TO CONSTRUCTION. CONTRACTOR MAY CLOSE STREETS TO THRU TRAFFIC IN INCREMENTS AS LONG AS ACCESS IS MAINTAINED TO ALL RESIDENCES, BUSINESSES, & ADJOINING STREETS. TRAFFIC CONTROL PLAN WILL BE IN ACCORDANCE WITH TXDOT'S TCP (2-2) - 18 ON SHEET 16 AND THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED AND AT CONTRACTOR'S EXPENSE.
11. ANY DAMAGE TO EXISTING PAVEMENT, DRAINAGE OR EXISTING STRUCTURES SHALL BE REPAIRED TO PRE-CONSTRUCTION CONDITION AT CONTRACTOR'S EXPENSE.
12. "THESE PLANS, PREPARED BY THE CITY OF KINGSVILLE ENGINEERING DEPARTMENT DO NOT EXTEND TO OR INCLUDE DESIGNS OR SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK. THE SEAL OF CITY OF KINGSVILLE'S REGISTERED PROFESSIONAL ENGINEER(S) HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED IN THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS, INCLUDING THE PLANS AND SPECIFICATIONS REQUIRED BY THE HOUSE BILLS 662 AND 665 ENACTED BY THE TEXAS LEGISLATURE IN THE 70TH LEGISLATURE - REGULAR SESSION."
13. CONTRACTOR SHALL GIVE NOTICE TO ALL AUTHORIZED INSPECTORS, SUPERINTENDENTS, OR PERSONS IN CHARGE OF PRIVATE AND PUBLIC UTILITIES AFFECTED BY HIS OPERATIONS PRIOR TO COMMENCEMENT OF WORK. NOTIFY TEXAS ONE CALL FOR UTILITY LOCATIONS PRIOR TO ANY & ALL EXCAVATIONS. COORDINATION OF ALL RELOCATION OF UTILITY POLES, ECT. TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR TO COORDINATE WITH THE CITY OF KINGSVILLE ON WORK SCHEDULES, TESTING, GENERAL INSPECTION, AND EXISTING LINES.
15. CONTRACTOR TO EXERCISE CAUTION WHEN WORKING NEAR EXISTING FACILITIES AND/OR UTILITIES. ALL DAMAGE TO BE REPAIRED AT CONTRACTOR'S EXPENSE. ALL COSTS FOR INTERRUPTION OF GAS, ELECTRICAL, COMMUNICATIONS AND/OR WATER SERVICE DUE TO CONTRACTOR'S WORK SHALL BE BORNE BY THE CONTRACTOR.
16. INFORMATION ON EXISTING UTILITIES IS FROM BEST AVAILABLE INFORMATION OF RECORD AND SPOT FIELD LOCATIONS. CONTRACTOR IS RESPONSIBLE FOR FIELD LOCATION OF THESE UNDERGROUND UTILITIES AS REQUIRED AT NO SEPARATE PAY. CITY OF KINGSVILLE PERSONNEL WILL BE AVAILABLE FOR ASSISTANCE AND OPERATION OF VALVES AS REQUIRED. CONTRACTOR TO COORDINATE WITH OTHER UTILITY COMPANIES, INCLUDING AEP ON ELECTRICAL UTILITIES, CENTERPOINT ENERGY ON GAS UTILITIES AND SOUTHWESTERN BELL ON TELEPHONE UTILITIES.
17. ALL SPOIL MATERIAL AND DEBRIS SHALL BE DISPOSED OF BY CONTRACTOR. FURNISHING AND TRANSPORTATION OF ALL OFFSITE MATERIAL TO BE AT CONTRACTOR'S EXPENSE.
18. UPON COMPLETION OF CONSTRUCTION, CONTRACTOR SHALL RETURN THE SITE TO ORIGINAL CONTOURS UNLESS DIFFERENT FINISHED ELEVATIONS ARE SHOWN ON PLANS. CONTRACTOR TO INSURE NO AREAS OF PONDING ARE PRESENT.
19. CONTRACTOR TO INSURE SAME DAY ACCESS TO SCHOOL, ALL RESIDENCES AND BUSINESSES ADJACENT TO CONSTRUCTION.
20. DEMOLITION, REMOVAL & DISPOSAL OF ALL EXCESS CONCRETE, CURBS, RUBBLE, ETC. TO BE AT CONTRACTOR'S EXPENSE.
21. STREET & SUBGRADE EXCAVATION TO BE THE PROPERTY OF THE CITY AND TO BE STOCK PILED AT THE CITY OF KINGSVILLE LANDFILL AS DIRECTED BY THE CITY REPRESENTATIVE.
22. CONCRETE NOTES:
 - a) ALL CONCRETE WORK TO BE FORMED, UNLESS OTHERWISE APPROVED.
 - b) ALL CONCRETE TO BE 3000 PSI MINIMUM AT 28 DAYS, UNLESS OTHERWISE SHOWN. STRENGTH TO BE DETERMINED BY CYLINDER BREAK TEST.
 - c) ALL REINFORCING STEEL TO BE ASTM A-615, GRADE 60, UNLESS OTHERWISE SHOWN.
 - d) ALL EXPOSED CONCRETE WORK TO BE CHAMFERED.
23. CONTRACTOR TO VERIFY THAT ALL CONCRETE SURFACES MEET THE FOLLOWING SLOPE CRITERIA PRIOR TO PLACEMENT OF CONCRETE.
 - a) RAMP SLOPE 12:1 (8.33%) (MAX)
 - b) RUNNING SLOPE 20:1 (5%) (MAX)
 - c) CROSS SLOPE 50:1 (2%) (MAX)(CROSS SLOPE IS SLOPE PERPENDICULAR TO THE DIRECTION OF TRAVEL.)
24. CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING SILT FENCE IN AREAS OF DISTURBED SOIL TO PREVENT EROSION FROM ENTERING DRIVEWAYS.

CALL BEFORE YOU DIG!



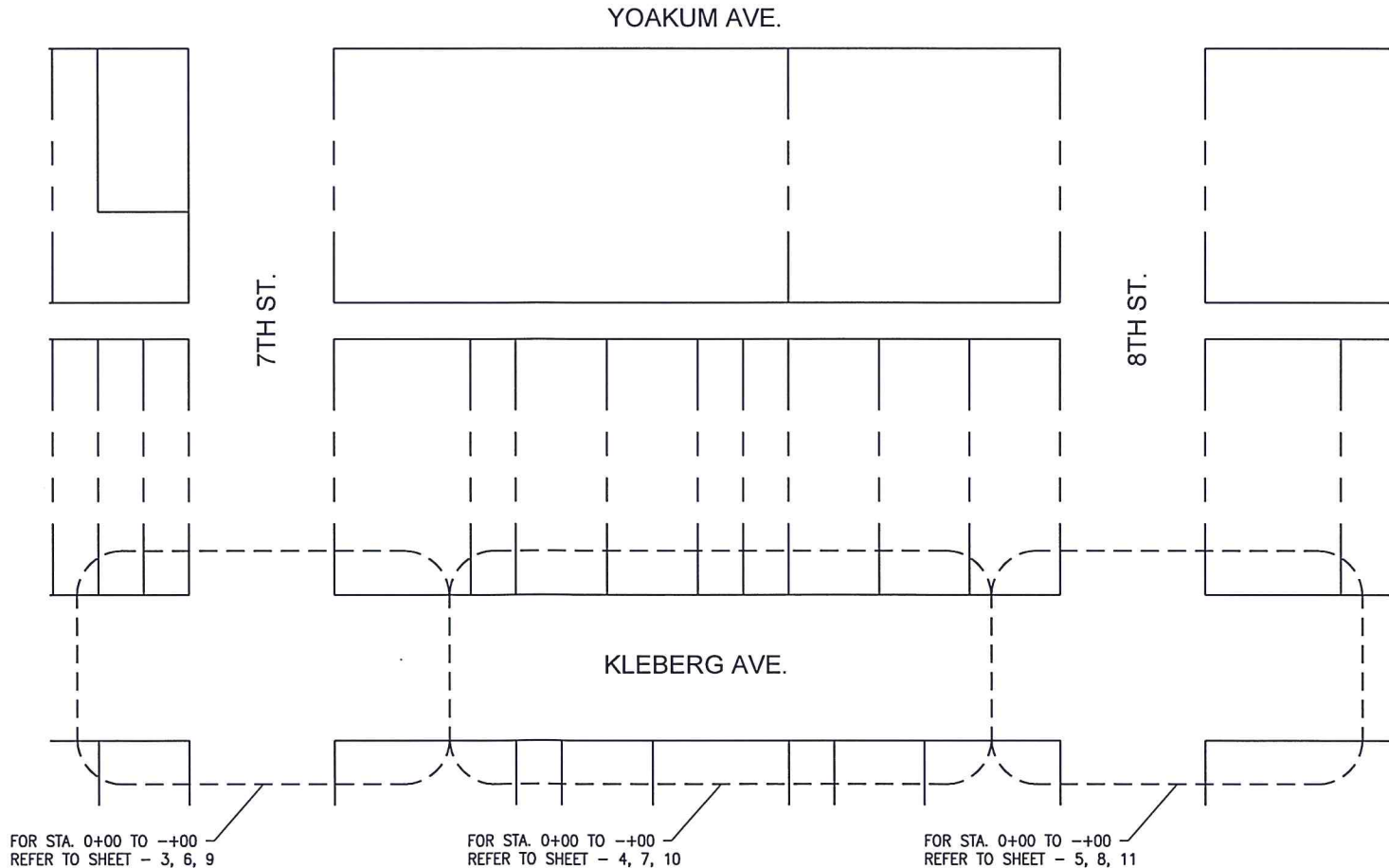
PARTICIPANTS REQUEST
48 HOURS NOTICE BEFORE YOU DIG,
DRILL OR BLAST - STOP AND CALL

811

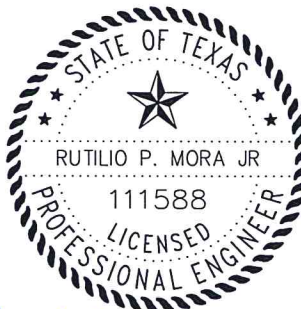
THE LONE STAR
NOTIFICATION COMPANY
AT 1-800-669-8344

LEGEND

EXISTING				PROPOSED			
	LIGHT POLE	60.04 TC	TOP OF CURB		DECORATIVE LAMP		SIDEWALK DRAIN
	STREET LAMP	60.69 GUT	GUTTER		SLOPE		CURB & GUTTER
	WATER VALVE	61.62 FF	FINISH FLOOR		SIGN		CROSSWALK
	FIRE HYDRANT	61.69 TW	TOP OF WALK		TOP OF CURB		STOP BAR
	POWER POLE	2.7%	SLOPE		GUTTER		SIDEWALK
	GUY WIRE				TOP OF STEP		PLANTER
	WATER METER		GRASS AREA		TOP OF WALK		VALLEY GUTTER
	BRICK PAVERS		TERAZZO		BRICK PAVERS		UNDERGROUND ELECTRICAL
	SIGN		ELECTRICAL BOX		SAW CUT		
	TREE		SIDEWALK		BUILT UP RAMP		
	SIDEWALK DRAIN		VALLEY GUTTER		STATIONING		



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Rutilio P. Mora Jr. 11/4/2019

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CITY OF KINGSVILLE
ENGINEERING DEPARTMENT

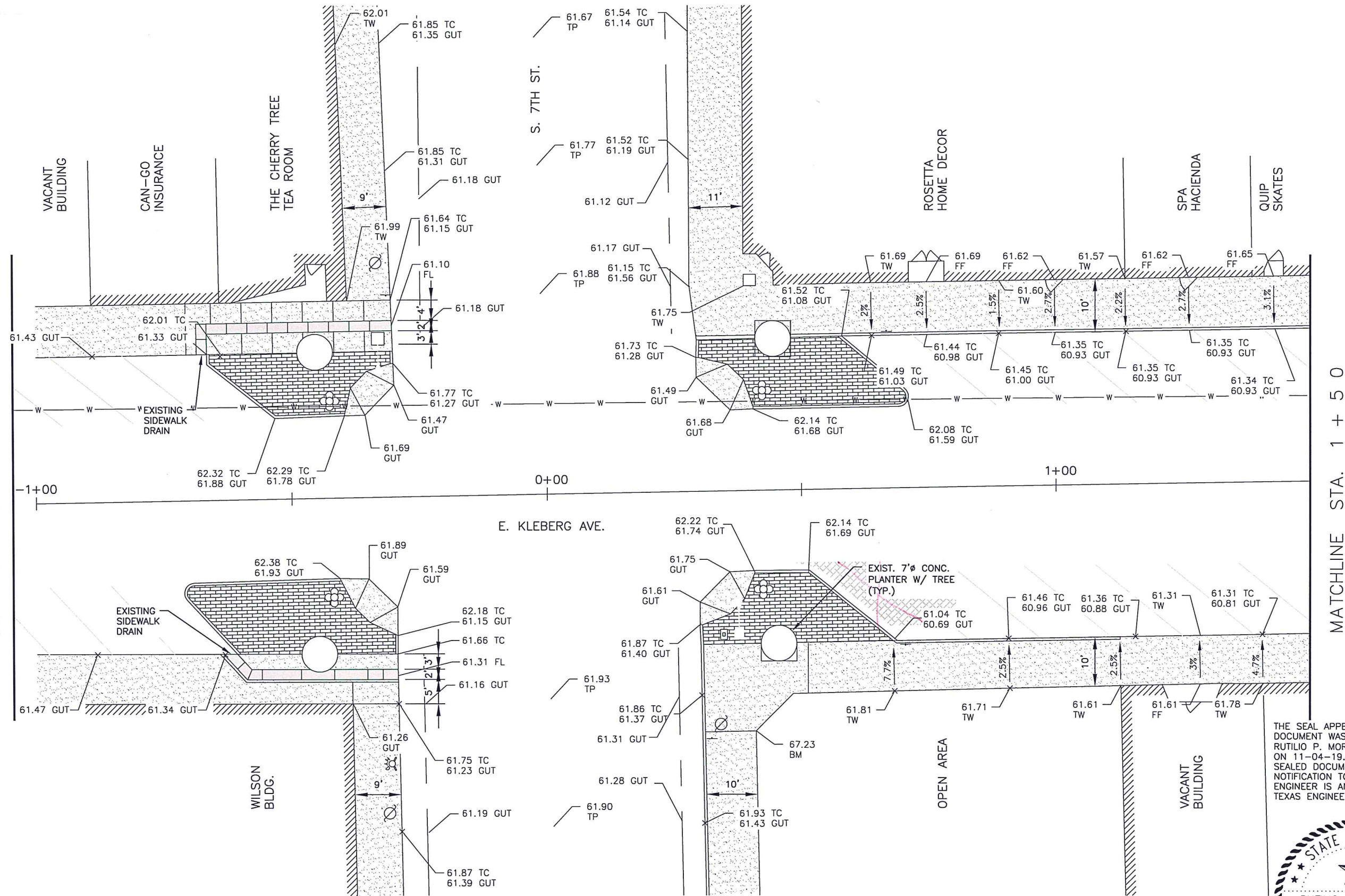
400 West King
Kingsville, Texas 78363
Office 361.595.8007
Fax 361.595.8035

2018-2019 DOWNTOWN IMPROVEMENTS

GENERAL NOTES & OVERALL SITE PLAN

SHEET
2

MATCHLINE STA. - 1 + 0 0



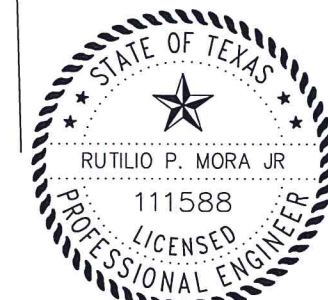
MATCHLINE STA. 1 + 5 0



A
3

STA. -1+00 - 1+50 (EXISTING PLAN)

SCALE: 1:20



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STA. -1+00 TO STA. 1+50 (EXISTING PLAN)

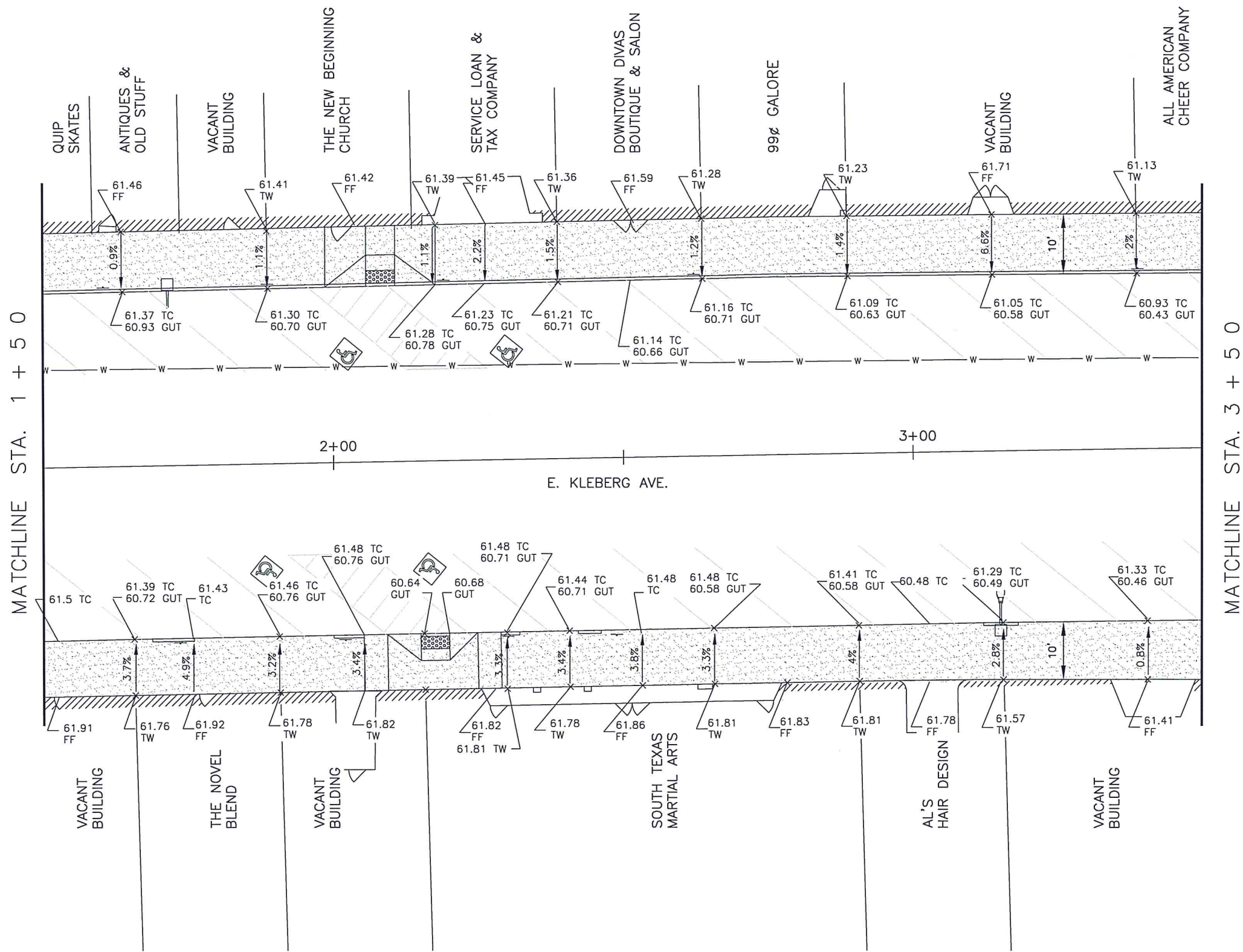
Drawn by: J. RAMIREZ
Date: 6/04/2019
Checked by: R. MORA
Job:

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ENGINEERING DEPARTMENT
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Kingsville, Texas 78363
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SHEET

3



A
4

STA. 1+50 - 3+50 (EXISTING PLAN)

SCALE: 1:20



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STA. 1+50 TO STA. 3+50 (EXISTING PLAN)

SHEET

4

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Date: 6/04/2019

Checked by: R. MORA

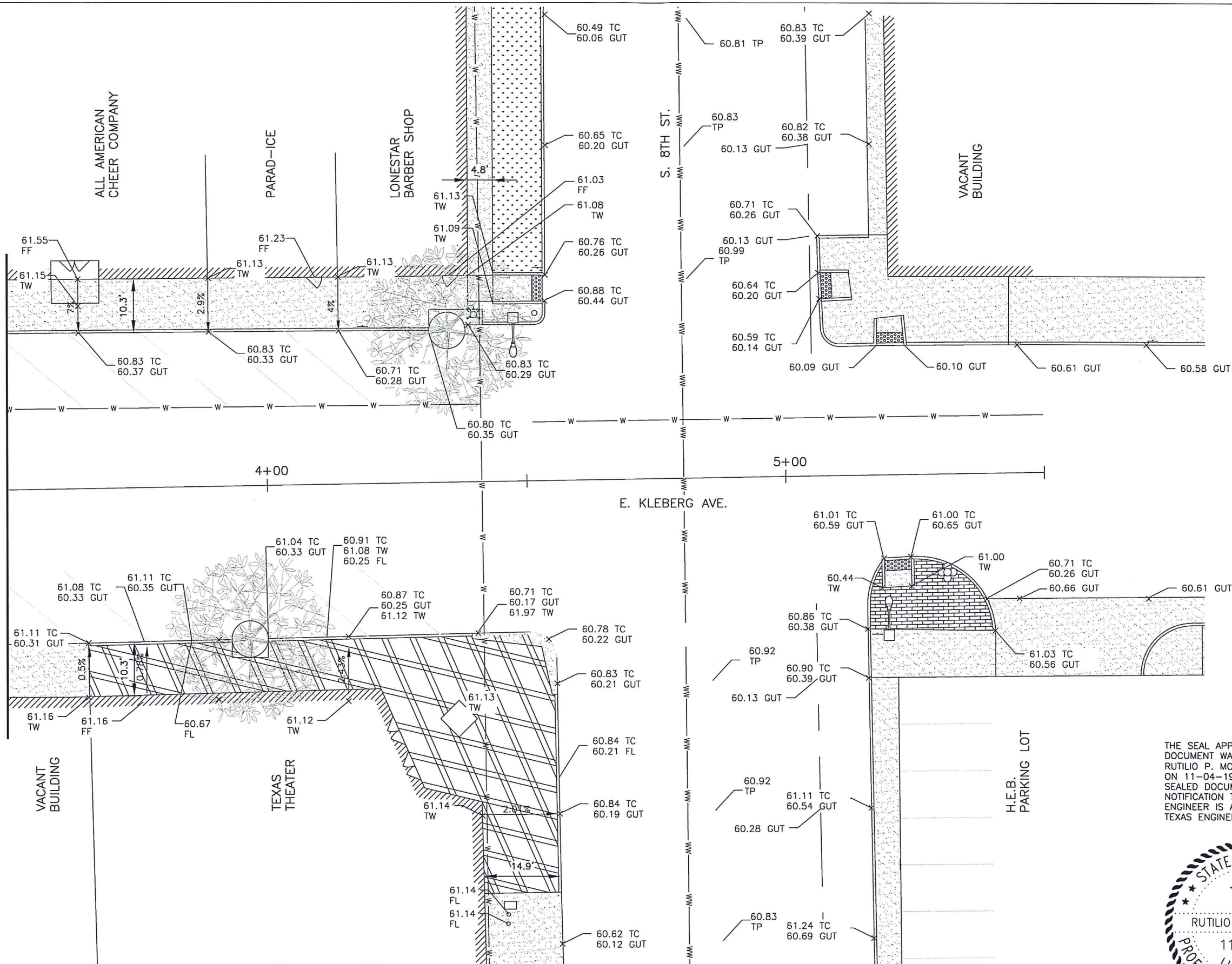
Job:

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ENGINEERING DEPARTMENT



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Kingsville, Texas 78363
Office 361.595.8007
Fax 361.595.8035

MATCHLINE STA. 3 + 50



A
5

STA. 3+50 - STA. END (EXISTING PLAN)

SCALE: 1:20



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STA. 3+50 TO STA. END (EXISTING PLAN)

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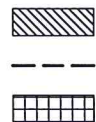
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SHEET

5

DEMOLITION NOTES

- ① REMOVE EXISTING RAMP
- ② REMOVE EXISTING CURB
- ③ REMOVE EXISTING SIDEWALK
- ④ REMOVE EXISTING PAVEMENT
- ⑤ REMOVE EXISTING TERRAZO
- ⑥ REMOVE SIGN & PROVIDE TO CITY (BASE BID)
- ⑦ REMOVE EXISTING BRICK PAVERS & STORE FOR REUSE
- ⑧ RELOCATE STOP SIGN
- ⑨ REMOVE SIGN & PROVIDE TO CITY (ALT. BID 1)



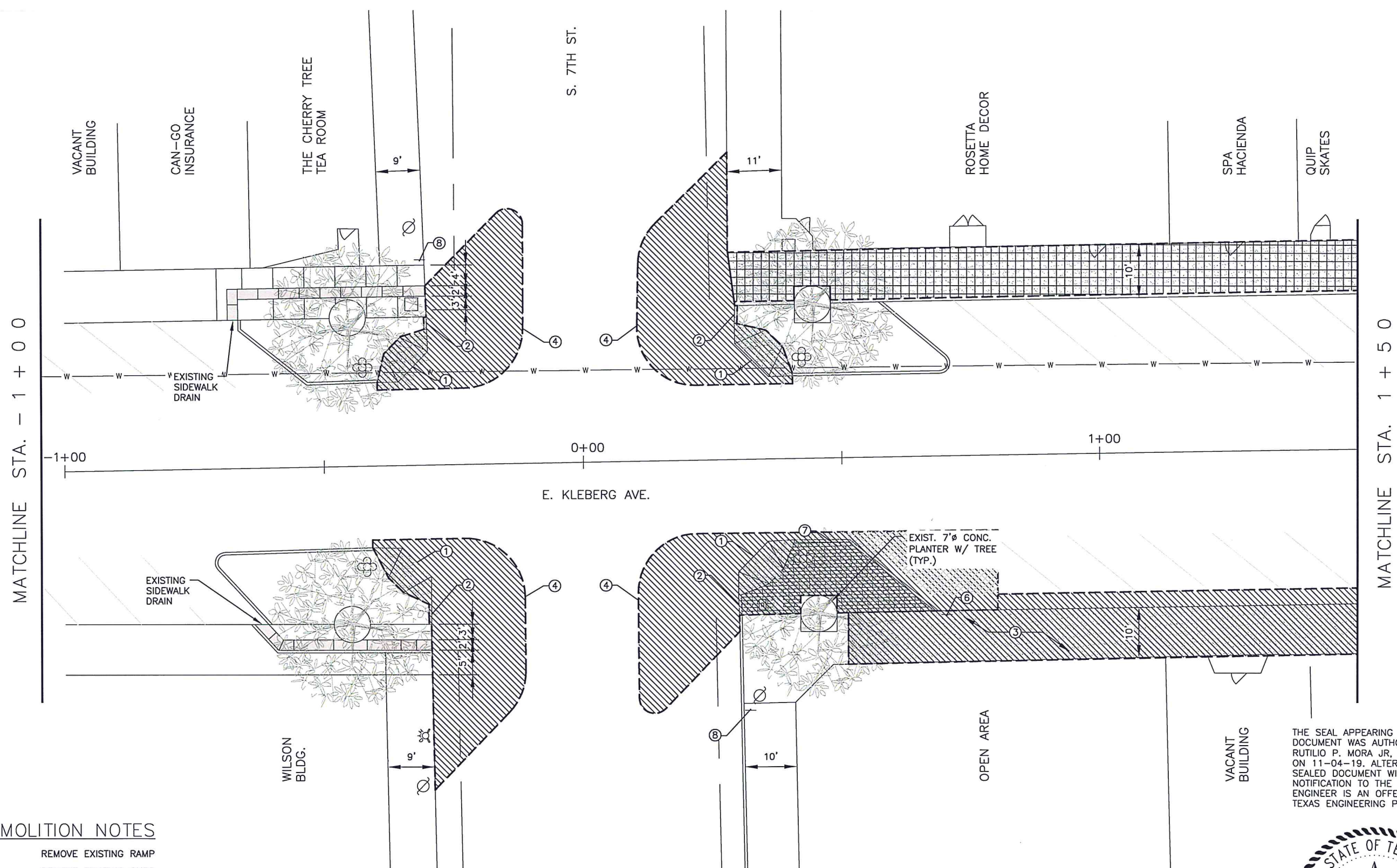
AREAS TO BE DEMOLISHED
 AREAS TO BE SAW-CUT
 AREAS TO BE DEMOLISHED (ALT. BID 1)



A
6

STA. -1+00 - 1+50 (DEMOLITION PLAN)

SCALE: 1:20



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2018-2019 DOWNTOWN IMPROVEMENTS

KLEBERG AVE. - 7TH ST. TO 8TH ST.

STA. -1+00 TO STA. 1+50 (DEMOLITION PLAN)

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Checked by: R. MORA

Job:

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 ENGINEERING DEPARTMENT



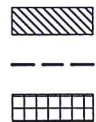
400 West King
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SHEET

6

DEMOLITION NOTES

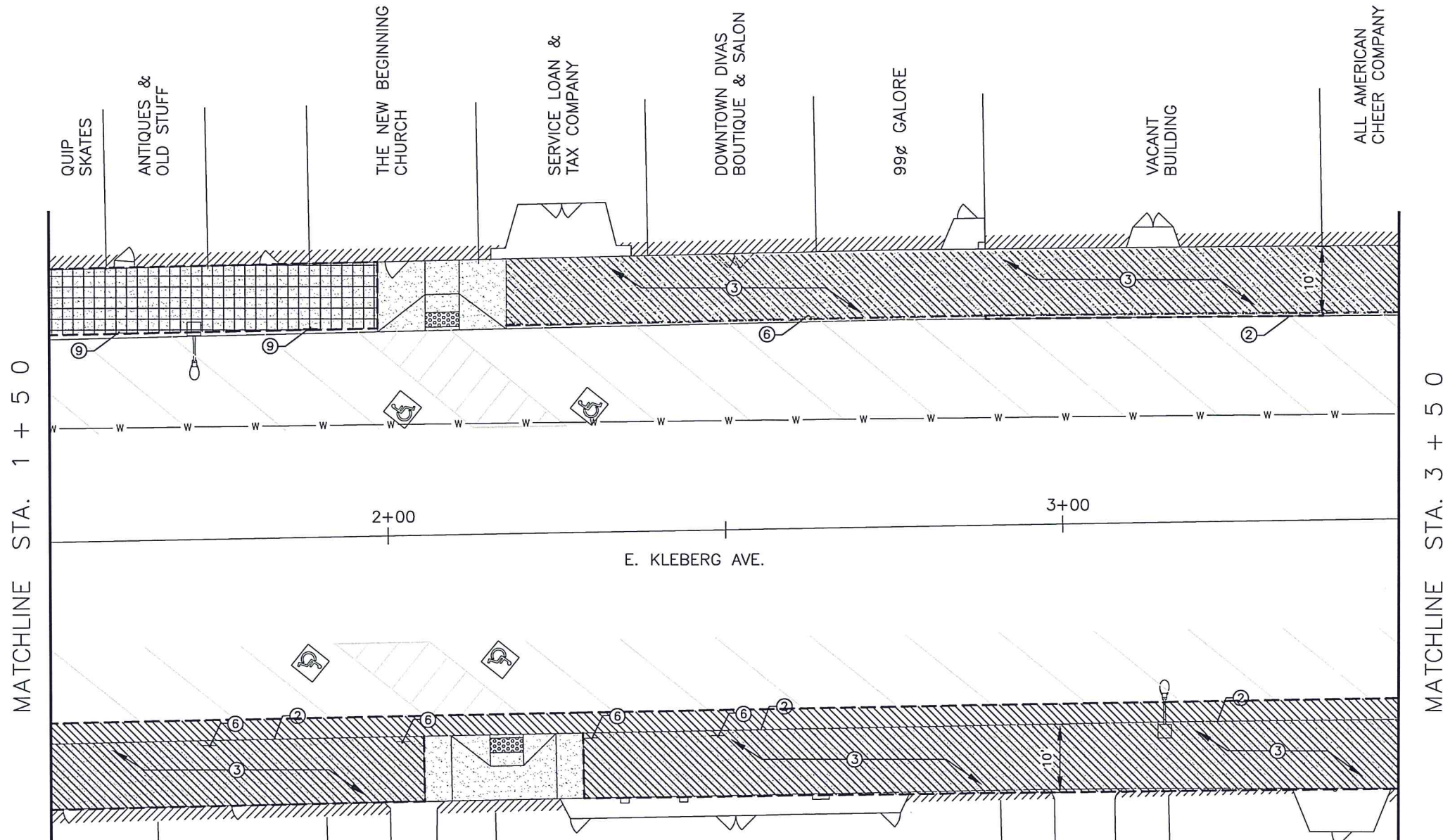
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- ⑧ RELOCATE STOP SIGN
- ⑨ REMOVE SIGN & PROVIDE TO CITY (ALT. BID 1)



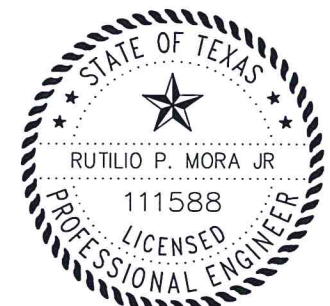
AREAS TO BE DEMOLISHED
 AREAS TO BE SAW-CUT
 AREAS TO BE DEMOLISHED (ALT. BID 1)



A
 7
 STA. 1+50 - 3+50 (DEMOLITION PLAN)
 SCALE: 1:20



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2018-2019 DOWNTOWN IMPROVEMENTS

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 STA. 1+50 TO STA. 3+50 (DEMOLITION PLAN)

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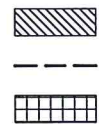
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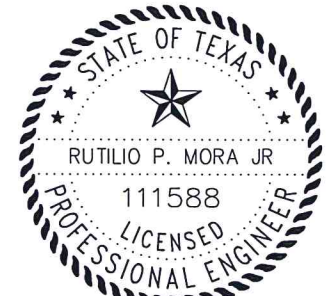
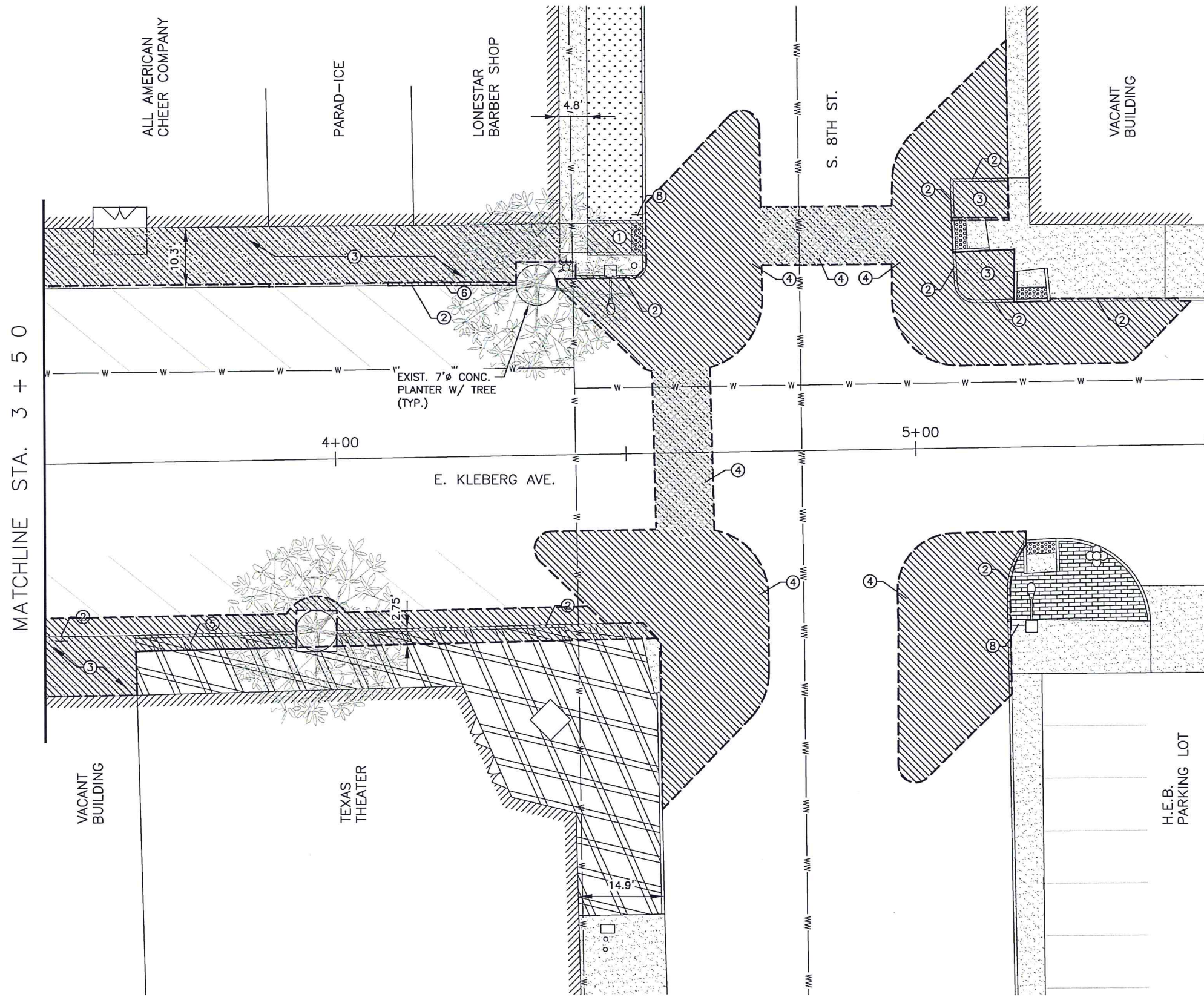
AREAS TO BE DEMOLISHED
AREAS TO BE SAW-CUT
AREAS TO BE DEMOLISHED (ALT. BID 1)



A
8

STA. 3+50 - STA. END (DEMOLITION PLAN)

SCALE: 1:20



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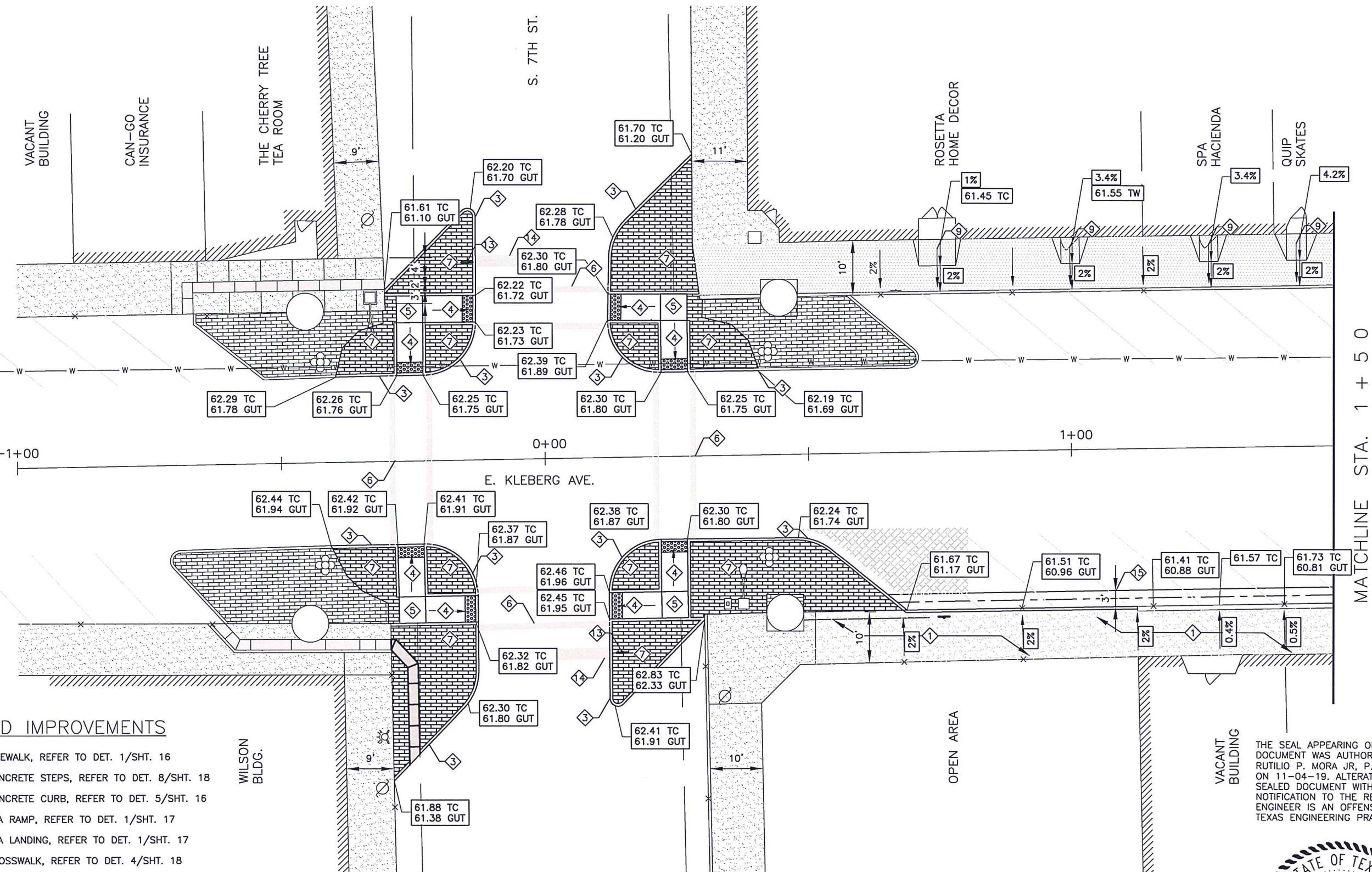
SHEET
8

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MATCHLINE STA. - 1 + 0 0



PROPOSED IMPROVEMENTS

- ① NEW SIDEWALK, REFER TO DET. 1/SHT. 16
- ② NEW CONCRETE STEPS, REFER TO DET. 8/SHT. 18
- ③ NEW CONCRETE CURB, REFER TO DET. 5/SHT. 16
- ④ NEW ADA RAMP, REFER TO DET. 1/SHT. 17
- ⑤ NEW ADA LANDING, REFER TO DET. 1/SHT. 17
- ⑥ NEW CROSSWALK, REFER TO DET. 4/SHT. 18
- ⑦ NEW BRICK PAVERS (MATCH EXISTING), REFER TO DET. 6/SHT. 18
- ⑧ NEW SIDEWALK DRAIN, REFER TO DET. 1/SHT. 18
- ⑨ NEW BUILT UP RAMP, REFER TO DET. 2/SHT. 19
- ⑩ NEW DECORATIVE LAMP WITH BASE, REFER TO SHT. 15
- ⑪ NEW PLANTER, REFER TO DET. 1/SHT. 19
- ⑫ NEW SIGN, REFER TO DET. 5/SHT. 18
- ⑬ RELOCATED STOP SIGN
- ⑭ STOP BAR, REFER TO DET. 4/SHT. 18
- ⑮ CONCRETE VALLEY GUTTER, REFER TO DET. 6/SHT. 16

WILSON
BLDG.

NEW SIDEWALK
(ALT. BID 1)



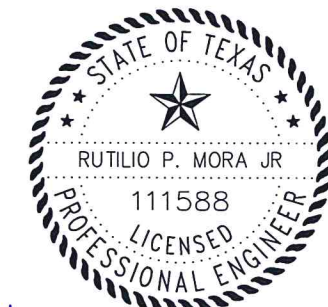
A
9

STA. -1+00 - 1+50 (PROPOSED PLAN)

SCALE: 1:20

VACANT
BUILDING

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RUTILIO P. MORA JR., P.E. NO. 111588

MATCHLINE STA. 1 + 5 0

2018-2019 DOWNTOWN IMPROVEMENTS

KLEBERG AVE. - 7TH ST. TO 8TH ST.
STA. -1+00 TO STA. 1+50 (PROPOSED PLAN)

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ENGINEERING DEPARTMENT



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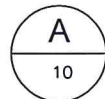
SHEET
9

PROPOSED IMPROVEMENTS

- ① NEW SIDEWALK, REFER TO DET. 1/SHT. 16
- ② NEW CONCRETE STEPS, REFER TO DET. 8/SHT. 18
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- ④ NEW ADA RAMP, REFER TO DET. 1/SHT. 17
- ⑤ NEW ADA LANDING, REFER TO DET. 1/SHT. 17
- ⑥ NEW CROSSWALK, REFER TO DET. 4/SHT. 18
- ⑦ NEW BRICK PAVERS (MATCH EXISTING), REFER TO DET. 6/SHT. 18
- ⑧ NEW SIDEWALK DRAIN, REFER TO DET. 1/SHT. 18
- ⑨ NEW BUILT UP RAMP, REFER TO DET. 2/SHT. 19
- ⑩ NEW DECORATIVE LAMP WITH BASE, REFER TO SHT. 15
- ⑪ NEW PLANTER, REFER TO DET. 1/SHT. 19
- ⑫ NEW SIGN, REFER TO DET. 5/SHT. 18
- ⑬ RELOCATED STOP SIGN
- ⑭ STOP BAR, REFER TO DET. 4/SHT. 18
- ⑮ CONCRETE VALLEY GUTTER, REFER TO DET. 6/SHT. 16

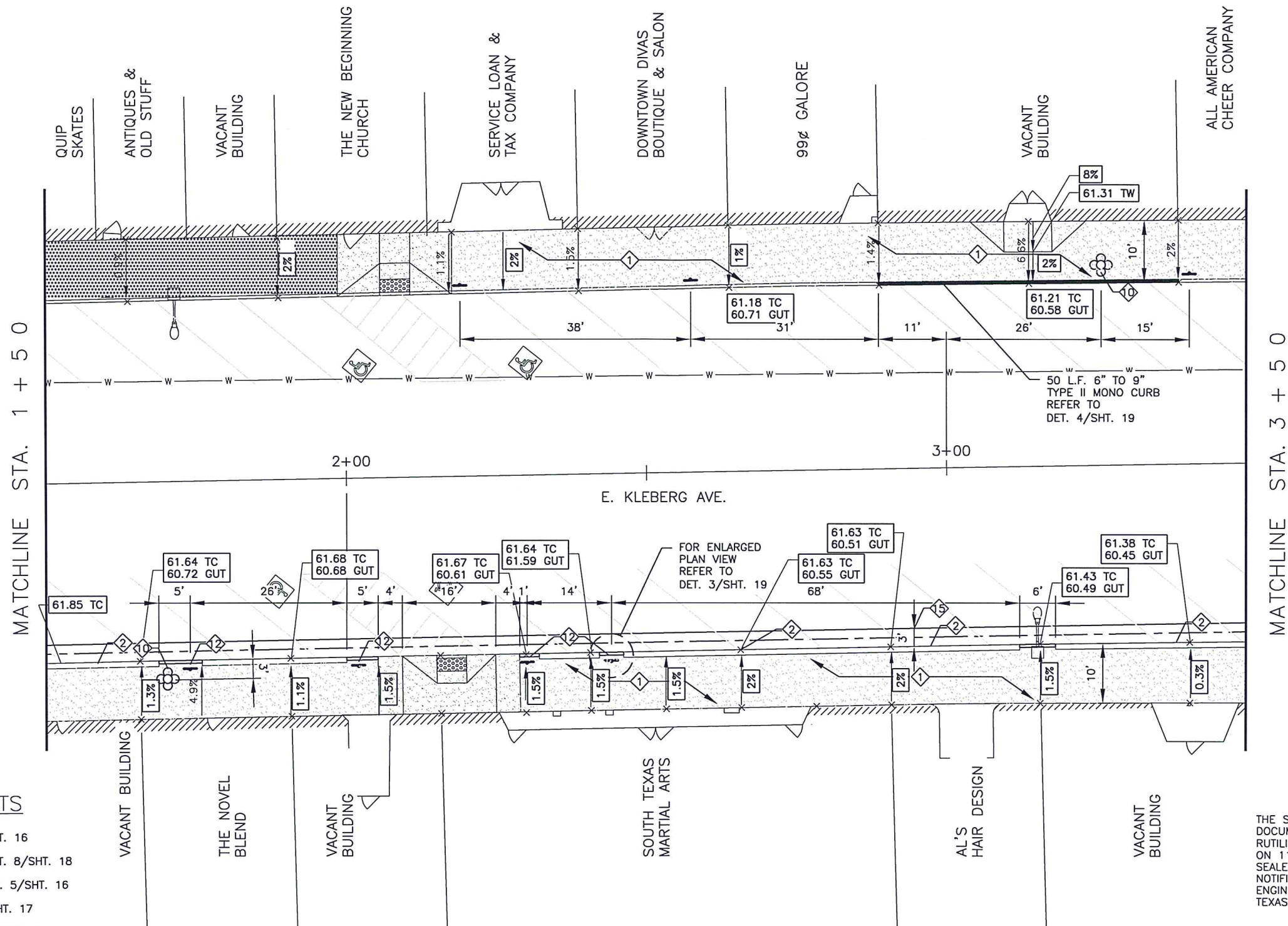


NEW SIDEWALK
(ALT. BID 1)

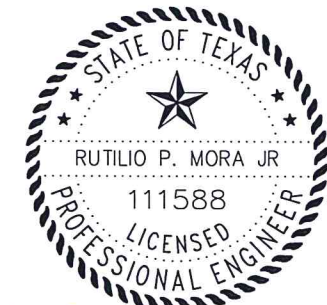


A STA. 1+50 - 3+50 (PROPOSED PLAN)

SCALE: 1:20



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Rutilio P. Mora Jr. 11/4/2019
RUTILIO P. MORA JR., P.E. NO. 111588

CITY OF KINGSVILLE
ENGINEERING DEPARTMENT



Drawn by: J. RAMIREZ

Date: 6/04/2019

Checked by: R. MORA

Job:

2018-2019 DOWNTOWN IMPROVEMENTS

KLEBERG AVE. - 7TH ST. TO 8TH ST.
STA. 1+50 TO STA. 3+50 (PROPOSED PLAN)

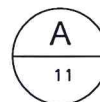
SHEET
10

PROPOSED IMPROVEMENTS

- 1 NEW SIDEWALK, REFER TO DET. 1/SHT. 16
- 2 NEW CONCRETE STEPS, REFER TO DET. 8/SHT. 18
- 3 NEW CONCRETE CURB, REFER TO DET. 5/SHT. 16
- 4 NEW ADA RAMP, REFER TO DET. 1/SHT. 17
- 5 NEW ADA LANDING, REFER TO DET. 1/SHT. 17
- 6 NEW CROSSWALK, REFER TO DET. 4/SHT. 18
- 7 NEW BRICK PAVERS (MATCH EXISTING), REFER TO DET. 6/SHT. 18
- 8 NEW SIDEWALK DRAIN, REFER TO DET. 1/SHT. 18
- 9 NEW BUILT UP RAMP, REFER TO DET. 2/SHT. 19
- 10 NEW DECORATIVE LAMP WITH BASE, REFER TO SHT. 15
- 11 NEW PLANTER, REFER TO DET. 1/SHT. 19
- 12 NEW SIGN, REFER TO DET. 5/SHT. 18
- 13 RELOCATED STOP SIGN
- 14 STOP BAR, REFER TO DET. 4/SHT. 18
- 15 CONCRETE VALLEY GUTTER, REFER TO DET. 6/SHT. 16



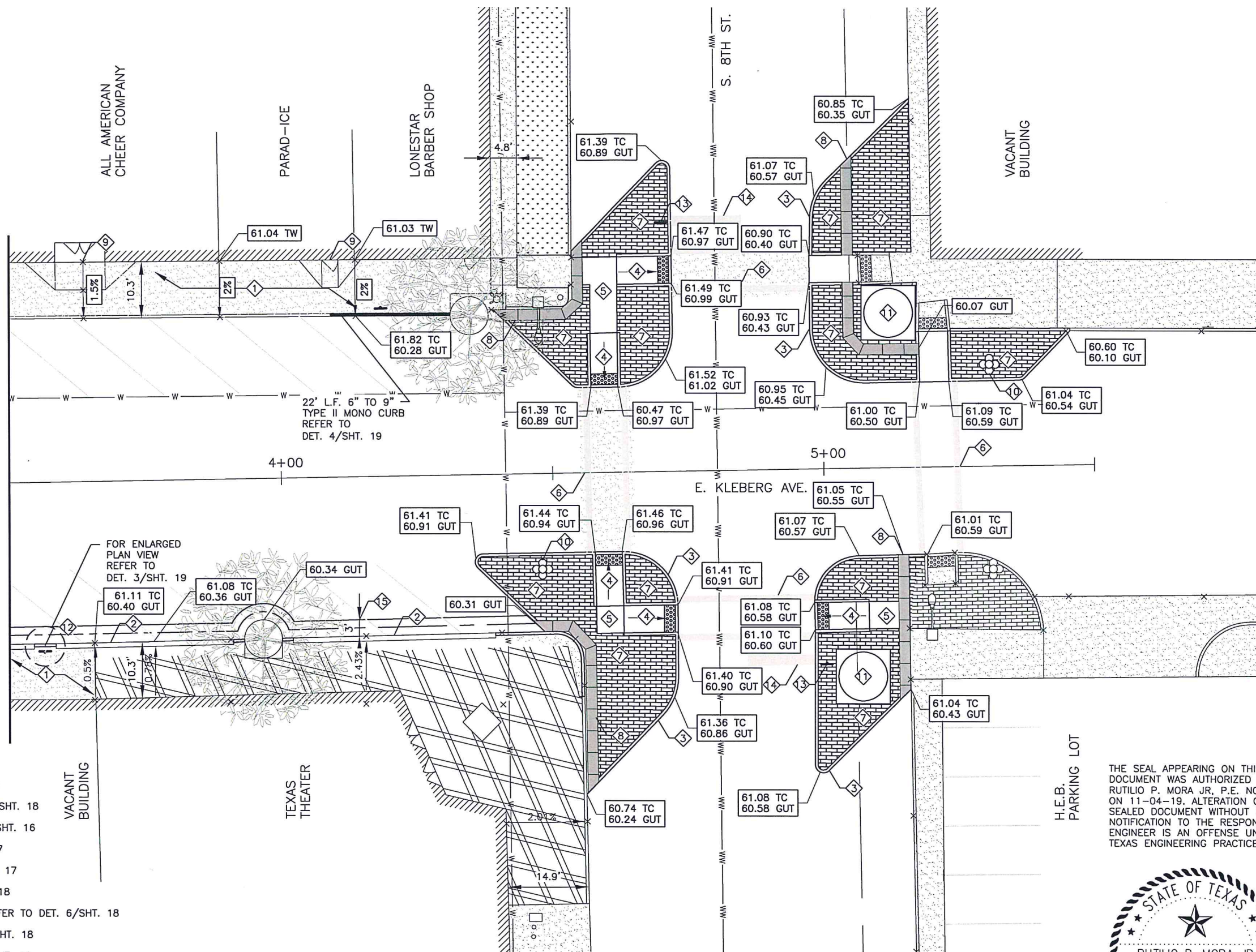
NEW SIDEWALK
(ALT. BID 1)



STA. 3+50 - STA. END (PROPOSED PLAN)

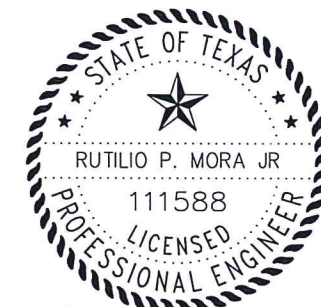
SCALE: 1:20

MATCHLINE STA. 3 + 50



H.E.B.
PARKING LOT

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2018-2019 DOWNTOWN IMPROVEMENTS

KLEBERG AVE. - 7TH ST. TO 8TH ST.
STA. 3+50 TO STA. END (PROPOSED PLAN)

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Job:

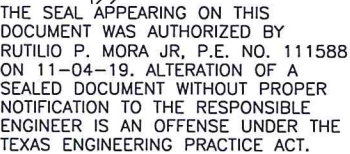
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400 West King
Kingsville, Texas 78363
Office 361.595.8007
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SHEET

11



Rutebo P. Mw 3. 11/4/2019

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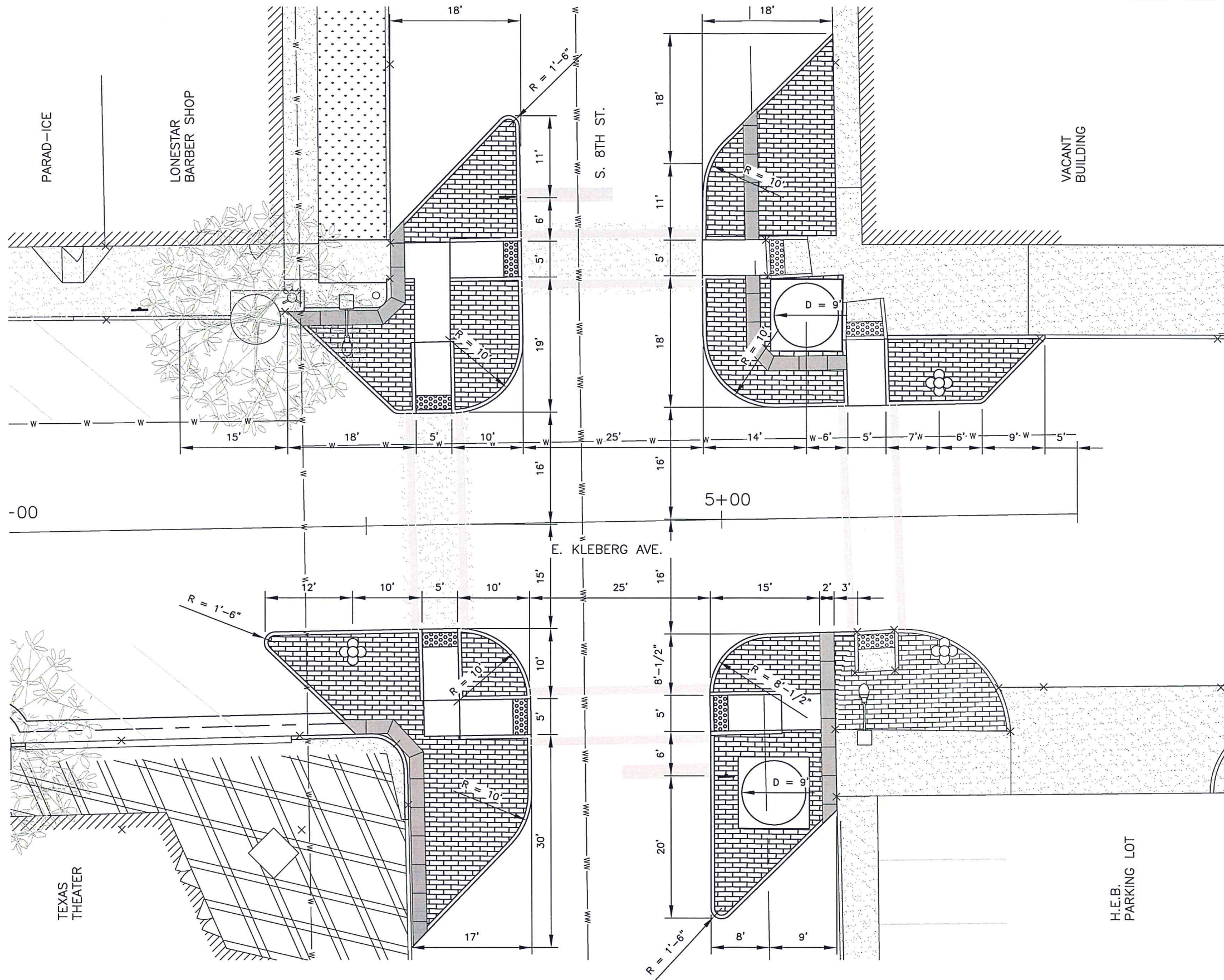
Checked by: R. MORA

Job:

2018-2019 DOWNTOWN IMPROVEMENTS

**KLEBERG AVE. - 7TH ST. TO 8TH ST.
PROPOSED KLEBERG AVENUE & 7TH ST. (BULB OUTS)**

SHEET
12



A
13

PROPOSED KLEBERG AVENUE & 8TH ST. (BULB OUTS)
SCALE: N.T.S.

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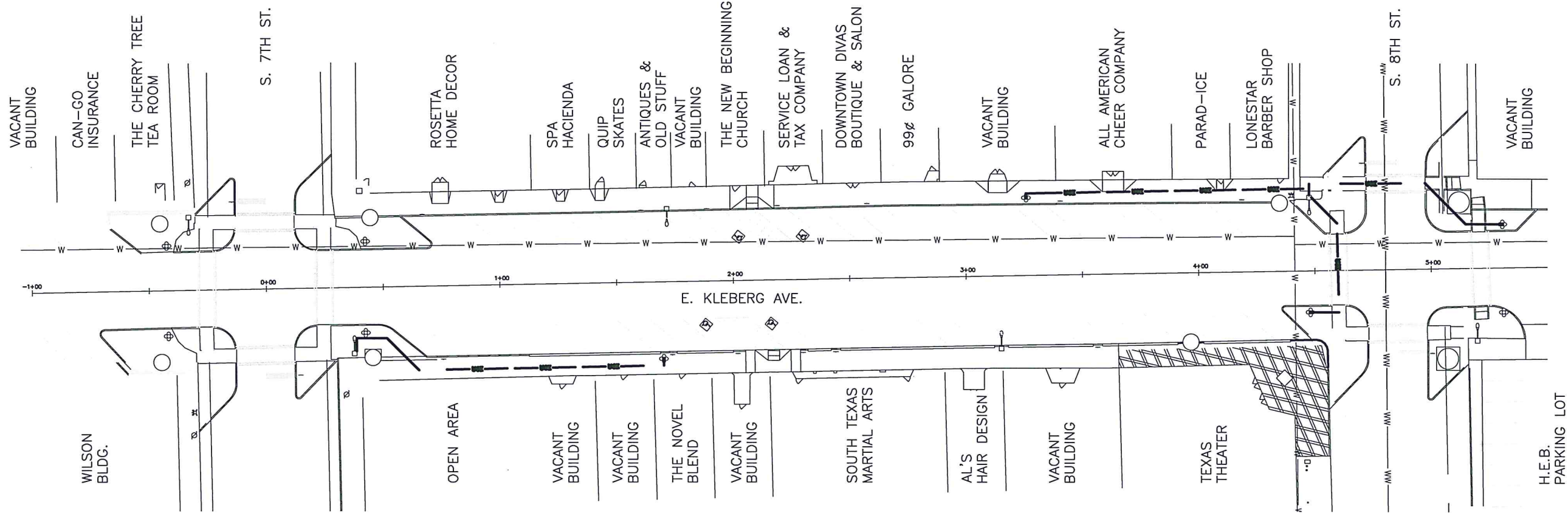


Drawn by: J. RAMIREZ
Date: 6/04/2019
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Job:

2018-2019 DOWNTOWN IMPROVEMENTS
KLEBERG AVE. - 7TH ST. TO 8TH ST.
PROPOSED KLEBERG AVENUE & 8TH ST. (BULB OUTS)

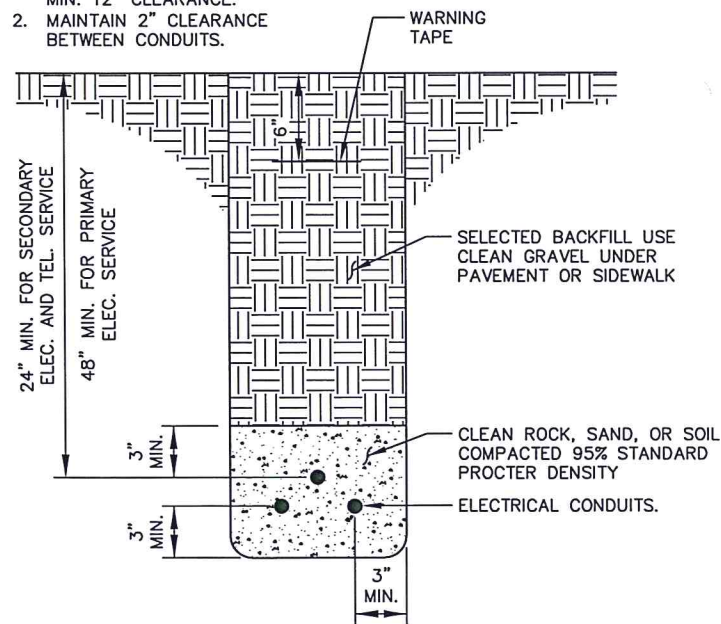
SHEET
13

NOTE:
CONTRACTOR TO VERIFY
UNDERGROUND ELECTRICAL
PLACEMENT AND
POWER CONNECTION.



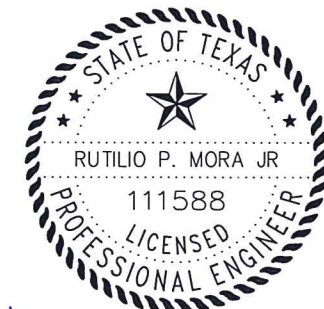
1 ELECTRICAL SITE PLAN
14 SCALE: 1:50

- NOTE:
1. WHERE CONDUITS ARE NEAR OTHER UTILITIES, MAINTAIN MIN. 12" CLEARANCE.
 2. MAINTAIN 2" CLEARANCE BETWEEN CONDUITS.



2 UNDERGROUND CONDUIT (TYP.)
14 SCALE: N.T.S.

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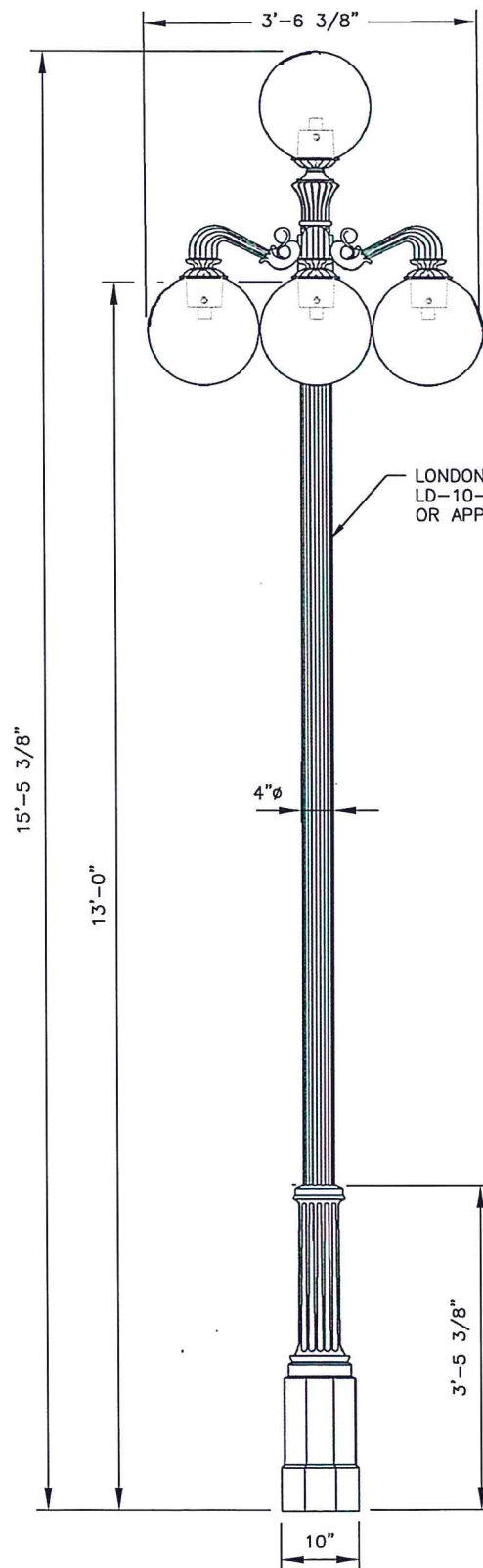
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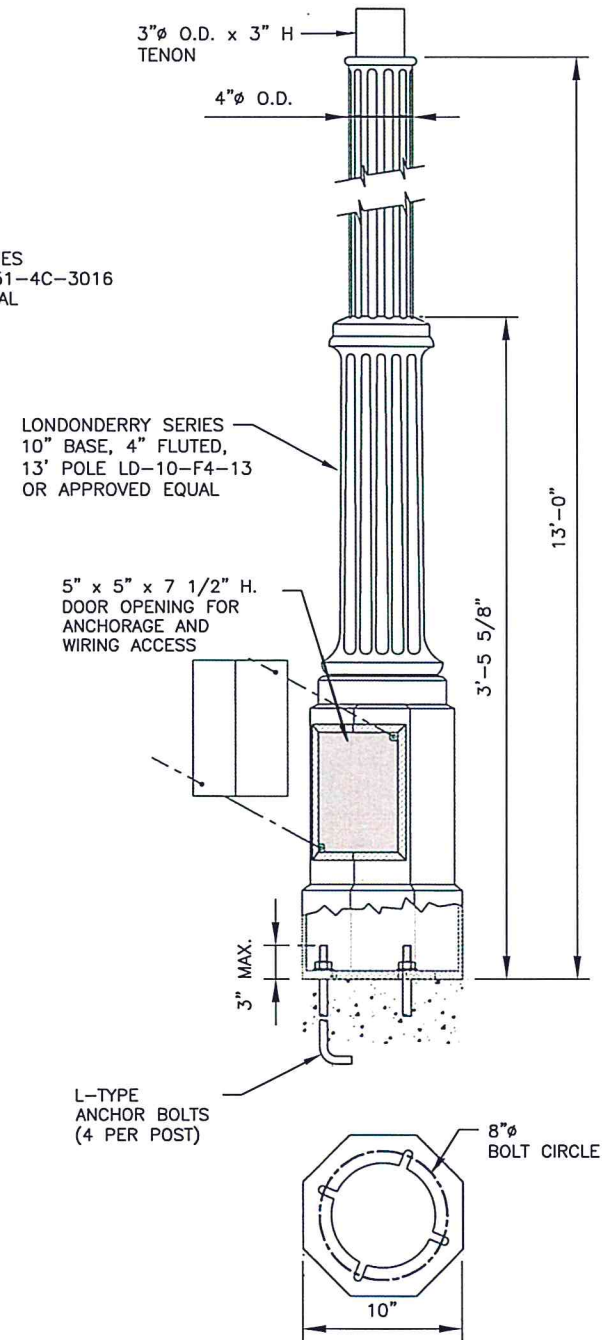
2018-2019 DOWNTOWN IMPROVEMENTS

ELECTRICAL SITE PLAN

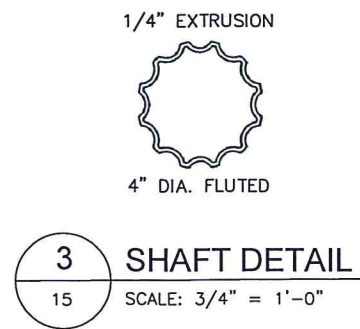


1 DECORATIVE LAMP DETAIL
15 SCALE: 1/2" = 1'-0"

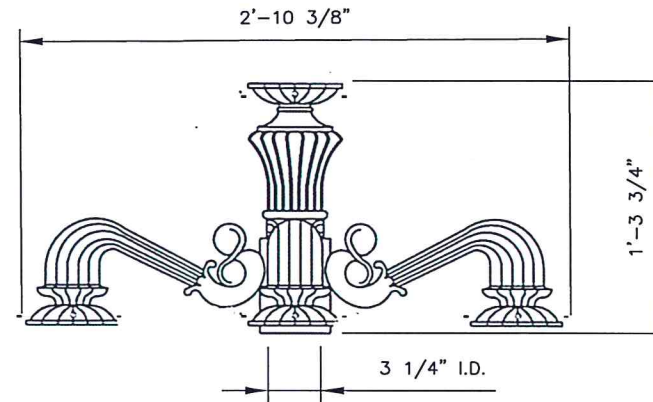
- ELECTRICAL NOTES:**
1. ALL WIRING SHALL BE CONTAINED IN CONDUIT OF PROPER SIZE.
 2. ALL WIRING SHALL CONFORM TO LOCAL, STATE AND FEDERAL CODES.
 3. ELECTRICAL PANEL TO BE LABELED CORRECTLY WITH LEGIBLE PRINT.
 4. ALL CONDUCTORS TO BE COPPER #12 AWG MINIMUM SIZE, OR AS REQUIRED BY LOAD AND OVER CURRENT PROTECTION.



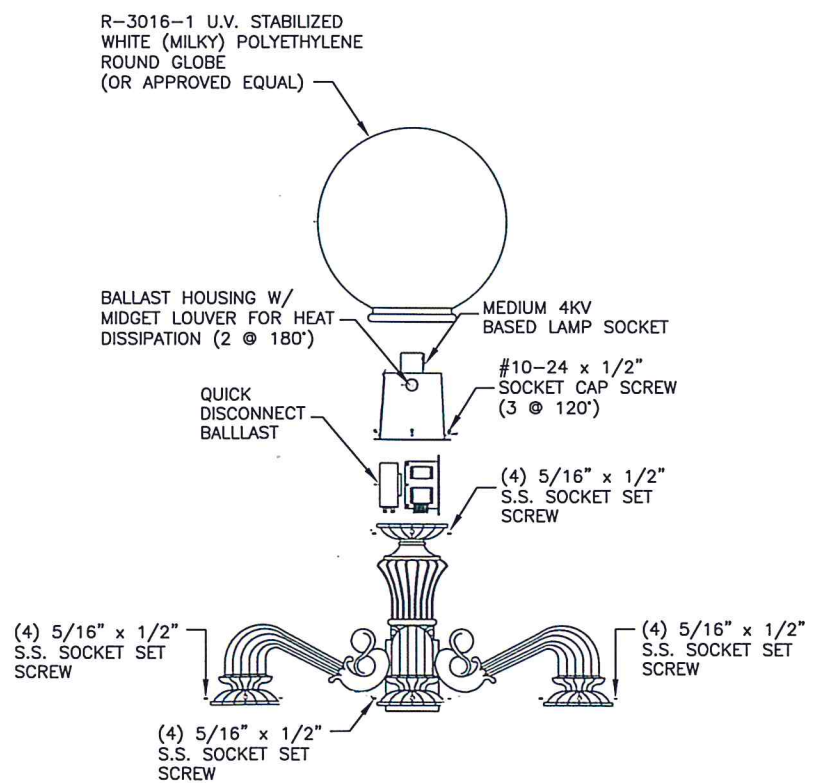
2 DECORATIVE LAMP BASE DETAIL
15 SCALE: 1" = 1'-0"



3 SHAFT DETAIL
15 SCALE: 3/4" = 1'-0"

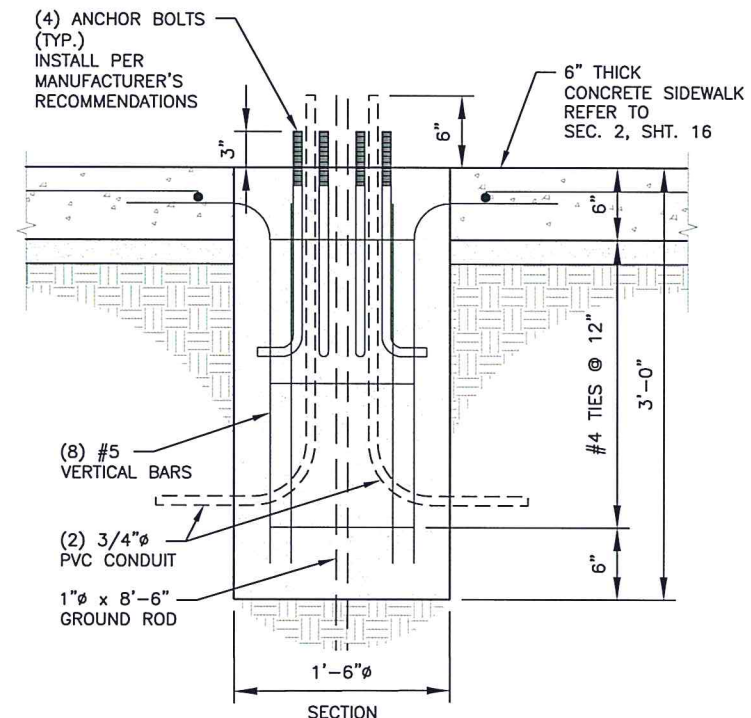
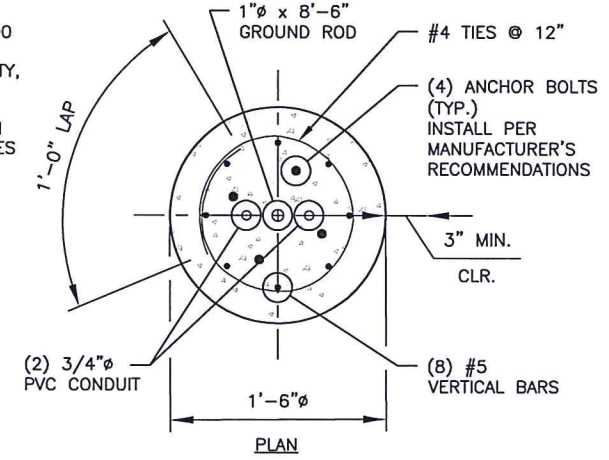


4 DECORATIVE LAMP BASE DETAIL
15 SCALE: 1" = 1'-0"



5 DECORATIVE LAMP BASE DETAIL
15 SCALE: 1/2" = 1'-0"

- NOTES:**
1. CONCRETE TO BE A MIN. 3000 PSI STRENGTH - 28 DAY
 2. VERIFY ANCHOR BOLT QUANTITY, SIZE AND PROJECTION WITH POLE MFR.
 3. CONFIRM POLE LOCATIONS ON SITE PRIOR TO POURING BASES & RUNNING CONDUITS.

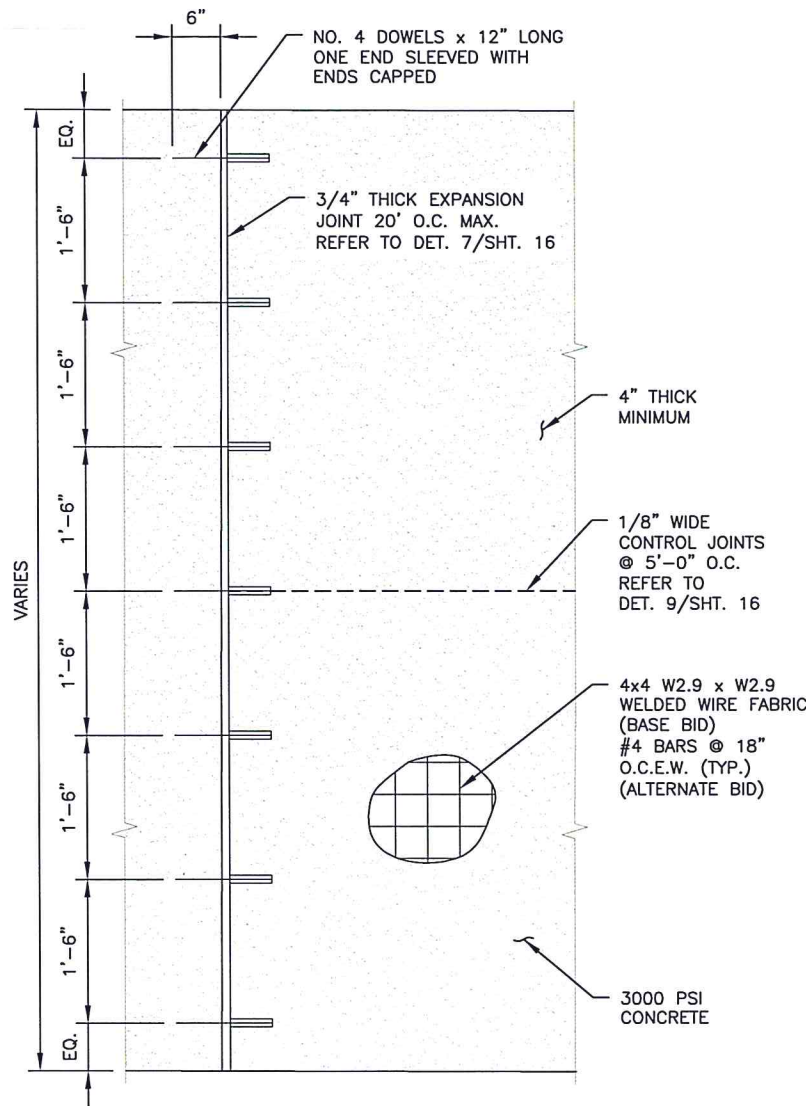


6 DECORATIVE LAMP BASE DETAIL
15 SCALE: 1/2" = 1'-0"

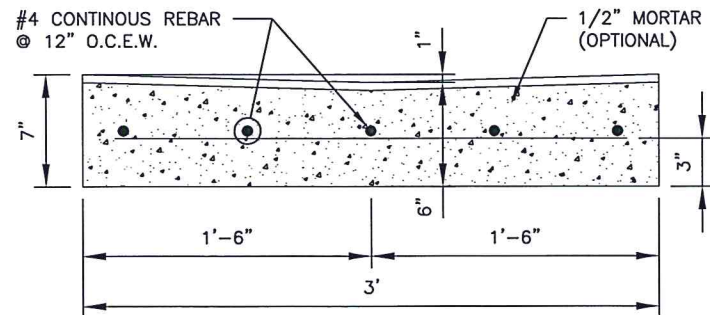
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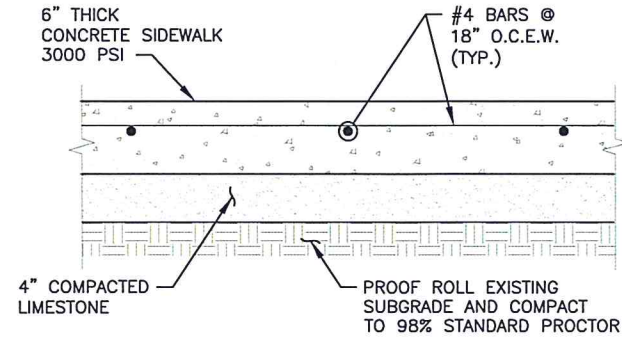
Rutilio P. Mora Jr 11/4/2019
RUTILIO P. MORA JR, P.E. NO. 111588



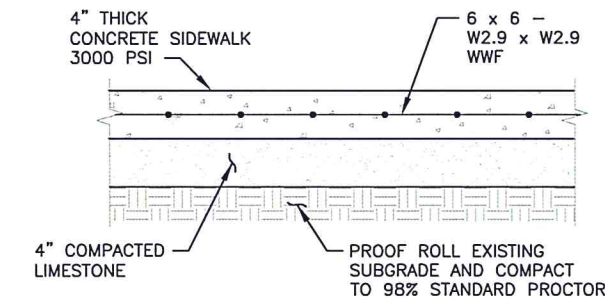
1 SIDEWALK DETAIL
16 SCALE: 1/2" = 1'-0"



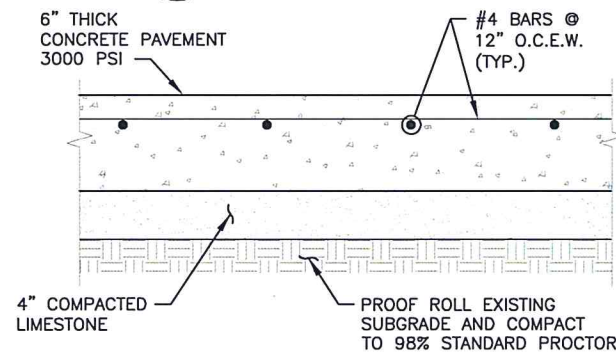
6 VALLEY GUTTER DETAIL
16 SCALE: NOT TO SCALE



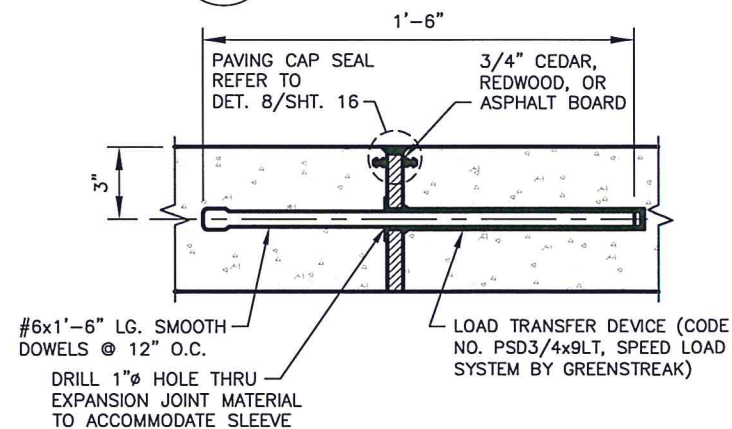
2 SIDEWALK SECTION (BASE BID)
16 SCALE: 3/4" = 1'-0"



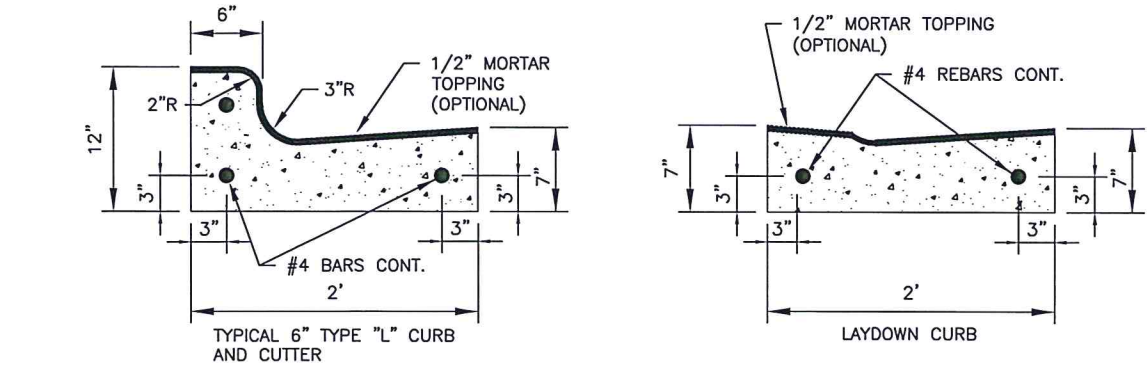
3 SIDEWALK SECTION (ALTERNATE BID #2)
16 SCALE: 3/4" = 1'-0"



4 PAVEMENT SECTION
16 SCALE: 3/4" = 1'-0"

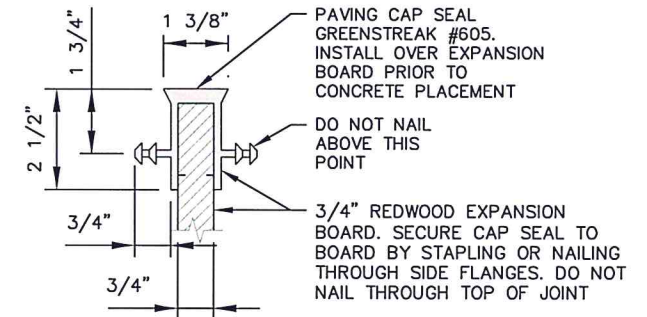


7 TYP. CONCRETE PAVEMENT EXPANSION JOINT DETAIL
16 SCALE: NOT TO SCALE

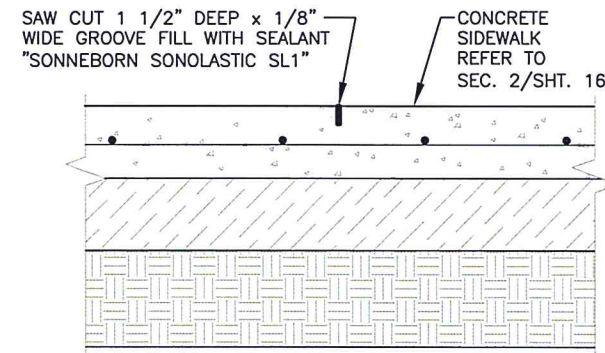


- NOTE:
1. BACKFILL BACK OF CURB.
2. REPAIR ALL HONEYCOMBING AND CRACKS IN MORTAR OR CONCRETE PRIOR TO BACKFILL AND PAVING.

5 CURB & GUTTER DETAILS
16 SCALE: 3/4" = 1'-0"



8 CAP SEAL DETAIL
16 SCALE: NOT TO SCALE



SAW CUT NOTE:
USE CONVENTIONAL SAW TO CUT JOINTS WITHIN 4 TO 12 HOURS AFTER FINISHING AND AS SOON AS THE CONCRETE HAS HARDENED SUFFICIENTLY TO PREVENT AGGREGATES FROM BEING DISLODGED BY THE SAW.

9 TYP. CONCRETE PAVEMENT CONTROL JOINT DETAIL
16 SCALE: 3/4" = 1'-0"

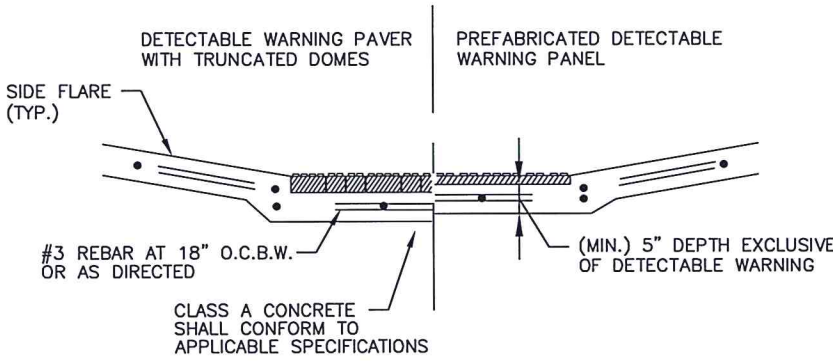
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CURB RAMPS

1. INSTALL A CURB RAMP OR BLENDED TRANSITION AT EACH PEDESTRIAN STREET CROSSING.
2. ALL SLOPES SHOWN ARE MAXIMUM ALLOWABLE. CROSS SLOPES OF 1.5% AND LESSER RUNNING SHOULD BE USED. ADJUST CURB RAMP LENGTH OR GRADE OF APPROACH SIDEWALKS AS DIRECTED.
3. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK AND CURB RAMP SURFACES IS 2%.
4. THE MINIMUM SIDEWALK WIDTH IS 5'. WHERE THE SIDEWALK IS ADJACENT TO THE BACK OF CURB, A 6' SIDEWALK WIDTH IS DESIRABLE. WHERE A 5' SIDEWALK CANNOT BE PROVIDED DUE TO SITE CONSTRAINTS, SIDEWALK WIDTH MAY BE REDUCED TO 4' FOR SHORT DISTANCES. 5' X 5' PASSING AREAS AT INTERVALS NOT TO EXCEED 200' ARE REQUIRED.
5. TURNING SPACES SHALL BE 5' X 5' MINIMUM. CROSS SLOPE SHALL BE MAXIMUM 2%.
6. CLEAR SPACE AT THE BOTTOM OF CURB RAMPS SHALL BE A MINIMUM OF 4' X 4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICULAR TRAVEL PATH.
7. PROVIDE FLARED SIDES WHERE THE PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP. FLARED SIDES SHALL BE SLOPED AT 10% MAXIMUM, MEASURED PARALLEL TO THE CURB. RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP, EITHER BECAUSE THE ADJACENT SURFACE IS PLANTED, SUBSTANTIALLY OBSTRUCTED, OR OTHERWISE PROTECTED.
8. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE LATEST DRAFT OF THE PROPOSED GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT OF WAY (PROWAG) AS PUBLISHED BY THE U.S. ARCHITECTURAL AND TRANSPORTATION BARRIERS COMPLIANCE BOARD (ACCESS BOARD).
9. TO SERVE AS A PEDESTRIAN REFUGE AREA, THE MEDIAN SHOULD BE A MINIMUM OF 6' WIDE, MEASURED FROM BACK OF CURBS. MEDIANS SHOULD BE DESIGNED TO PROVIDE ACCESSIBLE PASSAGE OVER OR THROUGH THEM.
10. SMALL CHANNELIZATION ISLANDS, WHICH DO NOT PROVIDE A MINIMUM 5'X 5' LANDING AT THE TOP OF CURB RAMPS, SHALL BE CUT THROUGH LEVEL WITH THE SURFACE OF THE STREET.
11. CROSSWALK DIMENSIONS, CROSSWALK MARKINGS AND STOP BAR LOCATIONS SHALL BE AS SHOWN ELSEWHERE IN THE PLANS. AT INTERSECTIONS WHERE CROSSWALK MARKINGS ARE NOT REQUIRED, CURB RAMPS SHALL ALIGN WITH THEORETICAL CROSSWALKS UNLESS OTHERWISE DIRECTED.
12. PROVIDE CURB RAMPS TO CONNECT THE PEDESTRIAN ACCESS ROUTE AT EACH PEDESTRIAN STREET CROSSING. HANDRAILS ARE NOT REQUIRED ON CURB RAMPS.
13. CURB RAMPS AND LANDINGS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM 531 "SIDEWALKS".
14. PLACE CONCRETE AT A MINIMUM DEPTH OF 5" FOR RAMPS, FLARES AND LANDINGS, UNLESS OTHERWISE DIRECTED.
15. FURNISH AND INSTALL NO. 3 REINFORCING STEEL BARS AT 18" O.C. BOTH WAYS, UNLESS OTHERWISE DIRECTED.
16. PROVIDE A SMOOTH TRANSITION WHERE THE CURB RAMPS CONNECT TO THE STREET.
17. EXISTING FEATURES THAT COMPLY WITH APPLICABLE STANDARDS MAY REMAIN IN PLACE UNLESS OTHERWISE SHOWN ON THE PLANS.



3 CURB RAMP DETAIL @ DETECTIBLE WARNINGS
17 SCALE: N.T.S.

DETECTABLE WARNING MATERIAL

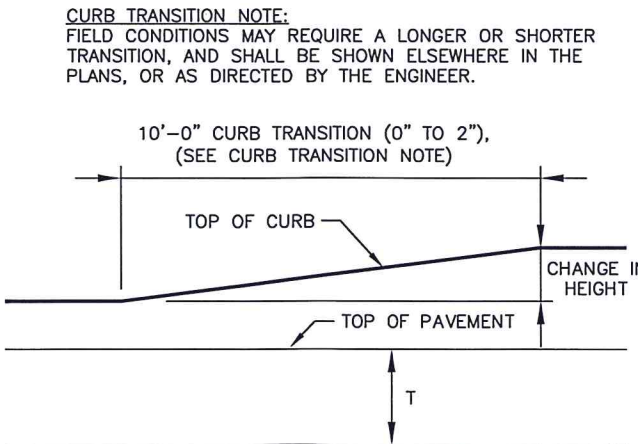
1. CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES COMPLYING WITH PROWAG. THE SURFACE MUST CONTRAST VISUALLY WITH ADJOINING SURFACES, INCLUDING SIDE FLARES. FURNISH AND INSTALL AN APPROVED CAST-IN-PLACE DARK BROWN OR DARK RED DETECTABLE WARNING SURFACE MATERIAL ADJACENT TO UNCOLORED CONCRETE, UNLESS SPECIFIED ELSEWHERE IN THE PLANS.
2. DETECTABLE WARNING MATERIALS MUST MEET TXDOT DEPARTMENTAL MATERIALS SPECIFICATION DMS 4350 AND BE LISTED ON THE MATERIAL PRODUCER LIST. INSTALL PRODUCTS IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
3. DETECTABLE WARNING SURFACES MUST BE FIRM, STABLE AND SLIP RESISTANT.
4. DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 24 INCHES IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL, AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR LANDING WHERE THE PEDESTRIAN ACCESS ROUTE ENTERS THE STREET.
5. DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS AT THE BACK OF CURB AND NEITHER END OF THAT EDGE IS GREATER THAN 5 FEET FROM THE BACK OF CURB. DETECTABLE WARNING SURFACES MAY BE CURVED ALONG THE CORNER RADIUS.
6. SHADED AREAS ON SHEET 1 OF 4 INDICATE THE APPROXIMATE LOCATION FOR THE DETECTABLE WARNING SURFACE FOR EACH CURB RAMP TYPE.

DETECTABLE WARNING PAVERS (IF USED)

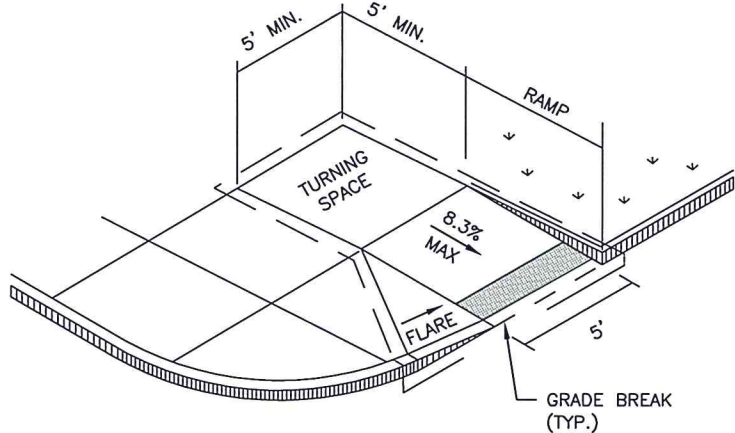
1. FURNISH DETECTABLE WARNING PAVER UNITS MEETING ALL REQUIREMENTS OF ASTM C-936, C-33. LAY IN A TWO BY TWO UNIT BASKET WEAVE PATTERN OR AS DIRECTED.
2. LAY FULL-SIZE UNITS FIRST FOLLOWED BY CLOSURE UNITS CONSISTING OF AT LEAST 25 PERCENT (25%) OF A FULL UNIT. CUT DETECTABLE WARNING PAVER UNITS USING A POWER SAW.

SIDEWALKS

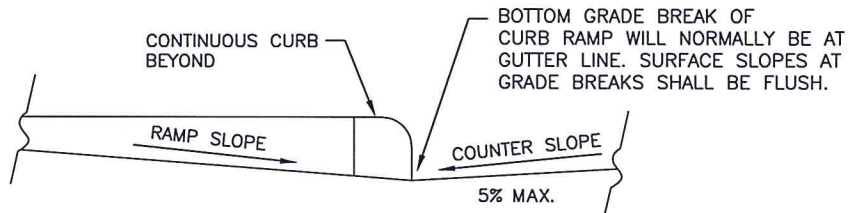
1. PROVIDE CLEAR GROUND SPACE AT OPERABLE PARTS, INCLUDING PEDESTRIAN PUSH BUTTONS. OPERABLE PARTS SHALL BE PLACED WITHIN UNOBSTRUCTED REACH RANGE SPECIFIED IN PROWAG SECTION R406.
2. PLACE TRAFFIC SIGNAL OR ILLUMINATION POLES, GROUND BOXES, CONTROLLER BOXES, SIGNS, DRAINAGE FACILITIES AND OTHER ITEMS SO AS NOT TO OBSTRUCT THE PEDESTRIAN ACCESS ROUTE OR CLEAR GROUND SPACE.
3. STREET GRADES AND CROSS SLOPES SHALL BE AS SHOWN ELSEWHERE IN THE PLANS. CHANGES IN LEVEL GREATER THAN 1/4 INCH ARE NOT PERMITTED.
5. THE LEAST POSSIBLE GRADE SHOULD BE USED TO MAXIMIZE ACCESSIBILITY. THE RUNNING SLOPE OF SIDEWALKS AND CROSSWALKS WITHIN THE PUBLIC RIGHT OF WAY MAY FOLLOW THE GRADE OF THE PARALLEL ROADWAY. WHERE A CONTINUOUS GRADE GREATER THAN FIVE PERCENT (5%) MUST BE PROVIDED, HANDRAILS MAY BE DESIRABLE TO IMPROVE ACCESSIBILITY. HANDRAILS MAY ALSO BE NEEDED TO PROTECT PEDESTRIANS FROM POTENTIALLY HAZARDOUS CONDITIONS. IF PROVIDED, HANDRAILS SHALL COMPLY WITH PROWAG R409.
6. HANDRAIL EXTENSIONS SHALL NOT PROTRUDE INTO THE USABLE LANDING AREA OR INTO INTERSECTING PEDESTRIAN ROUTES.
7. DRIVEWAYS AND TURNOUTS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM "INTERSECTIONS, DRIVEWAYS AND TURNOUTS". SIDEWALKS SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH ITEM, "SIDEWALKS".
8. SIDEWALK DETAILS ARE SHOWN ELSEWHERE IN THE PLANS.



4 CURB TRANSITION
17 SCALE: N.T.S.



1 PERPENDICULAR CURB RAMP
17 SCALE: N.T.S.



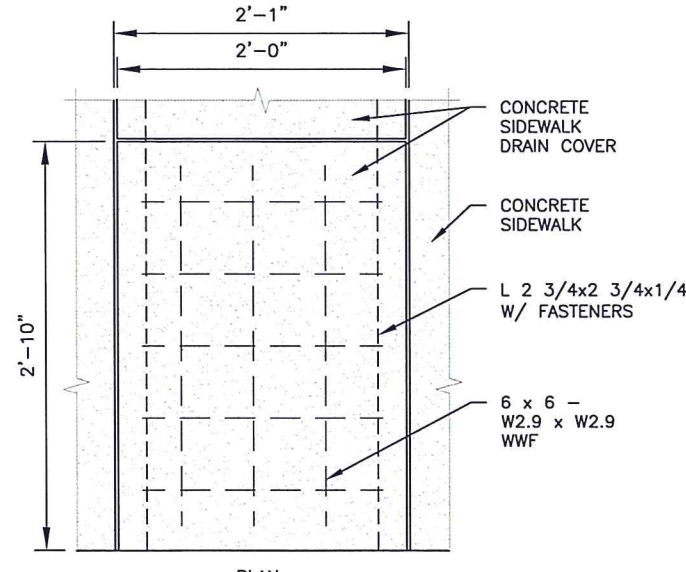
2 TYP. SECTION OF PERPENDICULAR CURB RAMP AT CONNECTION TO ROADWAY
17 SCALE: N.T.S.

CURB TRANSITION NOTE:
FIELD CONDITIONS MAY REQUIRE A LONGER OR SHORTER TRANSITION, AND SHALL BE SHOWN ELSEWHERE IN THE PLANS, OR AS DIRECTED BY THE ENGINEER.

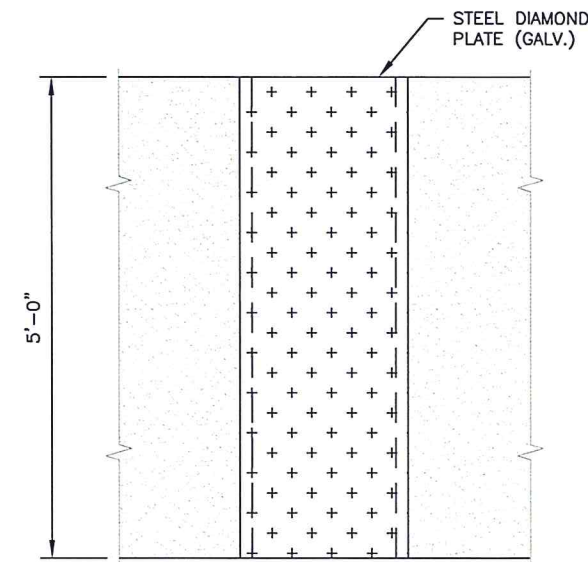
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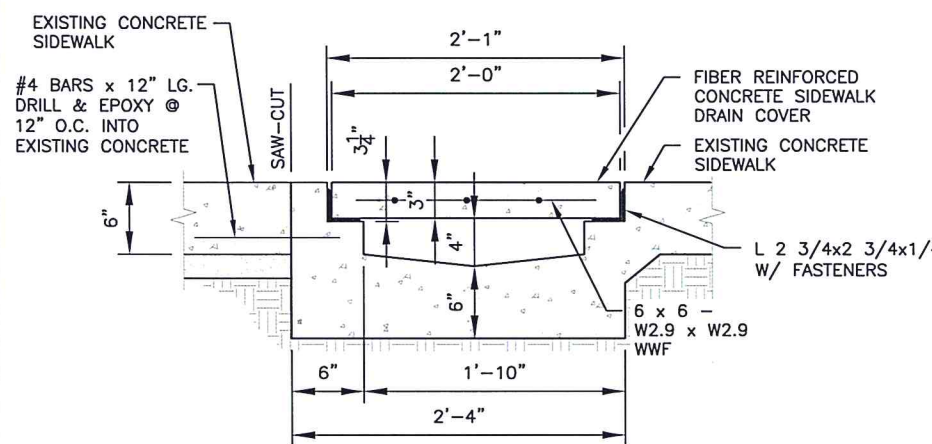
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RUTILIO P. MORA JR., P.E. NO. 111588



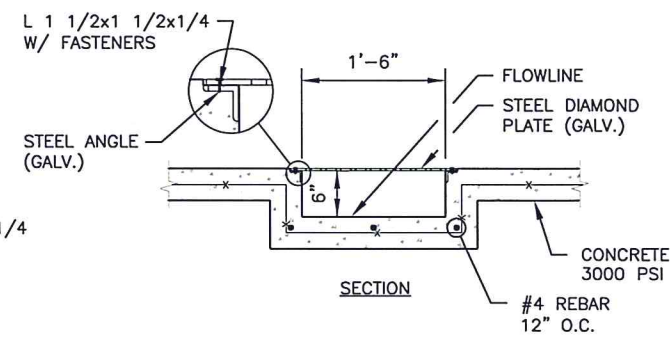
PLAN



PLAN



SECTION

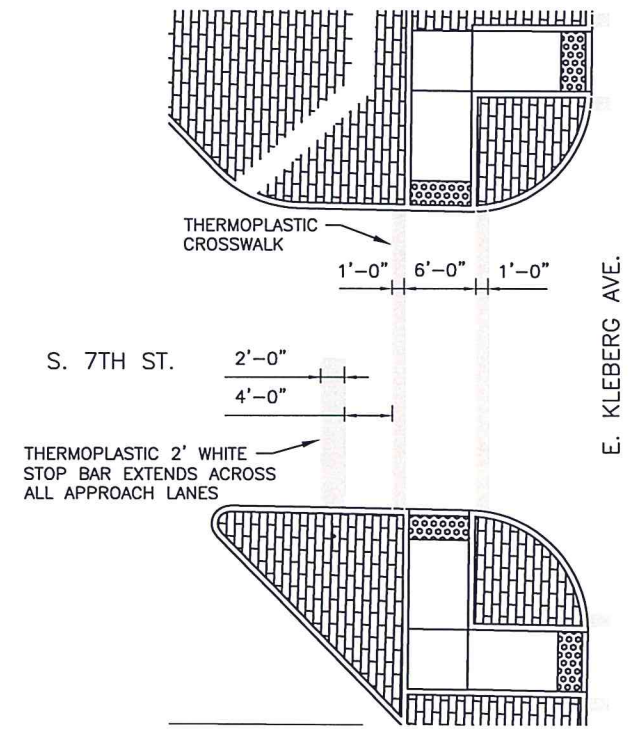


SECTION

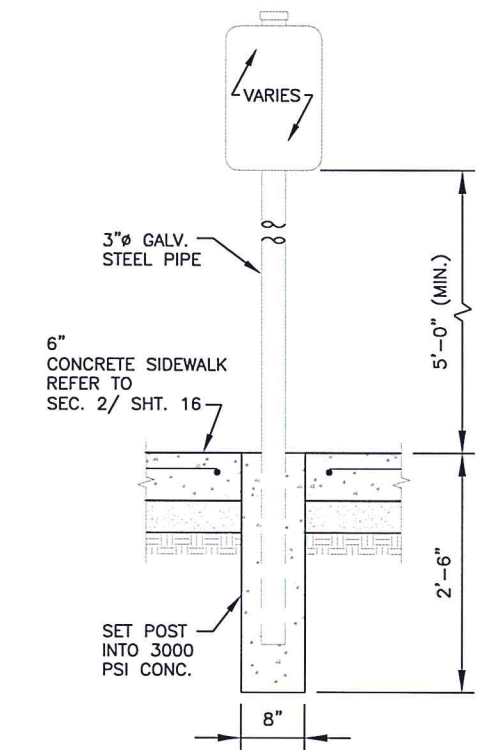
2
18 **SIDEWALK DRAIN DETAIL (ALTERNATE BID #4)**
SCALE: 1/2" = 1'-0"



3
18 **SIGN DETAILS**
SCALE: 3/4" = 1'-0"

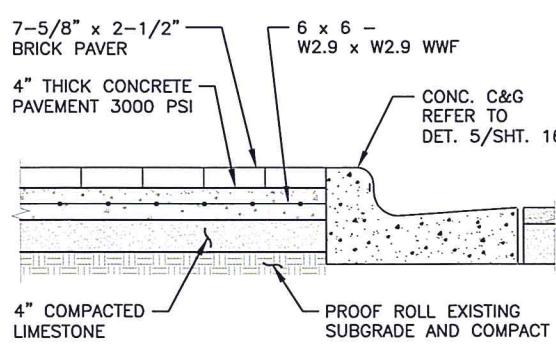


4
18 **CROSSWALK DETAIL**
SCALE: N.T.S.

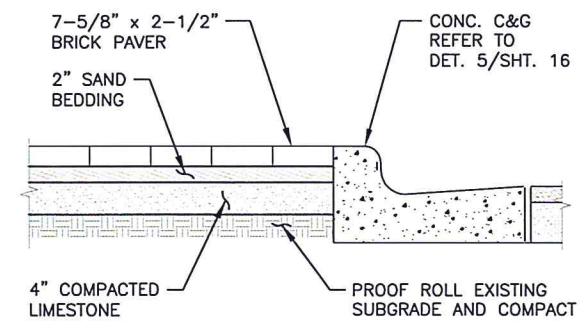


5
18 **SIGN POST DETAIL**
SCALE: 1/2" = 1'-0"

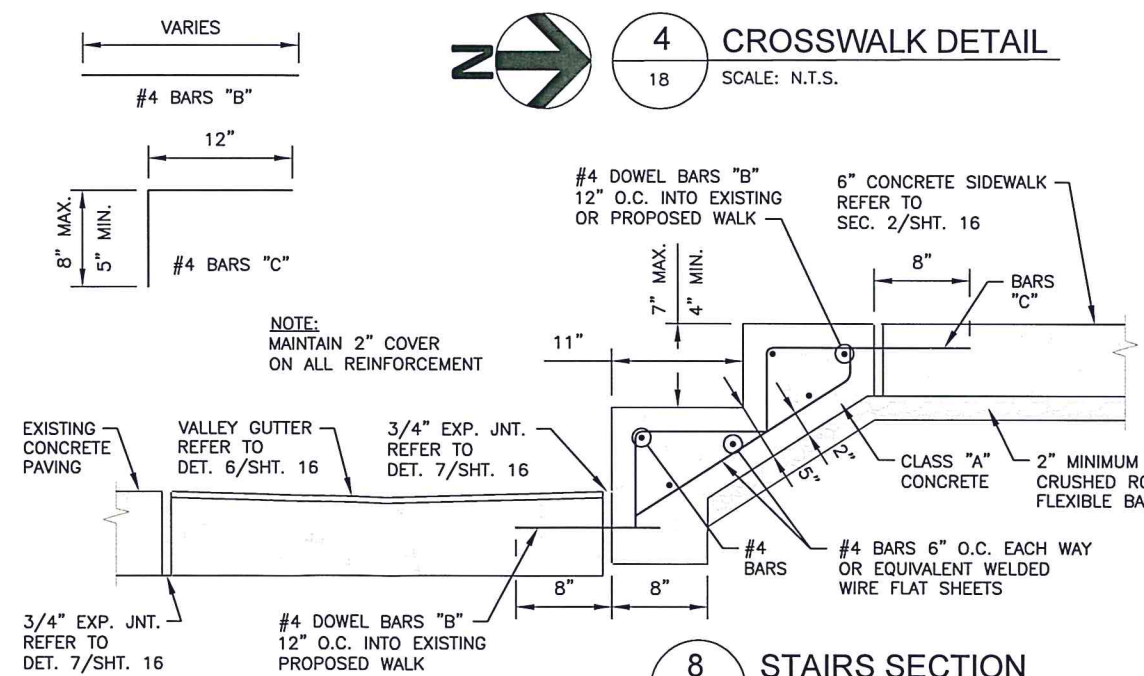
1
18 **SIDEWALK DRAIN DETAIL (BASE BID)**
SCALE: 3/4" = 1'-0"



6
18 **BRICK PAVER DETAIL (BASE BID)**
SCALE: 1/2" = 1'-0"

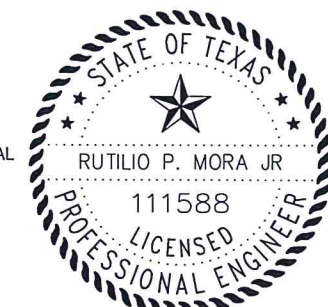


7
18 **BRICK PAVER DETAIL (ALTERNATE BID #3)**
SCALE: 1/2" = 1'-0"

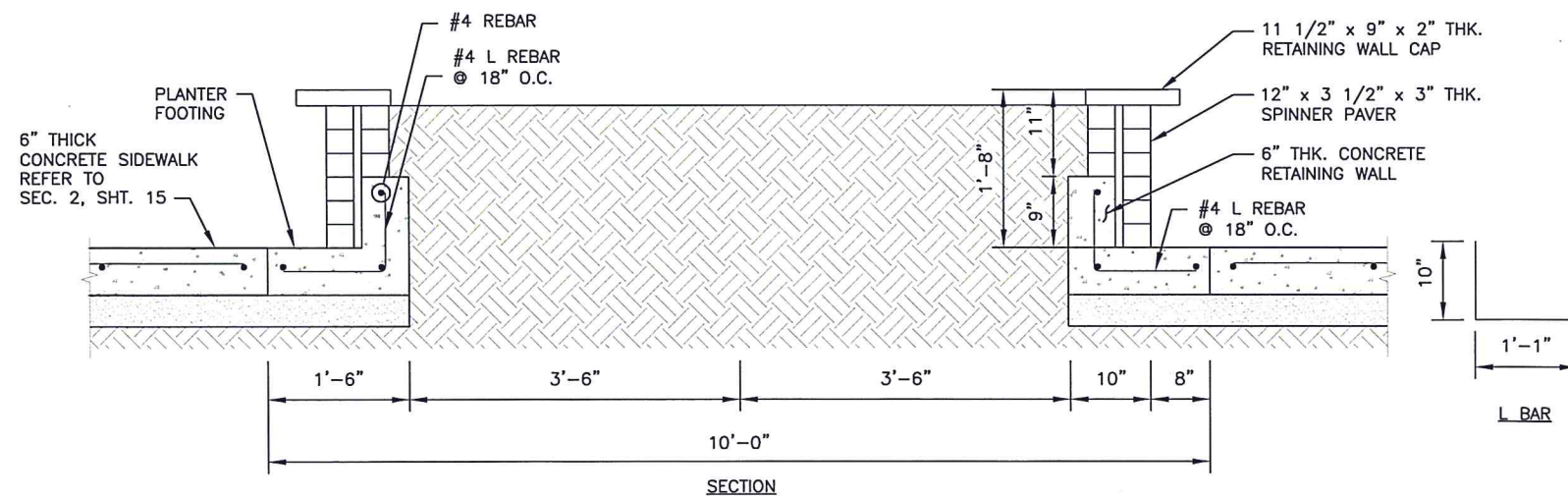
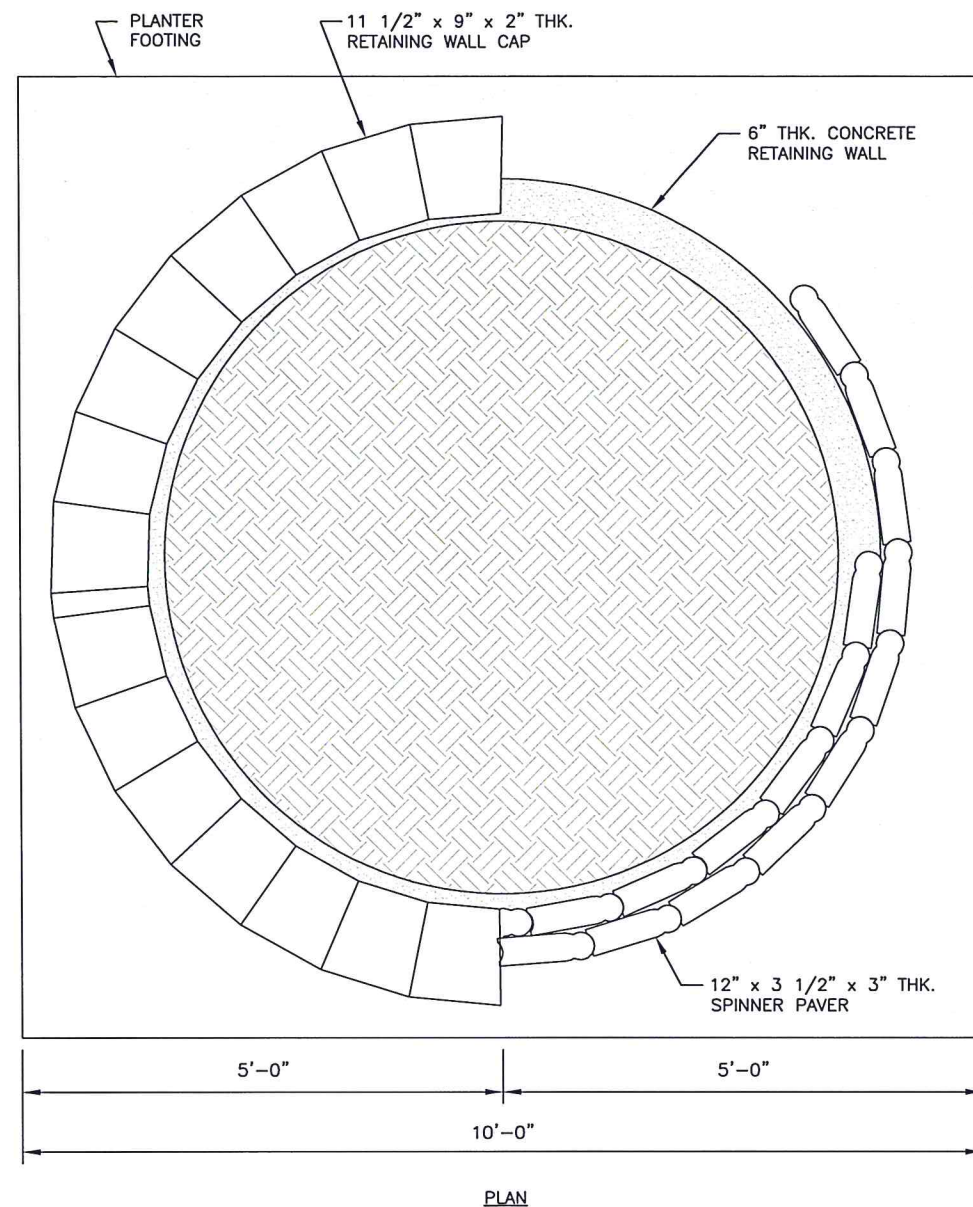


8
18 **STAIRS SECTION**
SCALE: 3/4" = 1'-0"

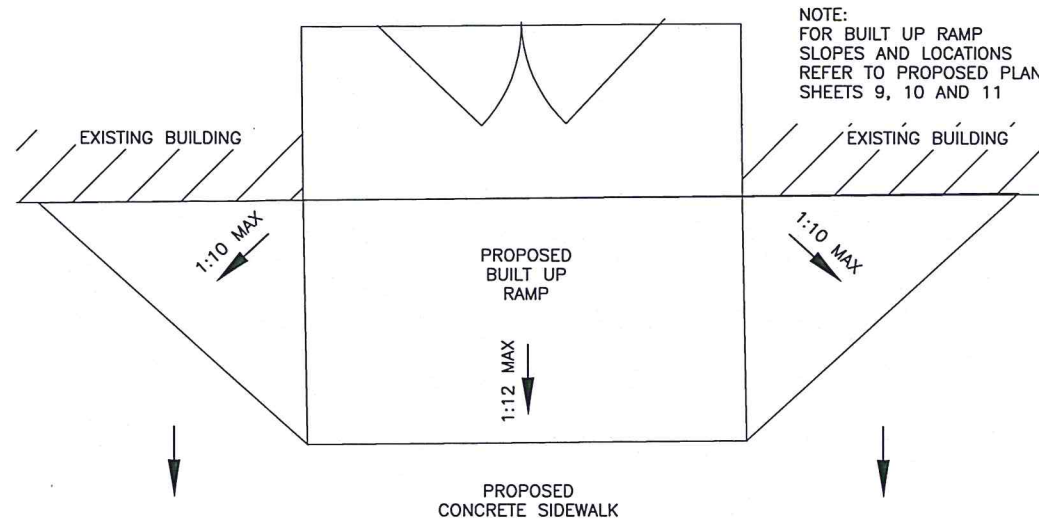
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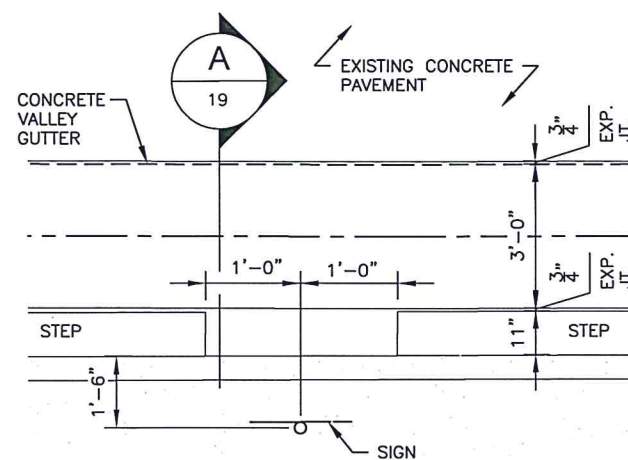
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RUTILIO P. MORA JR, P.E. NO. 111588



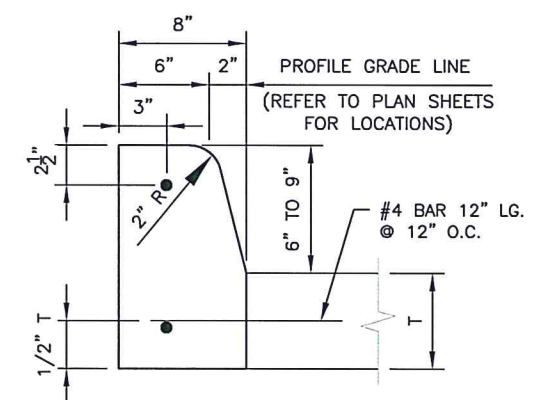
1 PLANTER DETAIL
19 SCALE: 1/2" = 1'-0"



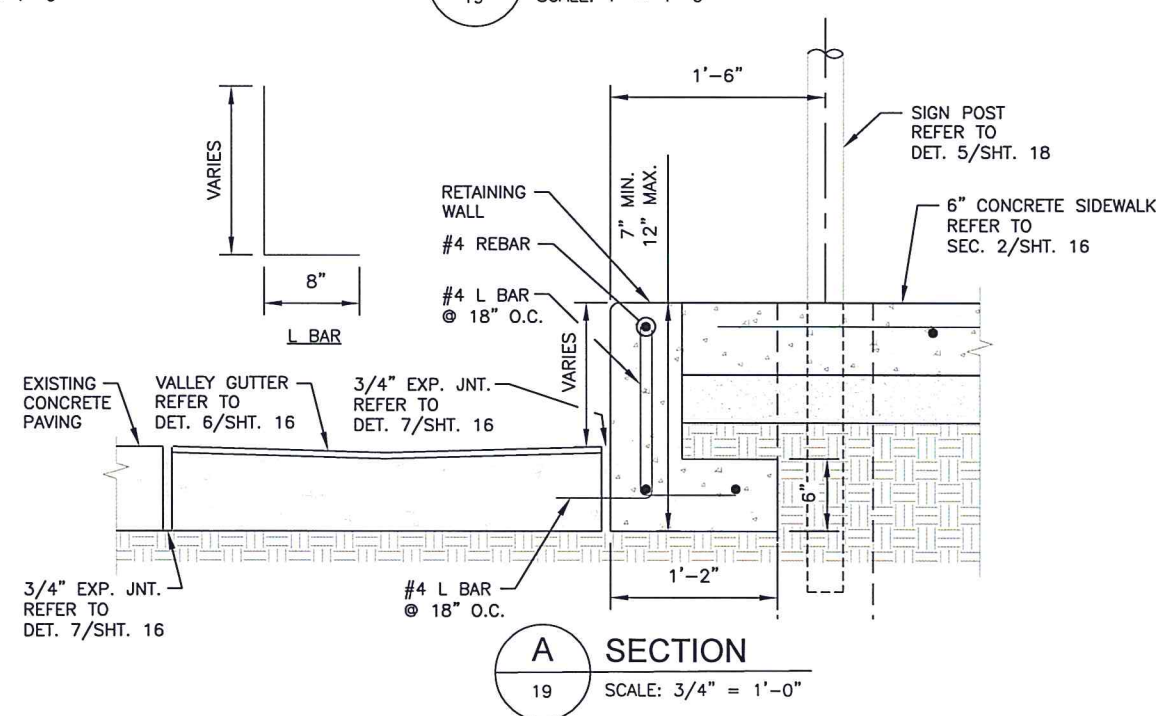
2 BUILT UP RAMP DETAIL
19 SCALE: 1/4" = 1'-0"



3 SIGN PLACEMENT DETAIL



4 TYPE II MONOLITHIC CURB DETAIL
19 SCALE: 1" = 1'-0"



NOTE:
FOR BUILT UP RAMP
SLOPES AND LOCATIONS
REFER TO PROPOSED PLAN
SHEETS 9, 10 AND 11

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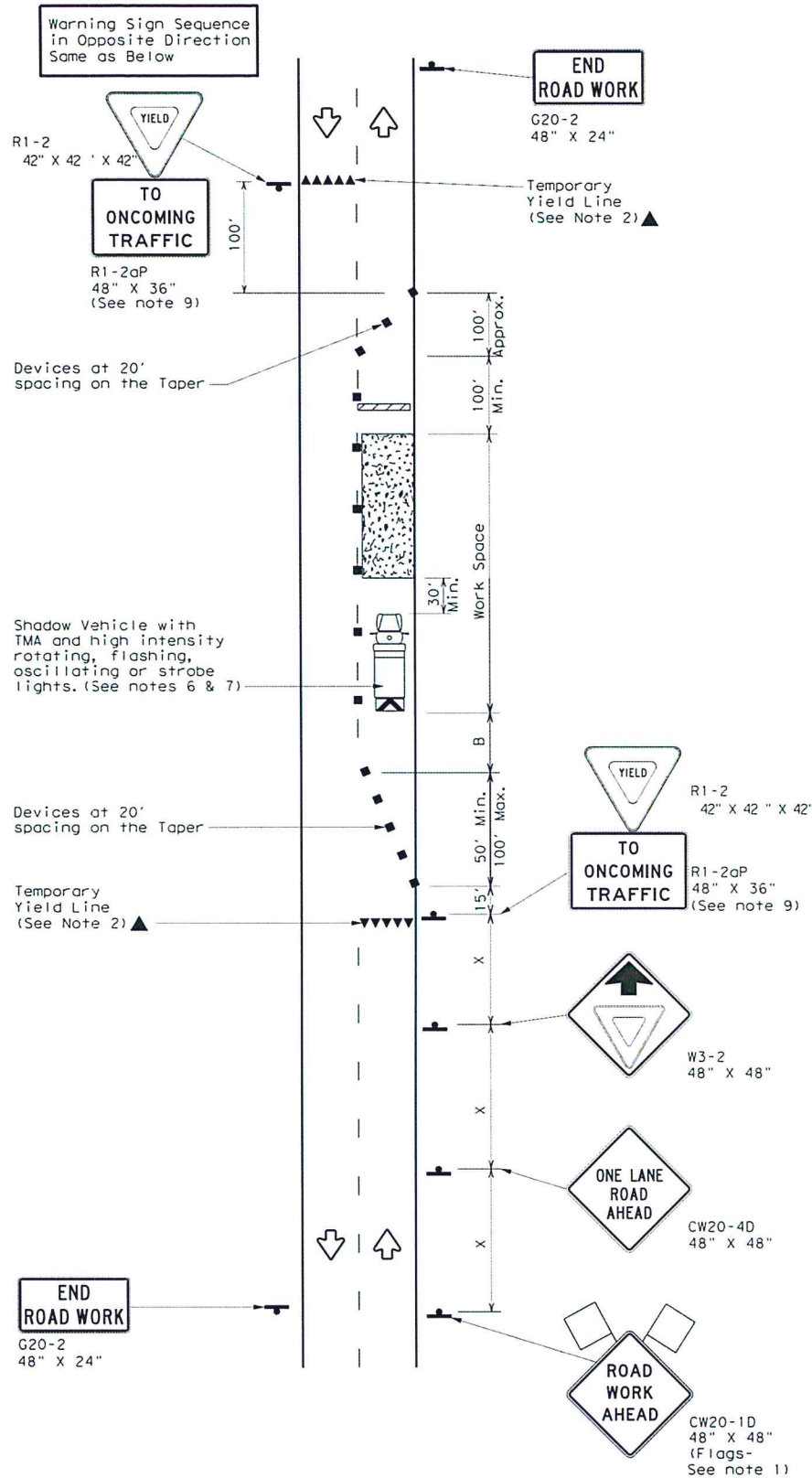


Ruthie L. Mora, 11/4/2019
RUTILIO P. MORA JR, P.E. NO. 111588

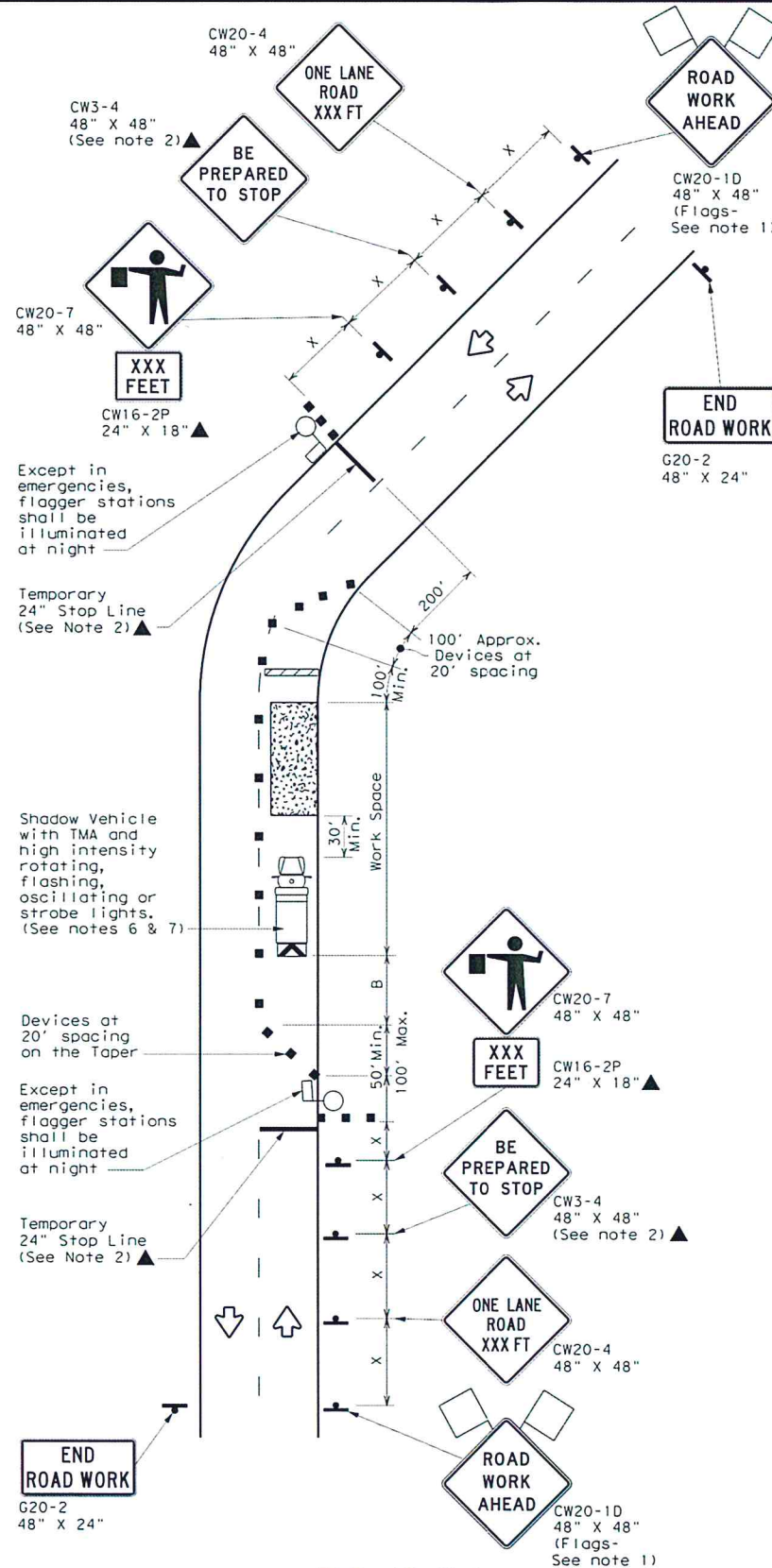


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DATE: FILE:



TCP (2-2a)
2-LANE ROADWAY WITHOUT PAVED SHOULDERS
ONE LANE TWO-WAY
CONTROL WITH YIELD SIGNS
(Less than 2000 ADT - See Note 9)



TCP (2-2b)
2-LANE ROADWAY WITHOUT PAVED SHOULDERS
ONE LANE TWO-WAY
CONTROL WITH FLAGGERS

LEGEND					
	Type 3 Barricade		Channelizing Devices		
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)		
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)		
	Sign		Traffic Flow		
	Flag		Flagger		

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "x" Distance	Suggested Longitudinal Buffer Space "B"	Stopping Sight Distance
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent			
30	$L = \frac{WS^2}{60}$	150'	165'	180'	30'	60'	120'	90'	200'
35		205'	225'	245'	35'	70'	160'	120'	250'
40		265'	295'	320'	40'	80'	240'	155'	305'
45	L = WS	450'	495'	540'	45'	90'	320'	195'	360'
50		500'	550'	600'	50'	100'	400'	240'	425'
55		550'	605'	660'	55'	110'	500'	295'	495'
60		600'	660'	720'	60'	120'	600'	350'	570'
65		650'	715'	780'	65'	130'	700'	410'	645'
70		700'	770'	840'	70'	140'	800'	475'	730'
75		750'	825'	900'	75'	150'	900'	540'	820'

* Conventional Roads Only
** Taper lengths have been rounded off.
L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓	✓	

GENERAL NOTES

- Flags attached to signs where shown, are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- The CW3-4 "BE PREPARED TO STOP" sign may be installed after the CW20-4 "ONE LANE ROAD XXX FT" sign, but proper sign spacing shall be maintained.
- Flaggers should use two-way radios or other methods of communication to control traffic.
- Length of work space should be based on the ability of flaggers to communicate.
- A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
- Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect a wider work space.

TCP (2-2a)

- The R1-2 "YIELD" sign traffic control may be used on projects with approaches that have adequate sight distance. For projects in urban areas, work space should be no longer than one half city block. In rural areas, roadways with less than 2000 ADT, work space should be no longer than 400 feet.
- The R1-2aP "YIELD TO ONCOMING TRAFFIC" sign shall be placed on a support at a 7 foot minimum mounting height.

TCP (2-2b)

- Channelizing devices on the center line may be omitted when a pilot car is leading traffic and approved by the Engineer.
- If the work space is located near a horizontal or vertical curve, the buffer distances should be increased in order to maintain stopping sight distance to the flagger and a queue of stopped vehicles. (See table above).
- Flaggers should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

		Traffic Operations Division Standard			
TRAFFIC CONTROL PLAN ONE-LANE TWO-WAY TRAFFIC CONTROL					
TCP (2-2) - 18					
FILE: tcp2-2-18.dgn	DN: CAS	DS: CAS	CS: CAS		
© TxDOT December 1985	CONT	SECT	JOB HIGHWAY		
REVISIONS					
8-95 3-03					
1-97 2-12					
4-98 2-18					
162					